

CITY OF POMONA COUNCIL REPORT

July 15, 2019

To: Honorable Mayor and Members of the City Council

From: Linda Lowry, City Manager

Submitted By: Anita D. Gutierrez, AICP, Development Services Director

SUBJECT: INTRODUCTION AND FIRST READING OF A COMPREHENSIVE

UPDATE TO THE DOWNTOWN POMONA SPECIFIC PLAN (SPA 6409-2016) AND RELATED UPDATES TO THE POMONA CORRIDORS

SPECIFIC PLAN (SPA 12106-2019) AND POMONA ZONING

ORDINANCE (ZONE 12104-2019)

RECOMMENDATION:

That the City Council open the public hearing and after receiving testimony and public comment close the public hearing and take the following actions:

1. Introduce for first reading an Ordinance approving a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) (Attachment 1).

ORDINANCE NO. 4267 - AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF POMONA, CALIFORNIA APPROVING A COMPREHENSIVE UPDATE TO THE DOWNTOWN POMONA SPECIFIC PLAN (SPA 6409-2016) AND RELATED UPDATES TO THE POMONA CORRIDORS SPECIFIC PLAN (SPA 12106-2019) AND POMONA ZONING ORDINANCE (ZONE 12104-2019).

EXECUTIVE SUMMARY:

The Downtown Pomona Specific Plan ("DPSP") was first adopted in 1994 and has had several targeted updates since its initial adoption, however the Plan remained in need of a complete update. In 2016, the City was awarded a grant from the Los Angeles County Metropolitan Transportation Authority ("Metro Los Angeles") in the amount of \$220,000 to create a regulatory environment supportive of transit-oriented development ("TOD") around station areas and adjacent transit corridors in Downtown Pomona ("Downtown"). The Planning Division initiated a comprehensive update to the Downtown Pomona Specific Plan to achieve this grant goal. This request is to adopt a comprehensive update to the Downtown Pomona Specific Plan, and related updates to the Pomona

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Corridors Specific Plan and Pomona Zoning Ordinance, with changes as recommended by the Planning Commission at its June 26, 2019 public hearing on the item (Attachment 2).

FISCAL IMPACT:

There are no anticipated short-term or long-term fiscal impacts associated with this action.

PUBLIC NOTICING REQUIREMENTS:

Public noticing was completed as required by Government Code 65091. A hearing for public notice was published on Friday, July 5, 2019 in the Inland Valley Daily Bulletin (Attachment 3).

PREVIOUS COUNCIL ACTION:

The City Council adopted the original Downtown Pomona Specific Plan in May 1994. It has since been amended eight times by City Council on the following dates: August 2005 (SPA 05-001), January 2006 (SPA 05-002), May 2007 (SPA 06-003 and SPA 07-002), September 2007 (SPA 07-003), April 2011 (SPA 10-002), April 2013 (SPA 12-001), and December 2013 (SPA 13-003).

PREVIOUS RELATED ACTION:

On June 26, 2019, the Planning Commission recommended approval of a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019), with recommended changes as found in the Errata Sheet to the City Council on a 6-0-0-1 vote, with Commissioner Bunce being absent..

ENVIRONMENTAL IMPACT:

According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified", except as might be necessary to determine whether there are project-specific significant effects. A Finding of Consistency was prepared to conduct this analysis (Attachment 4).

This Finding of Consistency evaluates the updated DPSP's consistency with the Final Environmental Impact Report for the City of Pomona General Plan Update. The 2014 Final EIR, which was certified in March 2014, establishes the City's vision for development y through the year 2035. The evaluation determined whether the Plan update would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. The Finding of Consistency has been prepared pursuant to the applicable provisions of §15183 of the California Environmental Quality Act (CEQA) Guidelines.

The Findings of Consistency concluded that development facilitated by the Plan update would be consistent with the development intensity established by the City's General Plan. Environmental impacts associated with the Plan update would be similar to those anticipated for Downtown in the

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2014 General Plan Final EIR and the Plan update would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required for the Plan update under CEQA.

PROJECT DESCRIPTION & BACKGROUND

In 2016, the Planning Division seized an opportunity to pursue grant funding to update the DPSP. The City was awarded a grant from the Metro Los Angeles in the amount of \$220,000 to create a regulatory environment supportive of TOD around station areas and adjacent transit corridors in Downtown. The Planning Division selected a consulting firm, RRM Design, to draft the comprehensive update ("Plan update"). Beginning in 2017, RRM Design conducted research, including stakeholder interviews and community forums, and delivered a draft of the Plan update in June 2018. Due to significant turnover in staffing, the Planning Division initiated a draft review six months later in December 2018. As the grant agreement with Metro Los Angeles was set to expire at the end of 2018, a six-month extension request was submitted and approved, giving the Planning Division until July 2019 to finalize the Plan update. To streamline this process, the following four key priorities for the Plan update adoption were established:

- 1. Reconcile Downtown Pomona Specific Plan boundaries with the Pomona Corridors Specific Plan
- 2. Implement the 2014 City of Pomona General Plan goals and policies
- 3. Achieve Metro's TOD goals
- 4. Streamline the DPSP as a document and the development review process

GOAL #1: RECONCILING BOUNDARIES

Cleaning Up Dual Zoning

The Pomona Corridors Specific Plan ("PCSP") was adopted in March 2014, affecting parcels along Holt Boulevard, Garey Avenue, and Mission Boulevard, which historically were included in the DPSP. As part of that action, a cleanup was approved to reconcile boundaries between these two plans. However, as the approval only identified rough boundaries, a parcel level cleanup was not been conducted. This has left parcels along these corridors with dual zoning, and in some cases split zoning, which has led to code applicability challenges for entitlement and design review. Moreover, several parcels on the edges of the PCSP remain entirely within the DPSP but are noncontiguous with the rest of Downtown. The Plan update will establish parcel level boundaries (Exhibits A-C of Attachment 1) to clarify code applicability and eliminate non-contiguous Downtown parcels.

Establishing Complementary Boundaries

When adopted, the PCSP included multiple parcels along Garey Avenue identified in either the "Downtown Gateway" or "Downtown Core" segment. This includes land use classifications, development standards, and public right-of-way improvement recommendations. In the "Downtown Core" segment, land use classifications defer to the DPSP, further blurring the lines of

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the two plans (Figure 1). This has effectively minimized the reach of the DPSP along Garey Avenue and diminished boundary identities and land use classifications for either plan in Downtown.

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Figure 1. Conflicting PCSP & DPSP boundaries diminish each Plan's identity

The proposed boundary of the Downtown Pomona Specific Plan (Figure 2) addresses such boundary conflicts by establishing a clear identity for Downtown Pomona, with Center Street as the northern boundary, Fourth Street as the general southern boundary, White Avenue as the western boundary, and Towne Avenue as the general eastern boundary. The boundary also establishes a clear complement and relationship to Pomona's major corridors of Holt and Mission Boulevards.

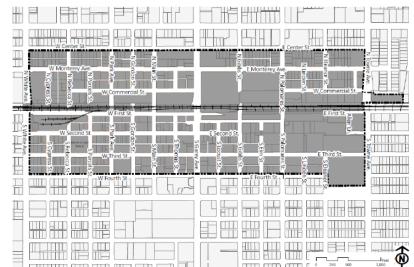


Figure 2. Proposed DPSP Boundary establishes a clear geographic Downtown identity

Furthermore, the land use classifications of "Downtown Core" segment in the PCSP will be updated and will no longer rely on the DPSP. Parcels south of Mission Boulevard currently zoned in the DPSP will change to the "Midtown Segment" of the PCSP, with updated land use

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classifications to mirror the classifications currently allowed (Exhibit B2 of Attachment 1). In effect, the PCSP running north along Garey Avenue will pause at Fourth Street and resume at Center Street, leaving the DPSP along this stretch of Garey Avenue uninterrupted.

Expanding Open Space

The proposed boundary adjustment also necessitated a re-designation of several parcels particularly at Civic Center Plaza and at two City parks within the existing DPSP (Memorial Park and Centennial Park) that are currently zoned mixed-use designations to prevent the creation of any new non-contiguous parcels. To prevent the creation of any new non-contiguous parcels, and to preserve the land development potential of these two City parks, the zoning of these parcels will change to Open Space in the Pomona Zoning Ordinance (Exhibit C of Attachment 1). This change of zone will officially add Memorial and Centennial Parks into the Open Space acreage total of the City, and will shift the identity of Civic Center Plaza from a Downtown destination to a public park that complements both the PCSP and DPSP. This action is consistent with Section .38-1 of the Pomona Municipal Code, which technically identifies Civic Center Plaza as a public park.

GOAL #2: IMPLEMENTING THE POMONA GENERAL PLAN

Updating Transect Density

The Pomona General Plan ("General Plan") was adopted in 2014. In the Land Use chapter, new density thresholds were established citywide, using the theory of transect zoning, which establishes development potential based on geographical context and street patterns. The highest densities are proposed for Downtown, however the DPSP includes density thresholds that are significantly less than what the General Plan has contemplated, creating an inconsistency in development potential (Figure 3).

Figure 3.	. Proposed	l densities in the	General Plan ti	hat correlate to	DPSP boundaries
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	T6-A DOWNTOWN CORE	T6-B Typical	T5 TYPICAL	T4-A TYPICAL
Height	2 floors min. 12 floors max.	2 floors or 24 ft. min. 6 floors max.	6 floors max.	4 floors max.
Housing Density	50 du/ac min. over 100 du/ac max.	20 du/ac min. 100 du/ac max.	80 du/ac max.	70 du/ac max.

In some cases, existing DPSP districts, such as Mixed-Use Central Business District, or Mixed-Use High Density Residential, need only an adjustment in density, as the intent of these districts is still consistent with the General Plan and transect. In other cases, such as Residential Single-Family, or Mixed-Use Light Industrial, the intent and densities of these DPSP districts are irreparably inconsistent with the General Plan and transect, and will need to be eliminated entirely. The proposed Plan streamlines the existing districts into four districts, as illustrated in Figure 4.

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Figure 4. Proposed DPSP Districts



In addition to the transect provisions of the General Plan, the Housing Element of the City of Pomona identified a portion of the existing Downtown Pomona Specific Plan as a transit-oriented district, which could accommodate additional growth in housing units. To be certain that such growth could be achieved, the Housing Element included an amendment to establish a minimum density of 40 units per acre for this area. When the transit-oriented district was first identified, it did not include the aforementioned single-family and mixed-use light industrial districts. However, the proposed Plan densities, and the elimination of low-density districts, which are consistent with the General Plan and transect, effectively increase the boundary of the transit-oriented district to be the entirety of the DPSP. Therefore, the Plan update expands upon the intent of the Housing Element update by establishing a minimum density of 40 units per acre in each of the four DPSP districts.

Expanding Arts Colony

Downtown Pomona has a long history of supporting artists and the arts. A major impetus for the establishment of the DPSP in 1994 was to formalize this support through land use permissions and design standards. Such permissions primarily focused on the core of Downtown, in the Mixed-Use Central Business District, which includes the historic Second Street Antique Row and Arts Colony. The General Plan built upon this vision by explicitly creating policies and goals to support the arts in Downtown (Figure 5). The Plan update intends to implement this vision through an expansion of the Arts Colony to include the entire DPSP, with by-right permissions for many art-related uses in all four districts, and a streamlined administrative review for more intense art-related uses, such as cultural institutions or heavy-equipment studios. Moreover, as discussed in Goal 3: Achieving Metro's TOD Goals, the Plan update establishes a new incentive for developers to provide affordable artist live/work lofts in exchange for concessions on parking requirements, and establishes a new Placemaking chapter, which calls for the implementation of the Art in Public Places Program, and the promotion of public art and the cultural arts across Downtown.

Figure 5. Selected General Plan policies related to arts in Downtown

Policy #	Policy
6E.P3	Permit a range of development types in the
	Downtown annex areas that support the creation of
	incubator space and districts of arts and innovation
6F.P22	Permit a range of development types in the eastern
	Downtown annex area that support the creation of
	incubator space, and districts of arts and innovation

Refining Design Standards

The General Plan added policies and goals to support improved standards for architectural design and site design citywide (Figure 6). The DPSP does not contain any mandates for design, instead relying on design guidelines that are optional and encouraged during plan review. To implement these General Plan design goals, the Plan update brings forward new mandatory urban form standards pertaining to building length, building articulation, street frontage, window transparency, and site pedestrian circulation. In addition, the design guidelines have been overhauled with new guidelines that encourage innovative building materials and encourages public art on blank walls. Additional policy provisions have been added that encourage the Planning Division to explore the feasibility of architectural design incentives in the future to protect historic and iconic building frontages in Downtown.

Figure 6. Selected General Plan policies related to design in Downtown

Policy #	Policy
7F.P4	Establish an appropriate relationship between public
	space and private development with building entrances
	oriented towards streets, utilizing shopfronts, porches,
	patios, or outdoor spaces that overlook or interact with
	front yards or sidewalks.
7F.P5	Promote developments that fit with the scale and
	character of their district or neighborhood by:
	-Utilizing varied massing, roof types, and floor plans.
	-Articulating building facades with distinctive
	architectural features such as windows, doors,
	chimneys and other such elements. Use articulation of
	building massing to reveal internal organization of
	building elements such as stairs and elevators, atriums,
	internal gathering spaces and major interior spaces.
7F.P6	Provide visual interest and express the human scale in
	building design with:
	-Architectural building base treatments
	-Varied building colors, materials, and site
	landscaping treatments
	-Pedestrian-scale signage and ornamental lighting

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7F.P9	Maintain an open relationship between buildings and
	street edge, avoiding fencing and significant landscape
	barriers, except for street trees and sidewalk plantings.
	Along major collectors and corridors, allow fencing,
	low walls, and/or landscaping that maintains visibility
	and visual interaction between residences and the
	street edge. Limit materials to wood, stone, decorative
	metal, or low hedges.

Revisiting Land Use Permissions

The General Plan envisions Downtown as a destination that supports a mix of uses, including retail, residential, artist studios, live/work lofts, and educational institutions and offices (Figure 7). In addition, proximity to the Downtown Pomona Metrolink Station encourages walkable neighborhoods which require street activation and active storefronts. This intent is implemented in the revised land use permissions in the Plan update. Land uses are classified into one of four permission types: by-right ("P"), by-right to the rear of tenant spaces or on second or higher floors ("P2"), by minor conditional use permit ("M"), and by conditional use permit ("C"). Use permissions were analyzed against their potential to activate Downtown with pedestrian activity and to encourage arts, entertainment, restaurants, and retail. Indoor sports and recreation spaces are permitted by-right, and cultural and arts related institutions and spaces can be processed as a minor conditional use permit. Such changes are intended to streamline the development process and encourage investment in Downtown.

Figure 7. Selected General Plan policies related to land uses in Downtown

Policy #	Policy
+	Make Downtown the centerpiece of the economic
	development strategy. Focus on increasing housing and
	employment opportunities to generate activity increases
	throughout the Downtown area.
7B.P25	Develop a strong concentration of mixed use activities
	Downtown as a major economic draw for residents and
	visitors.

GOAL #3: ACHIEVING METRO LOS ANGELES TOD GOALS

The Plan update is funded entirely through grant funding by Metro Los Angeles. The intent of the grant award is to encourage transit-oriented developments across Los Angeles County. To facilitate grantees with this process, Metro Los Angeles established a *Transit Supportive Toolkit*, which identifies various ways in which local jurisdictions can enact Specific Plan tools to build transit-oriented developments. The following are examples of such policies.

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Placemaking

Borne out of both conversations with Pomona stakeholders and analysis of Metro Los Angeles transit policies, the new Placemaking chapter is intended to offer the community an open door into exploring ways to activate public spaces in Downtown. This includes policies for historic preservation and art in public places; pathways to activate public property and public right-of-way with features such as parklets, temporary pedestrian plazas, street furniture, active alleys, and popup music; tree palettes customized to Downtown; wayfinding considerations; improving intersections and crossings; and standards for privately owned public open space. The chapter encourages the City to pursue additional documents to guide placemaking in Downtown, including a Complete Streets manual and adopting a placemaking permit fee and application to facilitate creative uses of the public realm.

More Permissive Parking Standards

The DPSP, currently defers to the Pomona Zoning Ordinance for parking standards. These standards are intended for zoning districts of far less intensity and density, and do not consider adjacency to the Downtown Pomona Metrolink Station. Moreover, Downtown is served by the Vehicle Parking District, which oversees City-owned parking lots. New parking provisions have been added to the Plan update that are in line with transit-oriented development, and provide a more permissive structure that does not overly rely on parking for land uses, and instead considers the ability for multiple modes of transportation to serve these uses, such as light rail, passenger bus, ride share, bicycling, and walking. Moreover, a new provision has been added to grandfather parking requirements for existing buildings up to two stories in the Mixed-Use Central Business District and Mixed-Use Institutional, and waive such standards up to two stories for new development in these districts. The equivalent parking demand generated by such a waiver can be accommodated by the multiple existing City-owned lots managed by the Vehicle Parking District. Parking relief can also be achieved through an applicant-provided parking demand study, certified by a licensed traffic engineer, subject to review and approval of the Development Services Director.

Incentives for Affordable Artist Housing and Public Open Space

As mentioned in **Goal 2: Implementing the General Plan**, the Plan update envisions the entire DPSP boundary to function as the Arts Colony, and to provide for land uses and policies that encourage the cultural arts and artist spaces. In addition, a new incentive has been added to encourage new developers to provide affordable artist housing in Downtown. A developer that provides at least 25% of their total proposed units as live/work units, meets Pomona Municipal Code live/work requirements, and is rented at fair market value, may receive a 50% reduction in total parking requirements, and 25% reduction in common open space requirements. The incentive is intended to increase the supply of affordable work/live units across all four districts in an effort to address affordability and viability of artist uses in Downtown.

A second incentive has also been added to address public open space in Downtown. All new development is subject to common open space requirements, with 100 square feet of common open space required for every new residential unit proposed. An applicant that provides such open space

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as "privately owned public open space," meeting the standards outlined in the Placemaking chapter, may receive a 75% reduction in total common open space requirements. For example, a proposed 100-unit development requires 10,000 square feet of common open space. Utilizing this incentive would bring this requirement down to 2,500 square feet, but would be publicly accessible and function as a community asset that is privately maintained. This incentive is intended to increase the availability of publicly accessible open space such as pocket parks and public plazas in Downtown.

GOAL #4: STREAMLINING THE DPSP AS A DOCUMENT AND DEVELOPMENT PROCESS

A key component of the Plan update is to ensure that the document is easy to use, accessible, and clear for all stakeholders, including the community, developers, business owners, and City staff. To streamline the provisions of the DPSP, the document has been divided into four chapters: *Introduction, Private Realm, Public Realm, and Implementation*. All requirements that affect private property owners in Downtown are found in the Private Realm chapter, including applicability for new development or significant additions ("Urban Form") and land use permissions ("Land Use"). All requirements that concern public property or the public right-of-way are contained in the Public Realm chapter, including mobility, placemaking, and infrastructure and utilities.

Another key component of the Plan update is to streamline the development process and establish application processes that are tailored to the type of project proposed. To this end, a new "Development Plan Review" process has been established for all parcels within the DPSP. A Development Plan Review is an application that requires a public hearing at the Development Services Director level, and is currently used in the PCSP. Such a review would only apply to any new construction, significant additions, or major façade changes. All proposed projects that solely concern land use, such as the siting of a new land use or expansion of land uses, would not go through a Development Plan Review, and instead remain governed by either a minor conditional use permit or conditional use permit that requires a public hearing at the Planning Commission.

COMMUNITY ENGAGEMENT

Pre-Draft Engagement

RRM Design initiated community outreach prior to providing the first draft of the Plan update. This included key stakeholders meetings conducted in January 2017 with Downtown business owners, educational institutions such as Western University, and land owners. In March 2017, a community workshop was conducted to solicit input on opportunities and constraints that could be addressed in the new plan. Key themes that emerged from this engagement include:

- Economic revitalization and business friendly approaches
- Access, mobility and connectivity
- Cleaner, safer and more welcoming environment
- Address undesirable uses and homelessness
- Community character and beautification

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- Mix of commercial, residential and institutional uses
- Importance of arts and education
- Family-orientation and need to accommodate all ages
- Protect historic resources
- Streamlined processes and clear policies

Draft Review and Revisions

In February and March 2019, the Planning Division conducted a series of presentations with the Planning Commission, Historic Preservation Commission, and Cultural Arts Commission to provide an update on the Plan and next steps. Staff also met with Building and Safety, Public Works, and other city departments to ensure the viability and accuracy of Plan update policies and standards. Staff also presented the same update to the Downtown Pomona Owner's Association.

The Planning Division circulated a public draft of the Plan update in May 2019. The draft was made available on the City of Pomona website, promoted on social media, and available at the Planning counter for review. As part of this public review period, Staff conducted an open house in Council Chambers to engage the community on the document, and subsequently discussed the draft with the Planning Commission and Historic Preservation Commission at its regularly scheduled meetings, and again with the Downtown Pomona Owner's Association.

PLANNING COMMISSION RECOMMENDATION

A Draft was presented at a public hearing at the June 26, 2019 Planning Commission meeting (Attachment 5). The Draft incorporated the comments and feedback received during the community engagement period in May 2019. The Planning Commission recommended approval of the Draft including all proposed changes with the exception of a recommended change to land use permission for on-sale alcohol.

The Draft proposes establishing all on-sale alcohol permits within the boundary of the Downtown Pomona Specific Plan as a Minor Conditional Use Permit, which does not require a public hearing and is a Development Services Director approval, instead of the current land use permission of a Conditional Use Permit, which requires a public hearing and Planning Commission approval. Offsale alcohol would remain a Conditional Use Permit.

The Planning Commission made the following two points against the recommended approval of on-sale alcohol permits via a Minor Conditional Use Permit:

- 1. Statistics were cited indicating a high incidence of substance abuse in the City.
- 2. The recent record of Planning Commission votes being split on on-sale alcohol applications was discussed as indicative of sufficient push back from the community that on-sale alcohol Minor Conditional Use permits in the DPSP would end up being appealed to the Planning Commission and render the streamlining of such permits moot.

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The Draft prepared for Council introduction includes an Errata Sheet (Attachment 2) that identifies recommendations and edits made to the Draft by the Planning Commission. If the City Council chooses to not amend the Draft with regard to the land use permission for on-sale alcohol as a Minor CUP, it would be appropriate by motion to amend the Errata Sheet to remove the last row on the table referencing page 55.

CONCLUSION

The comprehensive update to the Downtown Pomona Specific Plan will reconcile boundaries with the Pomona Corridors Specific Plan, implement the goals and policies of the 2014 Pomona General Plan, achieve the transit-oriented development goals set forth by Metro Los Angeles, and streamline the implementation of the Plan as a document and development process.

Prepared By:

Ata Khan Senior Planner

ATTACHMENTS

- 1. Draft Ordinance with Exhibits A-C
- 2. Draft Downtown Pomona Specific Plan Update & Errata Sheet
- 3. Proof of Publication of Public Hearing Notice
- 4. Environmental Findings of Consistency
- 5. Staff Report & Attachments of 6/26/19 Planning Commission Public Hearing

ORDINANCE 4267

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF POMONA, CALIFORNIA APPROVING A COMPREHENSIVE UPDATE TO THE DOWNTOWN POMONA SPECIFIC PLAN (SPA 6409-2016) AND RELATED UPDATES TO THE POMONA CORRIDORS SPECIFIC PLAN (SPA 12106-2019) AND POMONA ZONING ORDINANCE (ZONE 12104-2019).

WHEREAS, the City of Pomona was awarded a grant in 2016 in the amount of \$220,000 from the Los Angeles County Metropolitan Transportation Authority ("Metro Los Angeles") to create a regulatory environment supportive of transit-oriented development around station areas and adjacent transit corridors in Downtown Pomona;

WHEREAS, the City of Pomona subsequently initiated an application for a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) to fulfill the obligations of the Metro Los Angeles Grant;

WHEREAS, the City of Pomona selected RRM Design ("Consultant") to complete the comprehensive update to the Downtown Pomona Specific Plan;

WHEREAS, the Consultant conducted outreach between 2017 and 2019 with the City of Pomona to understand opportunities and constraints in Downtown Pomona;

WHEREAS, the City of Pomona subsequently initiated an application for related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) to reconcile boundaries with the Downtown Pomona Specific Plan and clarify land use permissions across various zoning documents;

WHEREAS, the proposed boundaries of the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) are generally bounded by Center Street to the North, Towne Avenue to the East, Fourth Street to the South, and White Avenue to the West, as defined in Exhibit A;

WHEREAS, the changes in boundaries and land use classifications to the Pomona Corridors Specific Plan (SPA 12106-2019) related to the comprehensive update of the Downtown Pomona Specific Plan are as defined in Exhibit B1 and B2;

WHERAS, the proposed change of zone from the Downtown Pomona Specific Plan to the Open Space Zoning District of the Pomona Zoning Ordinance (ZONE 12104-2019) related to the comprehensive update of the Downtown Pomona Specific Plan is as defined in Exhibit C;

WHEREAS, the Planning Commission of the City of Pomona has, after giving notice thereof as required by law, held a public hearing on June 26, 2019, concerning a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) and

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recommended City Council approval (6-0-0-1) with changes as noted in the Errata Sheet dated July 15, 2019 provided to City Council; and

WHEREAS, the City Council of the City of Pomona has, after giving notice thereof as required by law, held a public hearing on July 15, 2019 concerning the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019);

WHEREAS, the City Council of the City of Pomona, at its regularly scheduled public meeting on July 15, 2019, approved the introduction and first reading of an Ordinance for concerning the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) including recommended changes as found in the Errata Sheet adopted City Council at the public hearing on July 15, 2019;

NOW, THEREFORE, BE IT HEREBY RESOLVED by the City Council of the City as follows:

SECTION 1. The City Council, exercising independent judgment, finds that the project will not have an adverse impact on the environment. According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified," except as might be necessary to determine whether there are project-specific significant effects. A Findings of Consistency was prepared to evaluate the proposed project's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether it would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Environmental impacts associated with the proposed project would be similar to those anticipated for the project area in the 2014 General Plan Final EIR and would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required under CEQA.

SECTION 2. Based on consideration of the whole record before it, including but not limited to, the staff report, public testimony received at the public hearing on this matter, and evidence made part of the public record, the City Council hereby finds that the proposed comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) is in the public interest and in the interest of the furtherance of the public health, safety, and welfare and is consistent with the goals, objectives, policies, and programs of the Pomona General Plan.

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SECTION 3. Based on consideration of the whole record before it, including but not limited to, the staff report, public testimony received at the public hearing on this matter, and evidence made part of the public record, The City Council hereby finds and determines that the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) meet the findings outlined in Section .580(J) of the Pomona Zoning Ordinance:

1. The proposed Specific Plan Amendment systematically implements and is consistent with the General Plan.

The comprehensive update to the Downtown Pomona Specific Plan, along with related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance are in conformance with the policies and goals outlined in the 2014 Pomona General Plan. In particular:

- 6E.P3 Permit a range of development types in the Downtown annex areas that support the creation of incubator space and districts of arts and innovation
- 6F.P22Permit a range of development types in the eastern Downtown annex area that support the creation of incubator space, and districts of arts and innovation

The comprehensive update to the Downtown Pomona Specific Plan expands the notion of the Arts Colony to the entirety of Downtown, and brings in more permissive land use classifications that encourage arts and innovation.

- 7F.P4 Establish an appropriate relationship between public space and private development with building entrances oriented towards streets, utilizing shopfronts, porches, patios, or outdoor spaces that overlook or interact with front yards or sidewalks.
- 7F.P5 Promote developments that fit with the scale and character of their district or neighborhood by:

 ${\it Utilizing \ varied \ massing, \ roof \ types, \ and \ floor \ plans.}$

Articulating building facades with distinctive architectural features such as windows, doors, chimneys and other such elements. Use articulation of building massing to reveal internal organization of building elements such as stairs and elevators, atriums, internal gathering spaces and major interior spaces.

• 7F.P6 Provide visual interest and express the human scale in building design with: Architectural building base treatments

Varied building colors, materials, and site landscaping treatments

Ordinance No. 4267 Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-2016, SPA 12106-2019, ZONE 12104-2019 Page 4 of 6

Pedestrian-scale signage and ornamental lighting

• 7F.P9 Maintain an open relationship between buildings and street edge, avoiding fencing and significant landscape barriers, except for street trees and sidewalk plantings. Along major collectors and corridors, allow fencing, low walls, and/or landscaping that maintains visibility and visual interaction between residences and the street edge. Limit materials to wood, stone, decorative metal, or low hedges.

The comprehensive update to the Downtown Pomona Specific Plan establishes new standards for urban form across Downtown, including building length, window transparency, interior pedestrian circulation, and building articulation.

7B.P3 Make Downtown the centerpiece of the economic development strategy. Focus on increasing housing and employment opportunities to generate activity increases throughout the Downtown area.

7B.P25Develop a strong concentration of mixed use activities Downtown as a major economic draw for residents and visitors.

The comprehensive update to the Downtown Pomona Specific Plan and related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance bring forward new land use classifications that are intended to generate economic development and pedestrian activity and street engagement in Downtown. Moreover, all four proposed Downtown districts are intended to support a concentrated mixed use of activities and represent the central hub of activity in the City of Pomona.

2. The proposed Specific Plan Amendment will not be detrimental to the public interest, health, safety, convenience, or welfare of the City.

The comprehensive update to the Downtown Pomona Specific Plan will not be detrimental to the public interest, health, safety, convenience, or welfare of the City, as it is implementing goals and policies previously identified in the 2014 Pomona General Plan, is intended to provide a mix of residential and commercial opportunities in the Downtown area that is subject to land use and urban form standards and guidelines, has been zoned to concentrate densities specifically in the Downtown and to be mindful of density transitions to low density neighborhoods immediately adjacent, and will generate activity and opportunity that is in the interest of all residents in the City of Pomona.

3. The subject property is physically suitable for the requested land use designations and the anticipated land use developments.

Ordinance No. 4267 Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-2016, SPA 12106-2019, ZONE 12104-2019 Page 5 of 6

The comprehensive update to the Downtown Pomona Specific Plan and related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance has been reviewed by the Public Works Department for infrastructure, utilities, and mobility capacity. It has been determined that the proposed plans are suitable for the type of density and land use designations anticipated in Downtown Pomona and in the Pomona corridors.

4. The Specific Plan Amendment provides for the development of a comprehensively planned project that is superior to development otherwise allowed under conventional zoning classifications.

The comprehensive update to the Downtown Pomona Specific Plan is bringing forward a set of tools related to density, land use classifications, urban form, privately owned public open space, work/live units, placemaking, temporary use permits, parking standards, and development review processes that could not be achieved under conventional zoning classifications under the Pomona Zoning Ordinance.

5. The proposed Specific Plan Amendment will contribute to a balance of land uses so that local residents may work and shop in the community in which they live.

The comprehensive update to the Downtown Pomona Specific Plan will provide a healthy mix of land uses that include multi-unit residential, commercial retail and professional office space, cultural uses, public assembly, institutional uses, and other entertainment and recreation that will encourage Pomona residents to thrive in the community in which they live.

SECTION 4. Based upon the above findings, the City Council approves the following actions:

- 1. Adopt a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016), with boundaries and parcels as defined in Exhibit A;
- 2. Adopt an amendment to the Pomona Corridors Specific Plan (SPA 12106-2019), with parcels to be added or removed as defined in Exhibit B1, and with updated land use categories as defined in Exhibit B2; and
- 3. Adopt a change of zone (ZONE 12104-2019) for parcels to be changed from Downtown Pomona Specific Plan designations to the Open Space Zoning District of the Pomona Zoning Ordinance as defined in Exhibit C.

SECTION 5. The City Clerk shall attest and certify to the passage and adoption of this Ordinance and it shall become effective thirty (30) days after its adoption.

Ordinance No. 4267 Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-2016, SPA 12106-2019, ZONE 12104-2019 Page 6 of 6

APPROVED AND ADOPTED THIS 15TH DAY OF JULY, 2019.

	ATTEST: CITY OF POMONA:
Rosalia A. Butler, MMC, City Clerk	Tim Sandoval, Mayor
APPROVED AS TO FORM:	
Christi Hogin, Interim City Attorney	
STATE OF CALIFORNIA) COUNTY OF LOS ANGELES) ss. CITY OF POMONA)	
foregoing Ordinance was introduced for	Clerk of the City of Pomona do hereby certify that the first reading on July 15, 2019, and adopted at a regular of Pomona held on [Date of Second Reading] by the
AYES: NOES: ABSENT: ABSTAIN:	
	Rosalia A. Butler, MMC, City Clerk

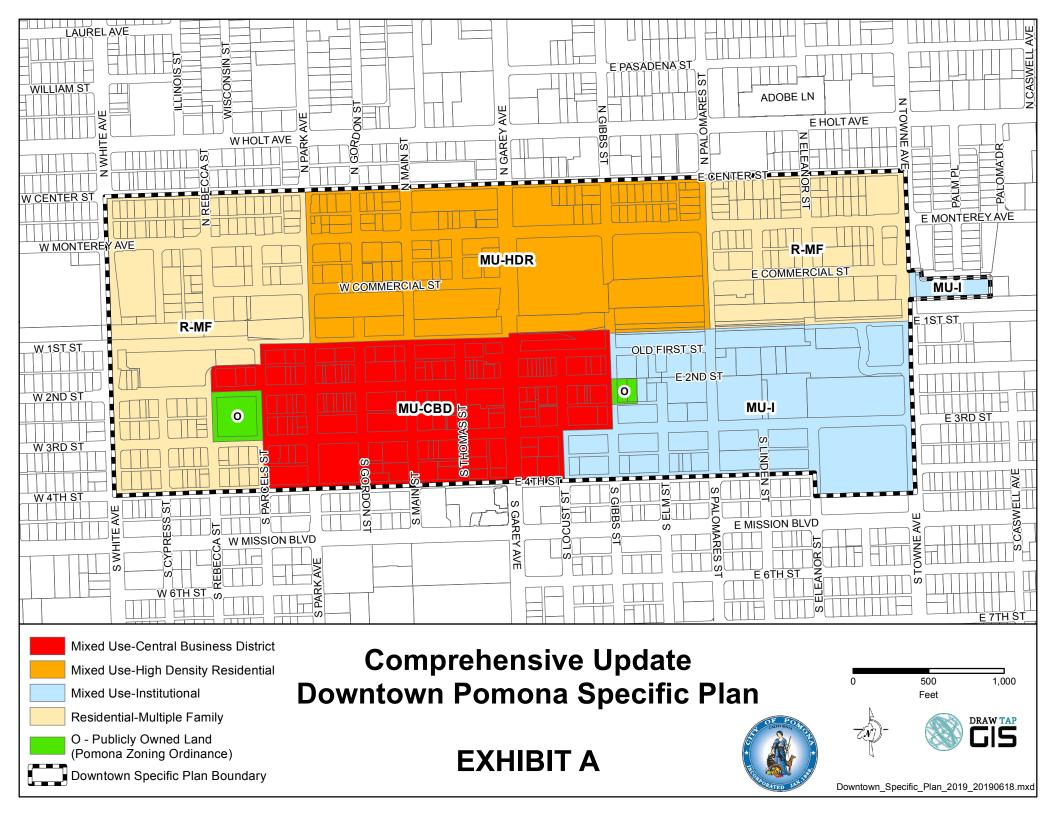


Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8335010013 CSP DownTown Core	DT-SP MU-CBD
8335010906 CSP DownTown Core	DT-SP MU-CBD
8335009005 CSP DownTown Core	DT-SP MU-CBD
8335009009 CSP DownTown Core	DT-SP MU-CBD
8335010014 CSP DownTown Core	DT-SP MU-CBD
8335009904 CSP DownTown Core	DT-SP MU-CBD
8335010012 CSP DownTown Core	DT-SP MU-CBD
8335010907 CSP DownTown Core	DT-SP MU-CBD
8335010905 CSP DownTown Core	DT-SP MU-CBD
8335009017 CSP DownTown Core	DT-SP MU-CBD
8335011007 CSP DownTown Core	DT-SP MU-CBD
8335009002 CSP DownTown Core	DT-SP MU-CBD
8335010010 CSP DownTown Core	DT-SP MU-CBD
8335010004 CSP DownTown Core	DT-SP MU-CBD
8335009011 CSP DownTown Core	DT-SP MU-CBD
8335010016 CSP DownTown Core	DT-SP MU-CBD
8335009008 CSP DownTown Core	DT-SP MU-CBD
8335010011 CSP DownTown Core	DT-SP MU-CBD
8335011014 CSP DownTown Core	DT-SP MU-CBD
8335009014 CSP DownTown Core	DT-SP MU-CBD
8335009010 CSP DownTown Core	DT-SP MU-CBD
8335010008 CSP DownTown Core	DT-SP MU-CBD
8335009015 CSP DownTown Core	DT-SP MU-CBD
8335009018 CSP DownTown Core	DT-SP MU-CBD
8335010003 CSP DownTown Core	DT-SP MU-CBD
8335009001 CSP DownTown Core	DT-SP MU-CBD
8335010005 CSP DownTown Core	DT-SP MU-CBD
8335011013 CSP DownTown Core	DT-SP MU-CBD
8341001907 CSP DownTown Core	DT-SP MU-CBD
8341001013 CSP DownTown Core	DT-SP MU-CBD
8341004051 CSP DownTown Core	DT-SP MU-CBD
8341001009 CSP DownTown Core	DT-SP MU-CBD
8341004039 CSP DownTown Core	DT-SP MU-CBD
8341004052 CSP DownTown Core	DT-SP MU-CBD
8341001030 CSP DownTown Core	DT-SP MU-CBD
8341001011 CSP DownTown Core	DT-SP MU-CBD
8341005002 CSP DownTown Core	DT-SP MU-CBD
8341001032 CSP DownTown Core	DT-SP MU-CBD
8341004042 CSP DownTown Core	DT-SP MU-CBD
8341001031 CSP DownTown Core	DT-SP MU-CBD
8341001904 CSP DownTown Core	DT-SP MU-CBD
8341001012 CSP DownTown Core	DT-SP MU-CBD
8341001035 CSP DownTown Core	DT-SP MU-CBD
8341004022 CSP DownTown Core	DT-SP MU-CBD
8341001034 CSP DownTown Core	DT-SP MU-CBD
8341004041 CSP DownTown Core	DT-SP MU-CBD

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

8341005023 CSP - DownTown Core DT-SP - MU-CBD 8341004018 CSP - DownTown Core DT-SP - MU-CBD 8341004054 CSP - DownTown Core DT-SP - MU-CBD 8336026900 CSP - Height Overlay - DT-SP - MU-HDR 8336025012 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336032004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336032002 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025003 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336027024 DT-SP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336027028 DT-SP - MU-AR DT-SP - MU-HDR 8336027013 DT-SP - MU-AR DT-SP - MU-HDR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR 8336027006 DT-SP - MU-AR DT-SP - MU-HDR 8336027006 DT-SP - MU-AR DT-SP - MU-HDR
8341004054 CSP - DownTown Core 8336026900 CSP - Height Overlay - 8336025012 CSP - Height Overlay - DownTown Gateway Segment 8336032004 CSP - Height Overlay - DownTown Gateway Segment 8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336027004 DT-SP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR DT-SP - MU-HDR
8336026900 CSP - Height Overlay - 8336025012 CSP - Height Overlay - DownTown Gateway Segment 8336032004 CSP - Height Overlay - DownTown Gateway Segment 8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-HDR 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR DT-SP - MU-HDR
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8336032004 CSP - Height Overlay - DownTown Gateway Segment 8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336027017 CSP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR
8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment B336032001 CSP - Height Overlay - DownTown Gateway Segment B336025004 CSP - Height Overlay - DownTown Gateway Segment B336025004 CSP - Height Overlay - DownTown Gateway Segment B336026017 CSP - Height Overlay - DownTown Gateway Segment B336027024 DT-SP - MU-AR B336027024 DT-SP - MU-AR B336027028 DT-SP - MU-AR B336027013 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B7-SP - MU-HDR B7-SP - MU-HDR B7-SP - MU-HDR B7-SP - MU-HDR
8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336026017 CSP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR 8736027005 DT-SP - MU-AR DT-SP - MU-HDR
8336032001 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
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8337027001 DT-SP MU-AR DT-SP R-MF
8337027003 DT-SP MU-AR DT-SP R-MF
8337027010 DT-SP MU-AR DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
	9 DT-SP MU-AR	DT-SP R-MF
833702700	4 DT-SP MU-AR	DT-SP R-MF
833702701	3 DT-SP MU-AR	DT-SP R-MF
833702701	6 DT-SP MU-AR	DT-SP R-MF
833702701	1 DT-SP MU-AR	DT-SP R-MF
833702700	6 DT-SP MU-AR	DT-SP R-MF
	9 DT-SP MU-AR	DT-SP R-MF
	5 DT-SP MU-AR	DT-SP R-MF
833702701	8 DT-SP MU-AR	DT-SP R-MF
	1 DT-SP MU-AR	DT-SP R-MF
	5 DT-SP MU-AR	DT-SP R-MF
	6 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	3 DT-SP MU-CBD	DT-SP MU-CBD
	0 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
	2 DT-SP MU-CBD	DT-SP MU-CBD
	6 DT-SP MU-CBD	DT-SP MU-CBD
	9 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
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	5 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	1 DT-SP MU-CBD	DT-SP MU-CBD
	7 DT-SP MU-CBD	DT-SP MU-CBD
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Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8341005015 DT-SP MU-CBD	DT-SP MU-CBD
8341006025 DT-SP MU-CBD	DT-SP MU-CBD
8341003010 DT-SP MU-CBD	DT-SP MU-CBD
8341002021 DT-SP MU-CBD	DT-SP MU-CBD
8341006005 DT-SP MU-CBD	DT-SP MU-CBD
8341001036 DT-SP MU-CBD	DT-SP MU-CBD
8341006022 DT-SP MU-CBD	DT-SP MU-CBD
8341001901 DT-SP MU-CBD	DT-SP MU-CBD
8341001902 DT-SP MU-CBD	DT-SP MU-CBD
8341002006 DT-SP MU-CBD	DT-SP MU-CBD
8341003006 DT-SP MU-CBD	DT-SP MU-CBD
8341002024 DT-SP MU-CBD	DT-SP MU-CBD
8341002007 DT-SP MU-CBD	DT-SP MU-CBD
8341003001 DT-SP MU-CBD	DT-SP MU-CBD
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8341002016 DT-SP MU-CBD	DT-SP MU-CBD
8341001900 DT-SP MU-CBD	DT-SP MU-CBD
8341002013 DT-SP MU-CBD 8341001039 DT-SP MU-CBD	DT-SP MU-CBD DT-SP MU-CBD
8341002022 DT-SP MU-CBD	DT-SP MU-CBD
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8341003004 DT-SP MU-CBD	DT-SP MU-CBD
8341004044 DT-SP MU-CBD	DT-SP MU-CBD
8341003011 DT-SP MU-CBD	DT-SP MU-CBD
8341005022 DT-SP MU-CBD	DT-SP MU-CBD
8341006024 DT-SP MU-CBD	DT-SP MU-CBD
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8341003904 DT-SP MU-CBD	DT-SP MU-CBD
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8341003903 DT-SP MU-CBD	DT-SP MU-CBD
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8341003907 DT-SP MU-CBD	DT-SP MU-CBD
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8341005904 DT-SP MU-CBD	DT-SP MU-CBD
8341005018 DT-SP MU-CBD	DT-SP MU-CBD
8336033012 DT-SP MU-HDR	DT-SP MU-HDR

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
833603480	2 DT-SP MU-HDR	DT-SP MU-HDR
833602401	2 DT-SP MU-HDR	DT-SP MU-HDR
833603201	4 DT-SP MU-HDR	DT-SP MU-HDR
833603201	0 DT-SP MU-HDR	DT-SP MU-HDR
833603301	3 DT-SP MU-HDR	DT-SP MU-HDR
833602400	7 DT-SP MU-HDR	DT-SP MU-HDR
833602401	9 DT-SP MU-HDR	DT-SP MU-HDR
833603301	8 DT-SP MU-HDR	DT-SP MU-HDR
833602400	5 DT-SP MU-HDR	DT-SP MU-HDR
833602402	3 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
833602400	4 DT-SP MU-HDR	DT-SP MU-HDR
833602401	7 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	9 DT-SP MU-HDR	DT-SP - MU-HDR
	9 DT-SP MU-HDR 8 DT-SP MU-HDR	DT-SP MU-HDR DT-SP MU-HDR
	8 DT-SP MU-HDR 1 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	4 DT-SP MU-HDR	DT-SP MU-HDR
00000	9 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	9 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	7 DT-SP MU-HDR	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8340029004	DT-SP MU-HDR	DT-SP R-MF
8340029001	DT-SP MU-HDR	DT-SP R-MF
8340029002	2 DT-SP MU-HDR	DT-SP R-MF
8340029024	I DT-SP MU-HDR	DT-SP R-MF
8340029023	B DT-SP MU-HDR	DT-SP R-MF
8340029017	DT-SP MU-HDR	DT-SP R-MF
8340036008	B DT-SP MU-HDR	DT-SP R-MF
8340036005	5 DT-SP MU-HDR	DT-SP R-MF
8340029006	5 DT-SP MU-HDR	DT-SP R-MF
8340036801	DT-SP MU-HDR	DT-SP R-MF
8340035003	B DT-SP MU-HDR	DT-SP R-MF
8340036002	PDT-SP MU-HDR	DT-SP R-MF
8340036802	2 DT-SP MU-HDR	DT-SP R-MF
8340036006	5 DT-SP MU-HDR	DT-SP R-MF
8340029008	B DT-SP MU-HDR	DT-SP R-MF
) DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	7 DT-SP MU-HDR	DT-SP R-MF
	I DT-SP MU-HDR	DT-SP R-MF
	2 DT-SP MU-HDR	DT-SP R-MF
	DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	DT-SP MU-HDR	DT-SP R-MF
	B DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	DT-SP MU-HDR BDT-SP MU-HDR	DT-SP MU-HDR DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	DT-SP MU-HDR	DT-SP MU-HDR
	B DT-SP MU-HDR	DT-SP MU-HDR
8336033029	DT-SP MU-HDR	DT-SP MU-HDR
8335013903	B DT-SP MU-I	DT-SP MU-I
8335013907	7 DT-SP MU-I	DT-SP MU-I
8335013800	DT-SP MU-I	DT-SP MU-I
8335013801	DT-SP MU-I	DT-SP MU-I
8335014918	B DT-SP MU-I	DT-SP MU-I
8335013904	I DT-SP MU-I	DT-SP MU-I
8335011800	DT-SP MU-I	DT-SP MU-I
	DT-SP MU-I	DT-SP MU-I
8335013906	5 DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	2 DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
8335013900) DT-SP MU-I	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8335013908	B DT-SP MU-I	DT-SP MU-I
8341002803	B DT-SP MU-I	DT-SP R-MF
8341002019	DT-SP MU-I	DT-SP MU-CBD
8342006010	DT-SP MU-I	DT-SP R-MF
8341002025	5 DT-SP MU-I	DT-SP MU-CBD
8342006005	5 DT-SP MU-I	DT-SP R-MF
8342006021	L DT-SP MU-I	DT-SP R-MF
8342006011	DT-SP MU-I	DT-SP R-MF
8342006004	I DT-SP MU-I	DT-SP R-MF
	5 DT-SP MU-I	DT-SP R-MF
8342006002	PDT-SP MU-I	DT-SP R-MF
8342001024	I DT-SP MU-I	DT-SP R-MF
8342001023	B DT-SP MU-I	DT-SP R-MF
	I DT-SP MU-I	DT-SP R-MF
	I DT-SP MU-I	DT-SP R-MF
8342006001	DT-SP MU-I	DT-SP R-MF
	5 DT-SP MU-I	DT-SP R-MF
	I DT-SP MU-I	DT-SP R-MF
) DT-SP MU-I	DT-SP MU-CBD
	B DT-SP MU-I	DT-SP R-MF
	I DT-SP MU-I	DT-SP MU-I
	B DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	L DT-SP MU-I	DT-SP MU-I
	7 DT-SP MU-I	DT-SP MU-I
	B DT-SP MU-I D DT-SP MU-I	DT-SP MU-I DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	B DT-SP MU-I	DT-SP MU-HDR
	5 DT-SP MU-I	DT-SP MU-HDR
	L DT-SP MU-I	DT-SP MU-HDR
	L DT-SP MU-I	DT-SP MU-HDR
	2 DT-SP MU-I	DT-SP MU-HDR
	I DT-SP MU-I	DT-SP MU-HDR
	5 DT-SP MU-I	DT-SP MU-HDR
	5 DT-SP MU-I	DT-SP MU-HDR
	DT-SP MU-I	DT-SP MU-I
8335007017	7 DT-SP MU-I	DT-SP MU-I
	B DT-SP MU-I	DT-SP MU-I
8335007014	I DT-SP MU-I	DT-SP MU-I
8335004054	I DT-SP MU-I	DT-SP MU-I
8335004067	DT-SP MU-I	DT-SP MU-I
8335004066	5 DT-SP MU-I	DT-SP MU-I
8335005044	I DT-SP MU-I	DT-SP MU-I
8335005050	DT-SP MU-I	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8335005049 DT-SP MU-I	DT-SP MU-I
8335005048 DT-SP MU-I	DT-SP MU-I
8335005038 DT-SP MU-I	DT-SP MU-I
8335005046 DT-SP MU-I	DT-SP MU-I
8335005047 DT-SP MU-I	DT-SP MU-I
8335005903 DT-SP MU-I	DT-SP MU-I
8335014044 DT-SP MU-I	DT-SP MU-I
8335014045 DT-SP MU-I	DT-SP MU-I
8335005045 DT-SP MU-I	DT-SP MU-I
8337024009 DT-SP MU-L	DT-SP R-MF
8337024006 DT-SP MU-L	DT-SP R-MF
8337024007 DT-SP MU-L	DT-SP R-MF
8337024016 DT-SP MU-L	DT-SP R-MF
8337029002 DT-SP MU-L	DT-SP R-MF
8337024010 DT-SP MU-L	DT-SP R-MF
8337028012 DT-SP MU-L	DT-SP R-MF
8337028013 DT-SP MU-L	DT-SP R-MF
8337028009 DT-SP MU-L	DT-SP R-MF
8337028007 DT-SP MU-L	DT-SP R-MF
8337023030 DT-SP MU-L	DT-SP R-MF
8337024020 DT-SP MU-L	DT-SP R-MF
8337024003 DT-SP MU-L	DT-SP R-MF
8337024012 DT-SP MU-L	DT-SP R-MF
8337023012 DT-SP MU-L	DT-SP R-MF
8337028010 DT-SP MU-L	DT-SP R-MF
8337028015 DT-SP MU-L	DT-SP R-MF
8337023013 DT-SP MU-L	DT-SP R-MF
8337024011 DT-SP MU-L	DT-SP R-MF
8337028011 DT-SP MU-L	DT-SP R-MF
8337028017 DT-SP MU-L	DT-SP R-MF
8337023019 DT-SP MU-L	DT-SP R-MF
8337029003 DT-SP MU-L	DT-SP R-MF
8337024015 DT-SP MU-L	DT-SP R-MF
8337028003 DT-SP MU-L	DT-SP R-MF
8337023010 DT-SP MU-L	DT-SP R-MF
8337028008 DT-SP MU-L	DT-SP R-MF
8337029005 DT-SP MU-L	DT-SP R-MF
8337029004 DT-SP MU-L	DT-SP R-MF
8337023031 DT-SP MU-L	DT-SP R-MF
8337028006 DT-SP MU-L	DT-SP R-MF
8337024013 DT-SP MU-L	DT-SP R-MF
8337024005 DT-SP MU-L	DT-SP R-MF
8337024014 DT-SP MU-L	DT-SP R-MF
8337028001 DT-SP MU-L	DT-SP R-MF
8337028002 DT-SP MU-L	DT-SP R-MF
8337024008 DT-SP MU-L	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

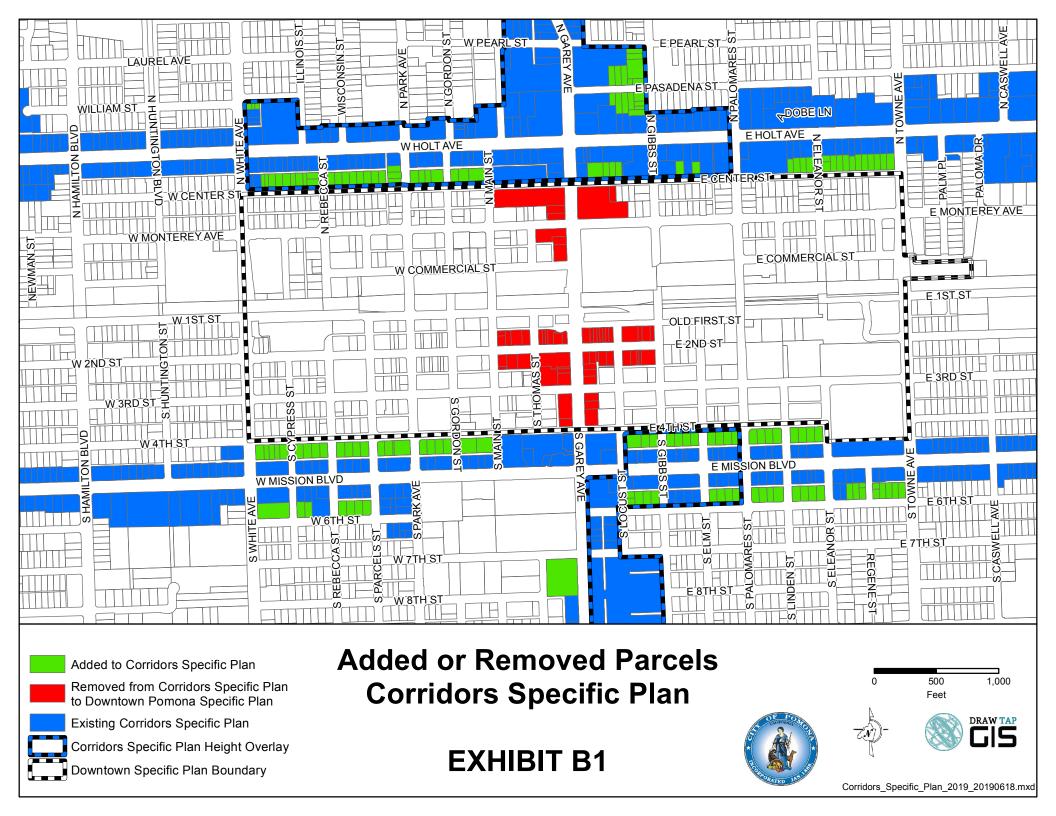
AIN	Existing Zoning	Proposed Zoning
833702301	4 DT-SP MU-L	DT-SP R-MF
833702400	2 DT-SP MU-L	DT-SP R-MF
833702302	8 DT-SP MU-L	DT-SP R-MF
833702800	4 DT-SP MU-L	DT-SP R-MF
833702801	6 DT-SP MU-L	DT-SP R-MF
833702900	1 DT-SP MU-L	DT-SP R-MF
833702400	4 DT-SP MU-L	DT-SP R-MF
833702301	1 DT-SP MU-L	DT-SP R-MF
833702800	5 DT-SP MU-L	DT-SP R-MF
833702303	2 DT-SP MU-L	DT-SP R-MF
833702302	0 DT-SP MU-L	DT-SP R-MF
833702200	5 DT-SP MU-L	DT-SP R-MF
833702200	4 DT-SP MU-L	DT-SP R-MF
833500180	1 DT-SP MU-L	DT-SP R-MF
833500190	3 DT-SP MU-L	DT-SP MU-HDR
833500180	0 DT-SP MU-L	DT-SP MU-HDR
833702301	5 DT-SP MU-L	DT-SP R-MF
833702200	2 DT-SP MU-L	DT-SP R-MF
833702302	9 DT-SP MU-L	DT-SP R-MF
833500180	9 DT-SP MU-L	DT-SP MU-HDR
833500180	2 DT-SP MU-L	DT-SP R-MF
833500190	4 DT-SP MU-L	DT-SP R-MF
833500180	7 DT-SP MU-L	DT-SP R-MF
833702200	3 DT-SP MU-L	DT-SP R-MF
833500180	8 DT-SP MU-L	DT-SP R-MF
834003400	4 DT-SP MU-L	DT-SP R-MF
834003401	7 DT-SP MU-L	DT-SP R-MF
834003400	5 DT-SP MU-L	DT-SP R-MF
	7 DT-SP MU-L	DT-SP R-MF
	0 DT-SP MU-L	DT-SP R-MF
	9 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
00.000.00	0 DT-SP MU-L	DT-SP R-MF
	2 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	5 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	3 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
	9 DT-SP MU-L	DT-SP R-MF
	8 DT-SP MU-L	DT-SP R-MF
	5 DT-SP MU-L	DT-SP MU-HDR
	4 DT-SP MU-L	DT-SP MU-HDR
833501505	5 DT-SP R-MF	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8335004052 DT-SP R-MF	DT-SP MU-I
8341006016 DT-SP R-SF	DT-SP R-MF
8341006012 DT-SP R-SF	DT-SP R-MF
8341006018 DT-SP R-SF	DT-SP R-MF
8341006019 DT-SP R-SF	DT-SP R-MF
8341006017 DT-SP R-SF	DT-SP R-MF
8341006014 DT-SP R-SF	DT-SP R-MF
8342006006 DT-SP R-SF	DT-SP R-MF
8341006013 DT-SP R-SF	DT-SP R-MF
8341006015 DT-SP R-SF	DT-SP R-MF
8342006008 DT-SP R-SF	DT-SP R-MF
8342007014 DT-SP R-SF	DT-SP R-MF
8342006007 DT-SP R-SF	DT-SP R-MF
8342007015 DT-SP R-SF	DT-SP R-MF
8342007001 DT-SP R-SF	DT-SP R-MF
8342007010 DT-SP R-SF	DT-SP R-MF
8342007016 DT-SP R-SF	DT-SP R-MF
8342006009 DT-SP R-SF	DT-SP R-MF
8341006020 DT-SP R-SF	DT-SP R-MF
8342006015 DT-SP R-SF	DT-SP R-MF
8342006017 DT-SP R-SF	DT-SP R-MF
8342007004 DT-SP R-SF	DT-SP R-MF
8342007002 DT-SP R-SF	DT-SP R-MF
8342007017 DT-SP R-SF	DT-SP R-MF
8342007003 DT-SP R-SF	DT-SP R-MF
8342007009 DT-SP R-SF	DT-SP R-MF
8342006019 DT-SP R-SF	DT-SP R-MF
8342007013 DT-SP R-SF	DT-SP R-MF
8342006018 DT-SP R-SF	DT-SP R-MF
8342007012 DT-SP R-SF	DT-SP R-MF
8342007019 DT-SP R-SF	DT-SP R-MF
8342007007 DT-SP R-SF	DT-SP R-MF
8342006016 DT-SP R-SF	DT-SP R-MF
8342007005 DT-SP R-SF	DT-SP R-MF
8342007018 DT-SP R-SF	DT-SP R-MF
8342006020 DT-SP R-SF	DT-SP R-MF
8342007006 DT-SP R-SF	DT-SP R-MF
8342007008 DT-SP R-SF	DT-SP R-MF
8342007020 DT-SP R-SF	DT-SP R-MF
8340029010 DT-SP R-SF	DT-SP R-MF
8340029013 DT-SP R-SF	DT-SP R-MF
8340030003 DT-SP R-SF	DT-SP R-MF
8340030015 DT-SP R-SF	DT-SP R-MF
8340030004 DT-SP R-SF	DT-SP R-MF
8340030009 DT-SP R-SF	DT-SP R-MF
8340030011 DT-SP R-SF	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8340029009	DT-SP R-SF	DT-SP R-MF
8340030010	DT-SP R-SF	DT-SP R-MF
8340029020	DT-SP R-SF	DT-SP R-MF
8340030006	DT-SP R-SF	DT-SP R-MF
8340030005	DT-SP R-SF	DT-SP R-MF
8340030007	DT-SP R-SF	DT-SP R-MF
8340030024	DT-SP R-SF	DT-SP R-MF
8340029025	DT-SP R-SF	DT-SP R-MF
8340030012	DT-SP R-SF	DT-SP R-MF
8340029021	DT-SP R-SF	DT-SP R-MF
8340030023	DT-SP R-SF	DT-SP R-MF
8340030002	DT-SP R-SF	DT-SP R-MF
8340029019	DT-SP R-SF	DT-SP R-MF
8340029014	DT-SP R-SF	DT-SP R-MF
8340029012	DT-SP R-SF	DT-SP R-MF
8340030021	DT-SP R-SF	DT-SP R-MF
8340030013	DT-SP R-SF	DT-SP R-MF
8340030008	DT-SP R-SF	DT-SP R-MF
8340030014	DT-SP R-SF	DT-SP R-MF
8340030020	DT-SP R-SF	DT-SP R-MF
8340030019	DT-SP R-SF	DT-SP R-MF
8340029011	DT-SP R-SF	DT-SP R-MF
8340029018	DT-SP R-SF	DT-SP R-MF
8340030022	DT-SP R-SF	DT-SP R-MF
8340030018	DT-SP R-SF	DT-SP R-MF
8337019004	M-1	DT-SP MU-I
8337021051	M-1	DT-SP MU-I



AIN	Existing Zoning	Proposed Zoning
8336025012	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336032004	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336032002	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336025003	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336032001	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336025004	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8336026017	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
8335010013	CSP DownTown Core	DT-SP MU-CBD
8335010906	CSP DownTown Core	DT-SP MU-CBD
8335009005	CSP DownTown Core	DT-SP MU-CBD
8335009009	CSP DownTown Core	DT-SP MU-CBD
8335010014	CSP DownTown Core	DT-SP MU-CBD
8335009904	CSP DownTown Core	DT-SP MU-CBD
8335010012	CSP DownTown Core	DT-SP MU-CBD
8335010907	CSP DownTown Core	DT-SP MU-CBD
8335010905	CSP DownTown Core	DT-SP MU-CBD
8335009017	CSP DownTown Core	DT-SP MU-CBD
8335011007	CSP DownTown Core	DT-SP MU-CBD
8335009002	CSP DownTown Core	DT-SP MU-CBD
8335010010	CSP DownTown Core	DT-SP MU-CBD
8335010004	CSP DownTown Core	DT-SP MU-CBD
8335009011	CSP DownTown Core	DT-SP MU-CBD
8335010016	CSP DownTown Core	DT-SP MU-CBD
8335009008	CSP DownTown Core	DT-SP MU-CBD
8335010011	CSP DownTown Core	DT-SP MU-CBD
8335011014	CSP DownTown Core	DT-SP MU-CBD
8335009014	CSP DownTown Core	DT-SP MU-CBD
8335009010	CSP DownTown Core	DT-SP MU-CBD
8335010008	CSP DownTown Core	DT-SP MU-CBD
8335009015	CSP DownTown Core	DT-SP MU-CBD
8335009018	CSP DownTown Core	DT-SP MU-CBD
8335010003	CSP DownTown Core	DT-SP MU-CBD
8335009001	CSP DownTown Core	DT-SP MU-CBD
8335010005	CSP DownTown Core	DT-SP MU-CBD
8335011013	CSP DownTown Core	DT-SP MU-CBD
8341001907	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
8341001032	CSP DownTown Core	DT-SP MU-CBD
8341004042	CSP DownTown Core	DT-SP MU-CBD

Exhibit B1: Parcels **Removed** from Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8341001031	CSP DownTown Core	DT-SP MU-CBD
8341001904	CSP DownTown Core	DT-SP MU-CBD
8341001012	CSP DownTown Core	DT-SP MU-CBD
8341001035	CSP DownTown Core	DT-SP MU-CBD
8341004022	CSP DownTown Core	DT-SP MU-CBD
8341001034	CSP DownTown Core	DT-SP MU-CBD
8341004041	CSP DownTown Core	DT-SP MU-CBD
8336026900	CSP - Height Overlay -	DT-SP MU-HDR
8341005023	CSP DownTown Core	DT-SP MU-CBD
8341004018	CSP DownTown Core	DT-SP MU-CBD
8341004054	CSP DownTown Core	DT-SP MU-CBD

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN Existing Zoning	Proposed Zoning
8337026010 DT-SP MU-AR	CSP City Gateway Segment
8337025016 DT-SP MU-AR	CSP City Gateway Segment
8337025020 DT-SP MU-AR	CSP City Gateway Segment
8337025019 DT-SP MU-AR	CSP City Gateway Segment
8337025018 DT-SP MU-AR	CSP City Gateway Segment
8337025011 DT-SP MU-AR	CSP City Gateway Segment
8337025010 DT-SP MU-AR	CSP City Gateway Segment
8337025013 DT-SP MU-AR	CSP City Gateway Segment
8337025017 DT-SP MU-AR	CSP City Gateway Segment
8337025014 DT-SP MU-AR	CSP City Gateway Segment
8337025012 DT-SP MU-AR	CSP City Gateway Segment
8337025021 DT-SP MU-AR	CSP City Gateway Segment
8337026009 DT-SP MU-AR	CSP City Gateway Segment
8337026008 DT-SP MU-AR	CSP City Gateway Segment
8337025022 DT-SP MU-AR	CSP City Gateway Segment
8337026011 DT-SP MU-AR	CSP City Gateway Segment
8335022008 DT-SP MU-AR	CSP MidTown Segment
8335021017 DT-SP MU-AR	CSP MidTown Segment
8335022016 DT-SP MU-AR	CSP MidTown Segment
8335021020 DT-SP MU-AR	CSP MidTown Segment
8335022007 DT-SP MU-AR	CSP MidTown Segment
8335021015 DT-SP MU-AR	CSP MidTown Segment
8335021055 DT-SP MU-AR	CSP MidTown Segment
8335022009 DT-SP MU-AR	CSP MidTown Segment
8335021054 DT-SP MU-AR	CSP MidTown Segment
8335021008 DT-SP MU-AR	CSP MidTown Segment
8335022017 DT-SP MU-AR	CSP MidTown Segment
8335021019 DT-SP MU-AR	CSP MidTown Segment
8335022014 DT-SP MU-AR	CSP MidTown Segment
8335021018 DT-SP MU-AR	CSP MidTown Segment
8335021053 DT-SP MU-AR	CSP MidTown Segment
8335021016 DT-SP MU-AR 8335022006 DT-SP MU-AR	CSP MidTown Segment CSP MidTown Segment
8341010041 DT-SP MU-AR	CSP MidTown Segment
8341010041 DT-SF MU-AR	CSP MidTown Segment
8341010024 DT-SP MU-AR	CSP MidTown Segment
8335022015 DT-SP MU-AR	CSP MidTown Segment
8342013015 DT-SP MU-AR	CSP MidTown Segment
8342013021 DT-SP MU-AR	CSP MidTown Segment
8341010040 DT-SP MU-AR	CSP MidTown Segment
8342013016 DT-SP MU-AR	CSP MidTown Segment
8341008016 DT-SP MU-CBD	CSP MidTown Segment
8341007020 DT-SP MU-CBD	CSP MidTown Segment
8341007900 DT-SP MU-CBD	CSP MidTown Segment
8341007027 DT-SP MU-CBD	CSP MidTown Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8341007030	DT-SP MU-CBD	CSP MidTown Segment
8341007022	DT-SP MU-CBD	CSP MidTown Segment
8341008904	DT-SP MU-CBD	CSP MidTown Segment
8341007033	DT-SP MU-CBD	CSP MidTown Segment
8341007026	DT-SP MU-CBD	CSP MidTown Segment
8335014912	DT-SP MU-I	CSP MidTown Segment
8335014910	DT-SP MU-I	CSP MidTown Segment
8335014907	DT-SP MU-I	CSP MidTown Segment
8335014913	DT-SP MU-I	CSP MidTown Segment
8335014909	DT-SP MU-I	CSP MidTown Segment
8335014915	DT-SP MU-I	CSP MidTown Segment
8335014908	DT-SP MU-I	CSP MidTown Segment
8341013030	DT-SP MU-I	CSP MidTown Segment
8335014916	DT-SP MU-I	CSP MidTown Segment
8341007002	DT-SP R-SF	CSP MidTown Segment
8341007004	DT-SP R-SF	CSP MidTown Segment
8341007003	DT-SP R-SF	CSP MidTown Segment
8342012010	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
8342012009	DT-SP R-SF	CSP MidTown Segment
8342012003	DT-SP R-SF	CSP MidTown Segment
8342012011	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
	DT-SP R-SF	CSP MidTown Segment
	DT-SP MU-AR	CSP - Height Overlay - City Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - City Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	CSP - Height Overlay -	CSP - Height Overlay - DownTown Gateway Segment
	CSP - Height Overlay -	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
		CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
0340020003	DI-DI IVIU-MN	Con Theight Overlay - Downhown Gateway Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8340028004	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8340028005	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8340028006	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8340028007	DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336017019	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016014	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017011	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016029	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017010	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016024	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017018	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016015	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016016	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8340027019	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8335024008	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
8335023007	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
8335024007	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
8335023004	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
8335023005	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
8335023006	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013809	DT-SP MU-I	CSP - Height Overlay - MidTown Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8335013019	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335012015	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013909	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013807	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013808	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013806	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335012901	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013030	DT-SP MU-I	CSP - Height Overlay - MidTown Segment

EXHIBIT B2

The following text changes shall be made to the Pomona Corridors Specific Plan as a related action to the comprehensive update to the Downtown Pomona Specific Plan:

1. Change the entirety of 2.2.1 Use Types in the Downtown Core Segment (2.1.2) to read as follows:

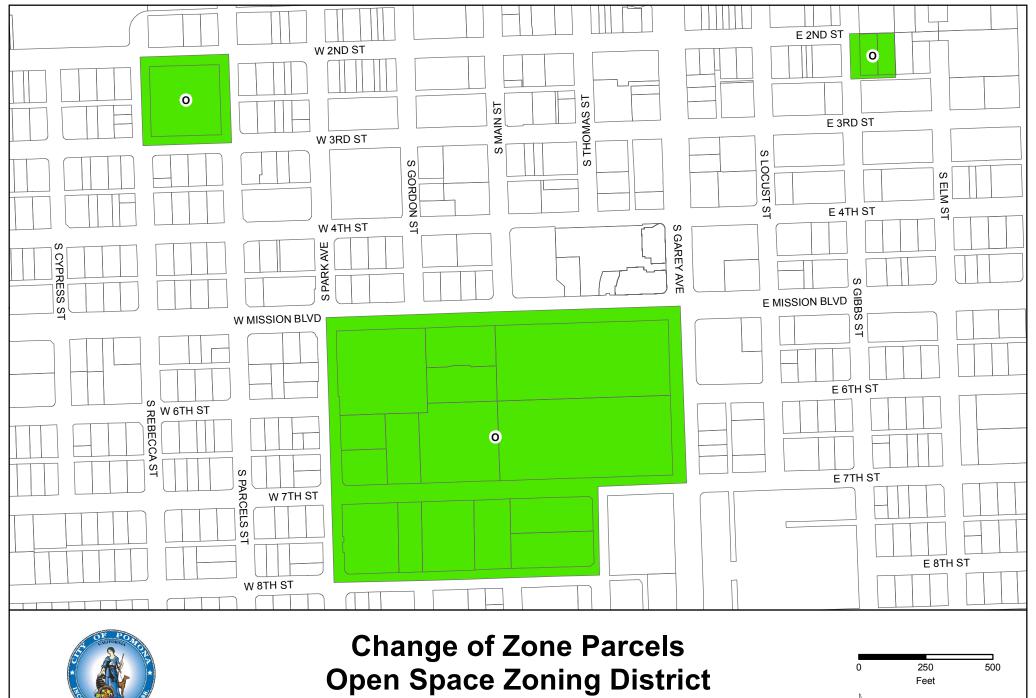
2.2.1 Use Types

A. Retail

- 1. Specialty Goods Anchors (--)
- 2. Community Oriented Anchors (NC)
 - 3. Entertainment Anchors (--)
- 4. Eating & Drinking Establishments (NC)
 - 5. Specialty Goods & Foods (NC)
- 6. Entertainment & Recreation (permitted)
 - 7. Convenience Uses (NC, CS)
 - 8. Business Services (permitted)
 - 9. Personal Services (permitted)
- 10. Service Commercial & Repair (conditional)
- 11. Large Scale Commercial Goods (conditional)
 - 12. Vehicle Sales (--)
 - B. Civic & Cultural (conditional)
 - C. Workplace
 - 1. Professional Services (permitted)
 - 2. Medical Services (permitted)
 - 3. Light Industrial (permitted)
 - D. Lodging (permitted)
 - E. Live Work (permitted)
 - F. Residential
 - 1. Multi-Family w/ Common Entry (permitted)
- 2. Multi-Family w/ Individual Entries (permitted)
 - 3. Attached Single Family (permitted)
 - 4. Detached Single Family (--)

2. Change "Community Oriented Anchors" in 2.2.1 Use Types in the Midtown Segment (2.1.11) to read as follows:

2. Community Oriented Anchors (permitted) (L2)



O - Publicly Owned Land

EXHIBIT C



Zoning_Open_Space_2019_20190613.mxd

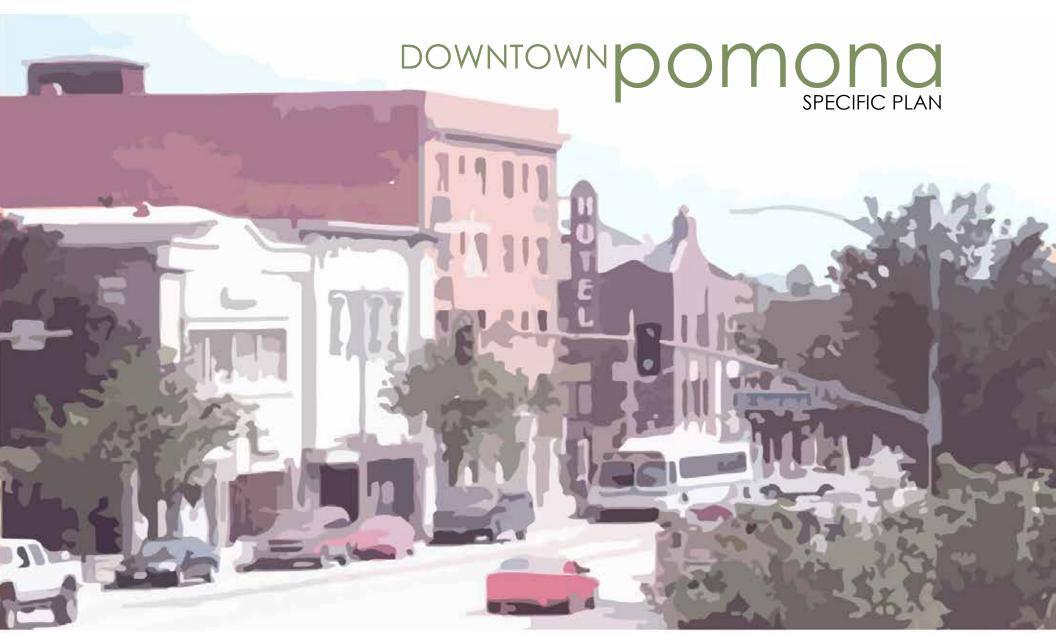
Exhibit C: Parcels Added to Open Space Zoning District in Pomona Zoning Ordinance

AIN	Existing Zoning	Proposed Zoning
8341009908	DT-SP MU-I	OS
8341011906	DT-SP MU-I	OS
8341013935	DT-SP MU-I	OS
8341014908	DT-SP MU-I	OS
8341012933	DT-SP MU-I	OS
8341011912	DT-SP MU-I	OS
8341003900	DT-SP MU-I	OS
8341013931	DT-SP MU-I	OS
8341012932	DT-SP MU-I	OS
8341014909	DT-SP MU-I	OS
8341009910	DT-SP MU-I	OS
8341013933	DT-SP MU-I	OS
8341013934	DT-SP MU-I	OS
8335007910	DT-SP MU-I	OS
8335007909	DT-SP MU-I	OS
8341011901	DT-SP MU-I	OS
8341009911	DT-SP MU-I	OS
8341010922	DT-SP MU-I	OS

ERRATA SHEET DATED JULY 15, 2019

The following table identifies recommended changes to the Draft of the Downtown Pomona Specific Plan approved by the Planning Commission at its June 26, 2019 meeting for consideration by City Council on July 15, 2019.

PAGE	CHANGE		
151	Add to Table 4-3 Implementation Action Plan Matrix:		
	"Linear Park: Develop a linear park along First Street frontage, between White and Towne Avenues." Timing: 2 Lead: Public Works, Support/Coordination: Development Services, Potential Funding Sources: Measure A, EFID		
56	Add to Minor Conditional Use Permit:		
	"The Development Services Director may escalate the review of a Minor Conditional Use Permit to the Planning Commission at his/her discretion."		
	Add to Development Plan Review:		
	"The Development Services Director may escalate the review of a Development Plan Review to the Planning Commission at his/her discretion."		
55-56	Change from		
	Offices Business Professional & Technology (P2/P2/P/-) Medical and Dental (P2/P2/P/-)		
	to		
	Offices Business Professional & Technology (P2*/P2/P/-) Medical and Dental (P2*/P2/P/-)		
	*"P" west of Park Ave.		
55	Change from		
	Alcohol (On-sale only) (M/M/M/-)		
	to		
	Alcohol (On-sale only) (C/C/C/-)		



JUNE 2019 DRAFT

ACKNOWLEDGMENTS

Mayor and City Council

Tim Sandoval, Mayor

Rubio R. Gonzalez, Council Member, District 1

Victor Preciado, Council Member, District 2

Nora Garcia, Council Member, District 3

Elizabeth Ontiveros-Cole, Council Member, District 4

Steve Lustro, Council Member, District 5

Robert S. Torres, Council Member, District 6

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DOWNTOWN POMONO SPECIFIC PLAN



PREPARED BY:



WITH SUPPORT FROM:

The Natelson Dale Group
Urban Crossroads





CHAPTER ONE	pg 1	1.1 Context
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	pg 10	1.3 Document Organization
	pg 11	1.4 Relationship to Other Planning Documents
	pg 12	1.5 Administration
	pg 14	1.6 Split Parcel
	pg 14	1.7 Substantial Conformance
CHAPTER TWO	pg 19	2.1 Introduction
PRIVATE REALM	pg 21	2.2 Urban Form
	pg 51	2.3 Land Use
	pg 61	2.4 Signage
CHAPTER THREE	pg 75	3.1 Introduction
PUBLIC REALM	pg 75	3.2 Mobility
	pg 103	3.3 Placemaking
	pg 115	3.4 Infrastructure and Utilities
CHAPTER FOUR	pg 125	4.1 Introduction
IMPLEMENTATION	pg 126	4.2 Attracting Private Investment and Providing Public Benefits
	pg 127	4.3 Economic Conditions Influencing Development Potentials in Downtown Pomons
	pg 137	4.4 Overview of Community Revitalization and Investment Authorities
	pg 141	4.5 Leveraging the City of Pomona's Opportunity Zones
	pg 144	4.6 Implementation Action Plan
appendix DEFINITIONS	pg 153	A.1 Definitions



1.1 Context

1.1.1 Purpose

The Downtown Pomona Specific Plan is a comprehensive document that implements the vision for Downtown Pomona established by the City of Pomona General Plan. The Specific Plan was originally adopted in 1994 and has had several targeted updates. In 2017, the City of Pomona began a comprehensive update to the Specific Plan, funded by a grant from the Los Angeles County Metropolitan Transportation Authority (Metro) Round 4 Transit Oriented Development (TOD) Planning Grant Funds. The purpose of the grant is to create a regulatory environment supportive of TOD around station areas and adjacent transit corridors. TOD increases accessibility and utilization of public transportation. An additional purpose of the specific plan update is to reconcile overlapping development standards, land uses, and boundary conditions with the Pomona Corridors Specific Plan and implement the Pomona General Plan update, both adopted in March 2014.



Second Street

1.1.2 Boundary

Downtown Pomona is centrally located near several freeways, including Interstate 10, State Route 60, and State Route 71. Major universities are located nearby including California Polytechnic State University, Mount San Antonio Community College, University of La Verne, the Claremont Colleges and Western University of Health Sciences. Several major regional parks and the Ontario International Airport are also located in the vicinity. The Downtown Pomona Specific Plan area is located in the heart of Pomona, as shown in **Figure 1-1, Regional Context**.

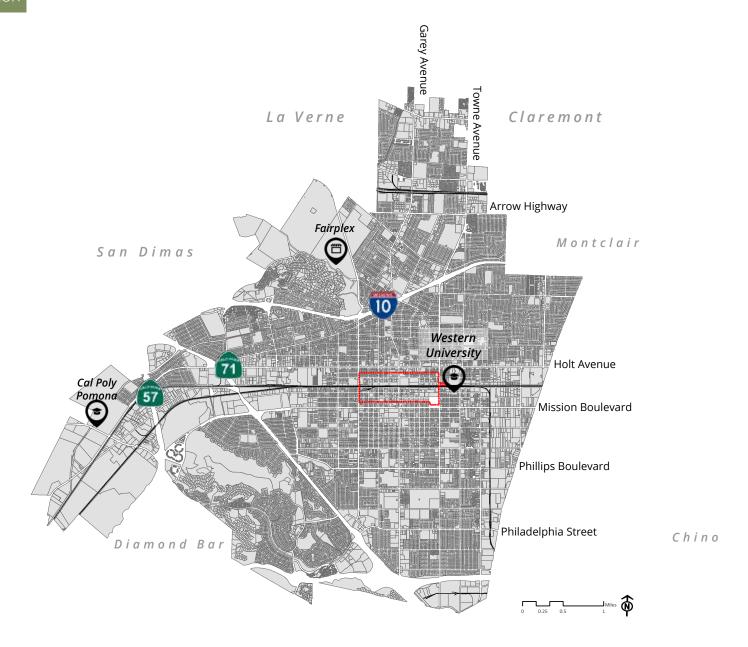


Figure 1-1, Regional Context

The Specific Plan pertains to all private and public properties that come under the purview of the Specific Plan as indicated in **Figure 1-2**, **Specific Plan Area**. The total acreage of the area is approximately 232 acres with approximate boundaries at Center Street to the north, Towne Avenue to the east, Fourth Street to the south, and White Avenue to the west.

1.1.3 General Plan Vision

As envisioned and described in the Pomona General Plan, Downtown is the community's center of business, culture and living. The Specific Plan area houses several assets and destinations such as the Pomona Transit Center and pedestrian bridge, the Arts Colony, Thomas Street, the Fox Theater and Western University of Health Sciences. Taking advantage of this diversity of uses, the General Plan reinforces Downtown as the mixed-use "heart of the City," placing emphasis upon its many activities and exceptional transit service. Building scale, location, and orientation foster the traditional pedestrian focus of Downtown. Likewise, streetscape and infrastructure improvements enhance walkability and the visual character of the City, helping to re-establish Pomona's identity and vitality.

Downtown will continue its renaissance as a mixed-use center enjoyed by locals and visitors alike. The greatest building heights and development intensities will be focused Downtown, paired with the Pomona Transit Center. Transit access — combined with Downtown's orientation to pedestrian use — makes it a place where people can live and work without relying on automobiles. As a result, particular emphasis will be placed on parking requirements appropriate to transit districts, strategically located shared parking facilities, and active parking management. As more people make Downtown their home, new parks and outdoor gathering spaces will make the district more livable and community-oriented.

Surrounding the activity and intensity of the Downtown core, the Downtown neighborhood will be a neighborhood with a difference. It will feature the City's widest range of contemporary housing types, and a wide mixture of uses that are compatible with the district's housing, all concentrated within walking distance of the Downtown Core's theater, shops, restaurants, cafes, nightlife, and amenities. As infill proceeds and the region continues to invest in its transit infrastructure, the neighborhood will benefit from an increasing level of service and activity generated by the Pomona Transit Center while also promoting transit ridership.

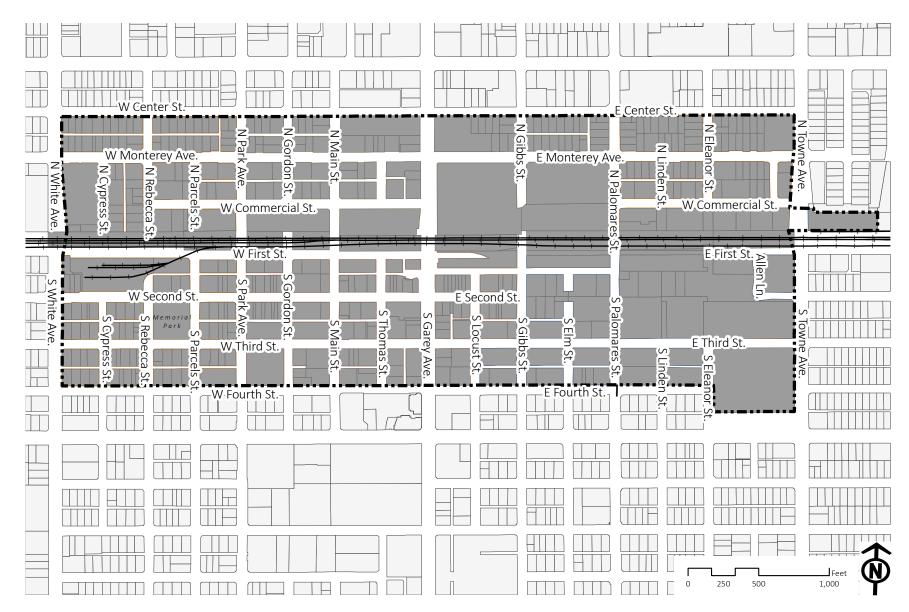
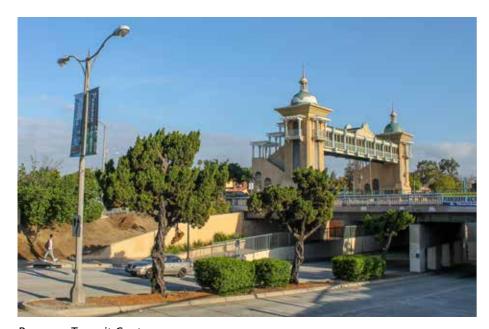


Figure 1-2, Specific Plan Area

Transit Center

The Pomona Transit Center, a facility providing both train and bus services, is located at the corner of Garey Avenue, between Commercial Street and the railroad tracks. The existing transit center has two distinct landmarks, the historic Pomona train station and the pedestrian bridge that services the station and connects to Downtown Pomona. In the immediate vicinity of the transit center, there are several sites potentially well-suited for future mixed-use development.



Pomona Transit Center

Historic Context

An important part of the Downtown Pomona Specific Plan is the preservation and enhancement of historic structures and areas. The Seventh Day Adventist Church and Fox Theater have already been designated as historic landmarks by City Council upon recommendation of the Historic Preservation Commission. There are four additional key structures identified in the original Specific Plan which were recommended to be designated as City of Pomona Historic Landmark Structures - Southern Pacific Rail Station, YMCA, Armory, and Masonic Temple. In 1986, the Edison Historic District was designated significant in the National Register of Historic Places for its contribution to architecture and engineering from 1875 through 1924. The District consists of 3.42 acres and includes eight contributing buildings located on the 500 block of West Second Street and two buildings on the 600 block of West Second Street. Edison Historic District is not listed on Pomona's Historic Register.

The Arts Colony and Antique Row run along Second Street, between Gordon and Gibbs Streets. This area is characterized by one-, two-, and three-story buildings presenting a fairly continuous scale. More than any other area in Downtown, this area maintains a feeling of a small rural California downtown Main Street with a few remaining buildings constructed of brick. Although individually none of the buildings are of significant architectural merit, the overall ambiance of the area has its own charm and should be maintained. Ideally, new buildings located within the area would complement the existing scale and character. Buildings in this area should be remodeled within the original style of each individual

building. These styles run from turn- of-the-century commercial buildings to streamline modern and 1950's styles. The small-town quality should be maintained and enhanced with additional street furniture and embellishments, such as colorful banners and flowers.

Thomas Street also contains a wide-variety of buildings including the Masonic Temple, a fine neo-classical building in brick, as well as the small, adjacent commercial structure, an example of a turn-of-the century commercial building. Thomas Street also contains excellent examples of Art Deco and Stream-Line Modern architectural styles.

Where new developments are proposed adjacent to existing buildings with unique architectural character and style(s), new development should take cues from the existing built environmental context including the definition of a clear architectural style, detailing appropriate to the chosen style, appropriate scale in relation to the existing structures, and pedestrian-orientation along Second and Thomas Streets.



Mayfair Hotel



Masonic Temple

Arts and Culture

The General Plan acknowledges the important role public art plays in relaying the story and identity of a city, district or neighborhood. Pomona has a rich and diverse culture and a variety of historic and cultural districts. Public art venues and forums create the opportunity for residents and visitors to participate in developing and sharing the City's culture and identity. The Arts Colony, originally established along Second Street, reflects the City's support and focus upon the arts and cultural expression, with its many galleries, performance venues and public murals. The Arts Colony has already moved beyond Second Street, and is envisioned to include the entirety of the Downtown Pomona Specific Plan boundary, as the arts are fundamental to Downtown's identity.

The General Plan and Specific Plan seek to further the growth of cultural and artistic awareness in the City by emphasizing public art along major transportation corridors and entryways into the City as well as within Downtown and neighborhood centers.

The enhancement of City streets, gateways and parks with public art is coupled with support for public murals and art installations throughout the City.

The Specific Plan supports integration of public art in new development and art and culture retail and service uses. It promotes the Downtown Pomona Arts Colony mission "...to make the arts inclusive and accessible, grow arts venues and organizations, nurture arts-friendly businesses, and celebrate emergent and established artists...bring communities together and revitalize the downtown with public art and festivals, exhibitions and performances, cuisine and entertainment..."



Antique Row

1.2 Community Engagement

Community engagement and analysis of opportunities and constraints informed development of key planning principles and provided the foundation for the Specific Plan.

1.2.1 Meetings and Workshops

The Specific Plan document is based on community input resulting from two community meetings, interviews with key stakeholders, Historical Preservation Commission and Vehicle Parking District Ad-Hoc Committee meetings, and the public hearing process with the Planning Commission and City Council. A brief summary of community engagement events and feedback themes is provided below.

- Key Stakeholder Meetings January 2017
- Community Workshop March 2017
- Community Open House May 2019

Key themes of public comments received include:

- Economic revitalization and business friendly approaches
- · Access, mobility and connectivity
- · Cleaner, safer and more welcoming environment
- Address undesirable uses and homelessness
- Community character and beautification
- Mix of commercial, residential and institutional uses

- · Importance of arts and education
- Family-orientation and need to accommodate all ages
- Protect historic resources
- Streamlined processes and clear policies





Community Open House

1.2.2 Planning Principles

Crafted from community input, the following planning principles shape the guidelines and standards contained in the Specific Plan.

- 1. **Heart of Pomona** Reinforce Downtown as the historic heart of the community and enhance its unique identity.
- 2. Celebrate Arts and Culture Continue and expand arts, culture and entertainment opportunities.
- 3. *Historic Heritage* Recognize, conserve and leverage Pomona's historic heritage and resources.
- 4. Inviting and Safe Foster a safe and clean downtown for residents, visitors and workers.
- 5. Expanded Mobility Improve and expand the mobility network that includes public transit, bicycle facilities, pedestrian corridors and links, as well as vehicle circulation and parking strategies.
- **6. Economic Revitalization** Support business owners and attract key employers to invest and thrive through education, resources, and programs.
- 7. Refresh and Maintain Encourage and facilitate resources for the upkeep and enhancement of properties.
- **8. Streetscape Beautification** Catalyze the beautification of streets and public spaces with trees, landscaping and furnishings.
- **9.** Residential Opportunities Create more places to live that offer a variety of options and inviting neighborhoods.
- **10.** *Implementation* Provide easy-to-use policies that are compatible with other documents and programs.





Downtown Pomona Streetscape

1.3 Document Organization

The Downtown Pomona Specific Plan is intended to be used by property owners, design professionals, developers, City staff, and review bodies to provide a clear set of policies for uses and development in the area over time. The Specific Plan consists of four chapters, briefly summarized below.

- *Chapter 1 Introduction* Provides an overview and context, describes the area affected, summarizes community engagement and vision, describes the relationship to other planning documents, and provides information on the administration of the specific plan.
- Chapter 2 Private Realm Provides the standards for form and property use, included permitted and prohibited land uses, areawide and specific standards, and design guidelines describing how the area's downtown, walkable character will be maintained and improved through architectural features and building and site standards.
- *Chapter 3 Public Realm* Describes plans for improvements to the area's network of streets, bicycles routes, sidewalks, parking and mobility, describes placemaking efforts, and provides information on infrastructure and utilities within the specific plan area.
- Chapter 4 Implementation Provides strategies and an action plan with which to implement the concepts described within the Specific Plan, as well as a guide to funding and financing sources.
- Appendix Provides background information such as a definition of terms.

1.4 Relationship to Other Planning Documents

The Specific Plan carries out and is influenced by a number of additional existing planning documents. This section briefly discusses key documents and policies considered in the formulation of the Specific Plan.

General Plan

Updated in 2014, the Pomona General Plan is the City's governance document for guiding decision making and outlines the City's visions and policies. The Downtown Pomona Specific Plan is consistent with the General Plan and provides for more precise implementation of the goals, objectives, and policies outlined within the General Plan.

Zoning Code

The City of Pomona Zoning Code is the primary regulatory document that implements the General Plan. The Zoning Code provides specific development regulations that are applicable to individual neighborhoods, districts and corridors that are consistent with the General Plan. In a case where the Specific Plan development standards and the City's Zoning Code should conflict, then the provisions and standards set forth in the Specific Plan shall prevail unless otherwise stated.

Corridors Specific Plan

Adopted concurrently with the Pomona General Plan update in March 2014, the Pomona Corridors Specific Plan provides regulations for public and private development and activities along key corridors including: Garey Avenue, Holt Avenue, and Foothill Boulevard. The Pomona Corridors Specific Plan boundary and regulations overlap with the Downtown Pomona Specific Plan, necessitating reconciliation of the boundaries and development of complementary development standards as part of the Downtown Pomona Specific Plan update.

Active Transportation Plan

Adopted in 2012, the Pomona Active Transportation Plan (ATP) provides recommendations for improving a system of bikeways connecting neighborhoods to key activity centers throughout the City, developing support facilities, such as bike parking, and education programs, and improving bicyclist safety. This document satisfies the requirements of the California Bicycle Transportation Act, granting the City eligibility for Bicycle Transportation Account (BTA) funding through the California Department of Transportation (Caltrans) improvements identified in this plan. The ATP also contains detailed recommendations for improving pedestrian safety and comfort at intersections and for prioritizing the installation of sidewalks where they do not currently exist.

Western University Master Plan

Situated in the southeastern portion of the Specific Plan, Western University of Health Sciences has developed a master plan. Western University is a private graduate school for health professionals and the Pomona campus consists of a renovated outdoor shopping center and nearby buildings. The Master Plan will address physical solutions to the future needs of the students, faculty and practitioners on campus. While not a City of Pomona document, the Western University Master Plan was considered in developing a collective vision for Downtown Pomona.



Western University

1.5 Administration

Authority and Procedure

The Specific Plan is established by the Pomona City Council in accordance with Section .584 of the Pomona Zoning Code, which establishes Specific Plans as an authorized mechanism for regulating land use and development in the City, as enabled by the State of California Government Code Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. All property within the Specific Plan area is subject to the development regulations of this Specific Plan. Where the Specific Plan is silent on a topic, the Zoning Ordinance will remain in force.

A specific plan may be adopted either by ordinance or by resolution (Government Code Section 65507). Should the legislative body wish to change a proposed specific plan recommended by the Planning Commission, the change must first be referred back to the Commission for consideration (Government Code Section 65504).

Specific Plan Amendments

Specific Plan amendments or modifications shall be made as described in Section .584 of the Pomona Zoning Code.

Interpretation

This section provides the rules for resolving questions about the meaning or applicability of any part of this Specific Plan and the General Plan. The following rules are established for interpretation.

- **A. Authority** The Development Services Director shall have the responsibility and authority to interpret the meaning and applicability of all provisions and requirements of this Specific Plan.
- **B. State Law Requirements** Where the Specific Plan references applicable provisions of State law (such as the California Government Code, Subdivision Map Act, Public Resources Code), the reference shall be construed to be to the applicable State law provision as they may be amended from time to time.
- **C. Conflicting Requirements** Any conflicts between the different requirements of this Specific Plan, or between this Specific Plan and other regulations, shall be resolved as follows:
 - **1. Specific Plan Provisions** In the event of any conflict among the provisions of this Specific Plan, the most restrictive requirement shall control.
 - **2. Specific Plans** In the event of any conflict between the requirements of the Zoning Code and standards adopted as part of this Specific Plan, the requirements of this Specific Plan shall control.

- **3. City Code Provisions** In the event of a conflict between requirements of this Specific Plan and other regulations of the City, the most restrictive shall control.
- 4. Private Agreements It is not intended that the requirements of this Specific Plan shall interfere with, repeal, abrogate or annul any easement, covenant, or other agreement that existed when this Specific Plan became effective. This Specific Plan applies to all land use and development regardless of whether it imposes a greater or lesser restriction on the development or use of structures or land than an applicable private agreement or restriction, without affecting the applicability or any agreement or restriction. The City shall not enforce any private covenant or agreement unless it is a party to the covenant or agreement.
- **D. Land Use District Boundaries** If there is uncertainty about the location of any land use district boundary shown on Figure 2-1, Specific Plan Districts, the following rules are to be used in resolving the uncertainty:
 - 1. Where the proposed zoning map shows any lot or area within a particular land use district, the zoning district shall extend to the centerline of any adjacent road, street, parkway or highway.
 - 2. If a district boundary divides a parcel and the boundary line location is not specified by distance printed on the zoning map, the Development Services Director shall determine the location of the boundary during project review by using the scale appearing on the zoning map.

3. Where a public street, alley, railroad, or other public right-of-way is officially vacated or abandoned, property that was within the former right-of-way shall be deemed to be included within the zoning district applicable to the property to which the abandoned right-of-way is being attached.

Severability

If any section, subsection, sentence, clause, phrase, or portion of this document, or any future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan document, or any future amendments or additions hereto. The City hereby declares that it would have adopted this document and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.

1.6 Split Parcel

A development site that spans multiple districts shall be subject to the Substantial Conformance process as defined in Section 1.7, which shall establish the development standards for the site in a manner most consistent with the underlying districts' standards. Any use that is permitted or conditionally-permitted over a portion of such a site shall be permitted or conditionally permitted anywhere on the site.

1.7 Substantial Conformance

Purpose

The purpose of the Substantial Conformance process is to determine whether the proposed development or use substantially complies with the standards, regulations, and guidelines of the Downtown Pomona Specific Plan and to maintain a degree of flexibility with respect to the details of the development approved within the Specific Plan area. Use of the Substantial Conformance process includes, but is not limited to the following purposes:

- 1. Determinations regarding issues, conditions, or situations that arise that are not addressed by the Downtown Pomona Specific Plan.
- 2. Determinations as to whether a use not listed in the Downtown Specific Plan is similar to the uses, including temporary uses, listed in Private Realm—Land Use.

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- 3. Approvals of modifications to the Urban Form standards and guidelines in Private Realm—Urban Form.
- 4. Approval of signs in Substantial Conformance with the sign provisions described in Private Realm—Signage.
- 5. Additions, deletions, and changes to the Specific Plan exhibits or text.
- 6. Determinations on applicable standards for split parcels.

Authority

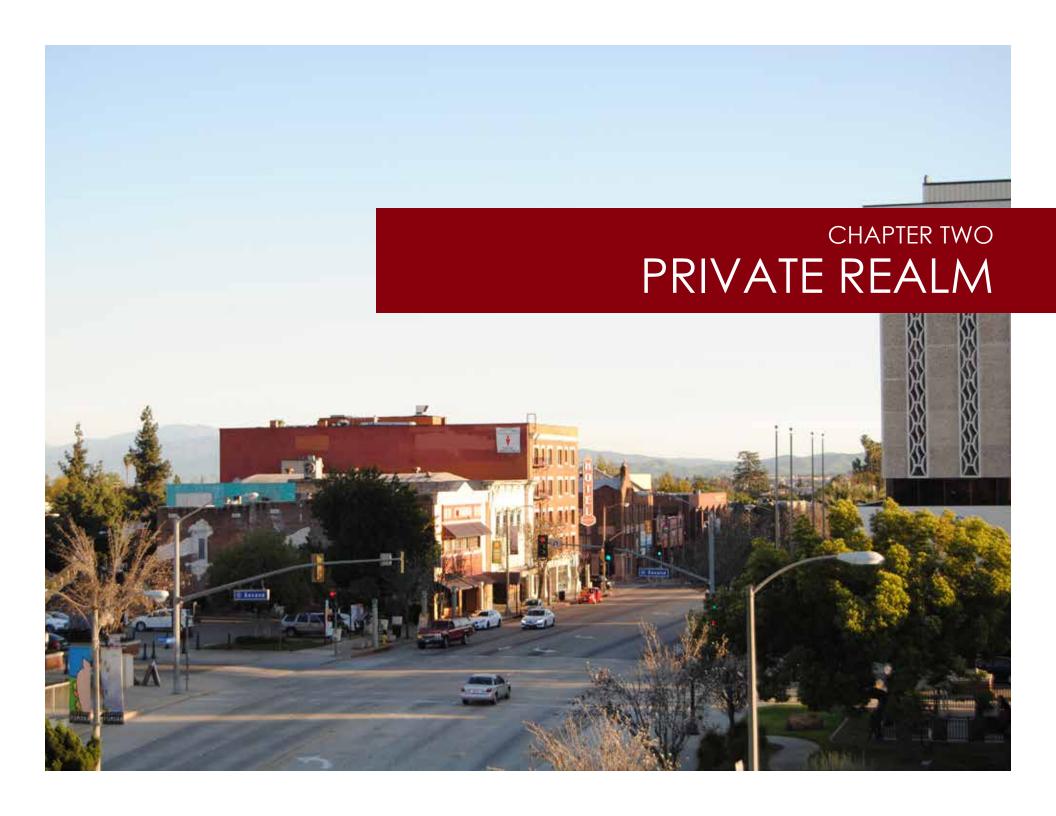
The Development Services Director shall review a request for a determination of Substantial Conformance and have the discretion to approve, with or without conditions; deny or refer the request to the Planning Commission and/or City Council for action in a noticed public hearing. Additional environmental review and/or analysis will be conducted to determine the potential impact of the request, if necessary under the California Environmental Quality Act (CEQA). Determinations of Substantial Conformance shall be provided to the applicant in writing and be based on findings that the request:

- 1. Substantially complies with the Downtown Pomona Specific Plan.
- 2. Will not adversely affect public health and Safety.
- 3. Will not adversely affect adjacent properties.

Application

Requests for a determination of Substantial Conformance shall be submitted on forms and with information as required by the Development Services Director.

The applicant or members of the City Council may appeal decisions of the Development Services Director, or his or her designee, regarding Substantial Conformance to the Planning Commission within 15 working days of receipt of the Development Services Director, or his or her designee's written decision. Decisions of the Planning Commission may be appealed within 15 working days of receipt of the Planning Commission's written decision to the City Council and payment of a fee consistent with the current fee resolution of the City Council.



2.1 Introduction

The Private Realm chapter provides direction for development of privately-owned areas within the Specific Plan to shape the design character and built environment. It sets forth general provisions for urban form, development standards, design guidelines and details permitted land uses. The provisions contained within this chapter apply to new construction, building additions, exterior remodels, relocations, and other modifications requiring a building permit. As stated in Chapter 1, Introduction, this Specific Plan supersedes the Zoning Code within the Specific Plan boundary. Where the Specific Plan is silent on a topic, the Zoning Code will remain in force.

2.1.1 How to Use This Chapter

This chapter sets the regulatory and design framework for developers, designers, city staff, and review bodies to develop, critique, and implement projects as they proceed through the entitlement process. It is divided into three sections; Urban Form, Land Use, and Private Signage. These sections are further categorized as follows:

Area Wide Urban Form Standards - Section 2.2.3 includes standards/regulations that apply to the entire Specific Plan area. Applicants must review and incorporate all applicable requirements. Reviewers shall ensure consistency between the project and the regulations/standards as applicable.



Pomona Holy Church



Urban Form Design Guidelines - Designers should comply with the intent of the guidelines that have been provided in Section 2.2.4 of this document. Reviewers should ensure compliance with the design guidelines.

District-Specific Urban Form Standards - Section 2.2.5 include tailored standards/regulations that serve as the zoning regulations for that sub-area. Applicants must comply with all applicable regulations. Reviewers shall ensure consistency between the project and the regulations/standards as applicable.

Land Uses - Applicants should review the list of permitted uses within Section 2.3 to ensure the proposed use is allowed.

Private Signage - Section 2.4 includes tailored standards/ regulations regulating private signage within the Specific Plan area. Applicants must comply with all applicable regulations. Reviewers shall ensure consistency between the project and the regulations/ standards as applicable.

2.2 Urban Form

The future urban form of Downtown Pomona will be established by providing opportunities for development that adheres to the following customized context-sensitive development standards and placemaking design guidelines. Projects shall comply with the urban form standards contained within this section and the intent of the guidelines.

Standards. Standards are specifications that the community considers essential to the creation and preservation of a high quality, sustainable and coherent city. Conformance with Standards is mandatory. Such provisions are indicated by the use of the words "shall," "must," "is required," and "is/is not permitted.

Guidelines. Guidelines provide additional information to assist the designers with fulfilling the intent of the Specific Plan. Guidelines pertain to issues of visual character and aesthetics. Conformance with Guidelines is recommended, especially to ensure the swiftest possible approval. Although conformance with Guidelines is recommended, developers are permitted to propose alternative design solutions if they are able to show that such design solutions meet the overall objectives of the Specific Plan. Guidelines are indicated by the use of the words "should," "may," or "is/are encouraged."

2.2.1 Applicability

Compliance with all Urban Form standards and guidelines may require review and approval of a Development Plan Review. The following thresholds require review and approval of a Development Plan Review:

- 1. New construction of a primary structure on a vacant or developed lot.
- 2. New construction of accessory structures that are greater than 25 percent of the gross floor area of the primary structure.
- 3. Additions to a primary structure that are greater than 25 percent of the gross floor area of the primary structure.
- 4. Reconstruction of a primary structure that will either a) change the façade of the existing primary structure, or b) exceed 25 percent of the gross floor area of the existing primary structure.

If a proposed project does not meet any of these four thresholds, it shall be subject to a Planning ministerial review as part of a plan check submittal to the Building and Safety Division. All nonconforming buildings and structures are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions. Applicability thresholds shall not apply to legally non-conforming single-unit residential, which shall be subject to R-1-10,000 standards.

2.2.2 Review and Approval Process

- 1. The Director of Development Services has the authority to approve, conditionally approve, or deny a Development Plan Review. To approve a Development Plan Review, the Director of Development Services shall make the following four findings:
 - a. The project is consistent with the City of Pomona General Plan and all applicable requirements of the City of Pomona Municipal Code; and
 - The project will not be detrimental to the general welfare of persons working or residing in the vicinity nor detrimental to the value of the property and improvements in the neighborhood; and
 - c. The project will not adversely affect the policies contained in the Public Realm chapter of the Downtown Pomona Specific Plan; and
 - d. The project complies with the applicable regulations contained in the Private Realm chapter of the Downtown Pomona Specific Plan and all other applicable Federal, State, and local regulations.
- 2. All Development Plan Reviews require a public hearing before the Director of Development Services. Noticing shall be given to the general public by the applicant at least 14 calendar days prior to the day of the scheduled hearing, to be sent by physical mail to owners and occupants of

- property located within a 400-foot radius of the project site boundaries. The City of Pomona may also voluntarily notice the public hearing through any other manner necessary.
- 3. The Director of Development Services has the authority to approve or deny a CEQA exemption, Negative Declaration, or Mitigated Negative Declaration, and to certify an Environmental Impact Report, in association with the Development Plan Review.
- 4. The action taken by the Development Services Director on a Development Plan Review at a public hearing is final unless appealed to the Planning Commission by either the applicant or a member of the Planning Commission within 20 calendar days of the action.
- 5. If a Development Plan Review is submitted in conjunction with another discretionary application, it shall be subject to review and approval by the highest applicable approving body as one item.

An approved Development Plan Review shall be valid for a period of 12 months. A maximum of two 12-month time extensions may be requested from the Director of Development Services. Such requests shall be submitted in writing by the applicant, property owner(s), and/or authorized designee at a minimum of 30 days prior to the expiration of the Development Plan Review. If the applicant has not obtained grading or building permits or consistently pursued necessary permits for construction approved under the Development Review within the approved or extended time period, the Development Plan Review shall be terminated.

2.2.3 Areawide Urban Form Standards

The following form standards apply to all districts within the Specific Plan area.

1. Site Planning

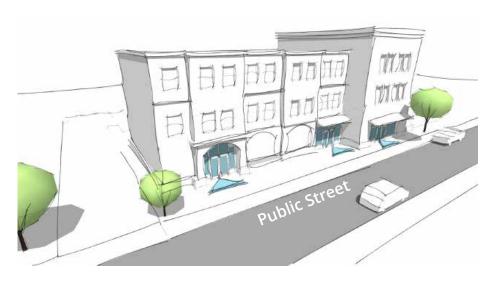
Site planning is key to quality development. The following standards promote orienting buildings toward the street, providing access for both vehicles and pedestrians, and facilitating internal pedestrian circulation.

A. Building Articulation

1. Any building over 125 feet wide shall be broken down to read as a series of buildings no wider than 75 feet each.

B. Building Orientation

- 1. Buildings shall be oriented and contain an entrance parallel to the primary street frontage.
- 2. Buildings shall be designed and oriented to minimize the visual intrusion into adjoining residential properties. Window, balcony and deck locations shall be directed away from window and private areas of adjoining residences (onsite or off-site).



Building Orientation

C. Pedestrian Access

On-site pedestrian circulation and access shall be provided according to the following standards:

- Internal Connections. A system of pedestrian walkways shall connect all buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas or pedestrian amenities.
- 2. To Circulation Network. Regular connections between onsite walkways and the public sidewalk shall be provided.
 - A. An on-site walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage.
 - B. Walkways shall be the shortest practical distance between the main entry and sidewalk, generally no more than 125 percent of the straight-line distance.
 - C. Where a required walkway crosses a driveway, parking area, or loading area, it must be clearly identifiable through the use of a raised crosswalk, a different paving material, or similar method.
 - D. Where a required walkway is parallel and adjacent to an auto travel lane, it must be raised or separated from the auto travel lane by a raised curb at least four inches (4") high, bollards or other physical barrier.

- To Neighbors. Direct and convenient access shall be provided from commercial and mixed-use projects to adjoining residential and commercial areas to the maximum extent feasible while still providing for safety and security.
- 4. To Transit. Safe and convenient pedestrian connections shall be provided from transit stops to building entrances. Sidewalk "bulb-outs" or bus "pull-outs" may be required at potential bus stops serving commercial centers (building floor area over 25,000 square feet) to provide adequate waiting areas for transit users and safety for passing motorists.

E. Limitations on Curb Cuts

Curb cuts shall be minimized and located in the location least likely to impede pedestrian circulation. Curb cuts shall be located at least 10 feet from an intersection curb return or pedestrian crosswalk.

F. Landscaping

Landscaping is subject to the requirements of Section .503-J-Landscaping of the Pomona Zoning Ordinance, except for percentage minimums for parcels on residential and non-residential landscape standards.

3. Parking

The following provisions apply to parking within the Downtown Pomona Specific Plan:

A. Limitations on Location of Parking.

- 1. Off-street parking spaces shall be located to the rear of principal buildings whenever possible. Above ground parking may not be located within 20 feet of a street-facing property line. Exceptions may be granted where the review authority makes the following findings:
 - a. The design incorporates habitable space built close to the public sidewalk to the maximum extent feasible; and
 - b. The site is small and constrained such that underground parking or surface parking located more than 40 feet from the street frontage is not feasible.

B. Parking Regulations.

- 1. Parking requirements shall be calculated on the total square footage for each primary use on a site. Total square footage includes any square footage where a primary use occurs, excluding incidental spaces such as storage rooms, restrooms, kitchens, and other secondary spaces that support a primary use. Determinations on which spaces can be considered secondary shall be made by the Development Services Director.
- 2. Parking requirements do not apply to the first two floors of any proposed development within the MU-CBD or MU-I districts.

 Moreover, the first two floors of any existing development within said boundaries are also not subject to parking requirements.
- 3. Parking shall be located on site or provided within 1,000 feet of proposed development. However, at least 50% of total required parking shall be provided on-site. Moreover, the use of off-site parking as part of a proposed development shall require conditions of approval to ensure the ability to use such parking for an extended period of time.
- 4. The maximum number of parking spaces permitted does not apply to parking structures.

- 5. Parking structures adjacent to residential uses shall incorporate design features to minimize light or sound impacts.
- 6. Light fixtures within parking strucutres shall be designed to minimize off-site spill lighting.

C. Parking Relief

 Provide a parking demand study, conducted by a registered traffic engineer, to substantiate a reduction in total parking requirements; studies are subject to review and approval by the Development Services Director.

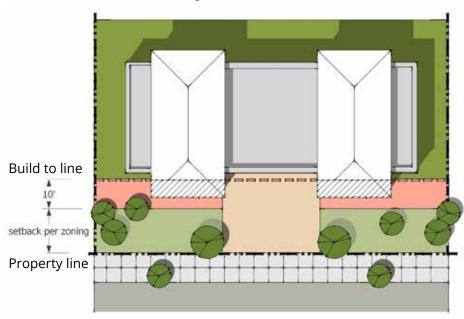
E. Parking Ratios

- 1. Residential Uses (per unit)
 - a. Live/Work: .5 min / .5max
 - b. Studio: .5 min / .5 max
 - c. One bedroom: 1 min / 1 max
 - d. Two bedroom: 1.5 min / 1.5 max
 - e. Additional bedrooms: .5/room
- 2. Public/Semi-Public Uses
 - a. One per 35 square feet of assembly area
- 3. Commercial (per 1,000 total square feet)
 - a. Retail: 2.5 min / 3.5 max
 - b. Eating & Drinking: 3 min / 8 max
 - c. Entertainment & Recreation: 5 min / 8 max
 - d. Office: 2.5 min / 3.5 max
 - e. Medical Office: 4 min / 5 max
- 4. Lodging (per guest room)
 - a. Hotel: 1 min / 1.2 max

4. Required Building Location

The following building location requirements apply in MU-CBD, HU-HDR, and MU-I districts:

A. Build-to Line. Buildings shall be located within 10 feet of the required setback line for at least 40 percent of the linear street frontage.



Build-to Line

- **B. Corner Build Area.** Buildings shall be located within 10 feet of the required setback line within 30 feet of the street corner.
- **C. Frontage Improvements.** The area between buildings and the property line shall be improved as part of a wider sidewalk, outdoor dining/seating area, or with landscaping. Paving shall be minimized.
- **D. Exceptions.** These requirements may be modified or waived by the review authority upon finding that:
 - 1. Entry courtyards, plazas, entries, or outdoor eating areas are located adjacent to the property line and buildings are built to the edge of the courtyard, plaza, or dining area; or
 - 2. The building incorporates an alternative entrance design that creates a welcoming entry feature facing the street.



5. Building Transparency/Required Openings.

The following building transparency and opening requirements apply in MU-CBD, MU-I, and MU-HDR districts:

Exterior walls facing and within 20 feet of a front or street side lot line or pedestrian walkway shall include windows, doors, or other openings for at least 50 percent of the building wall area located between 2.5 and 7 feet above the level of the sidewalk. Such walls may run in a continuous plane for no more than 30 feet without an opening.

A. Design of Required Openings. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.

- **B. Exceptions for Parking Garages.** Multi-level garages are not required to meet the ground-story transparency requirement.
- **C. Reductions.** The building transparency requirement may be reduced or waived by the review authority upon finding that:
 - 1. The proposed use has unique operational characteristics with which providing the required windows and openings is incompatible, such as in the case of a cinema or theater; and
 - 2. Street-facing building walls will exhibit architectural relief and detail and will be enhanced with landscaping in such a way as to create visual interest at the pedestrian level.



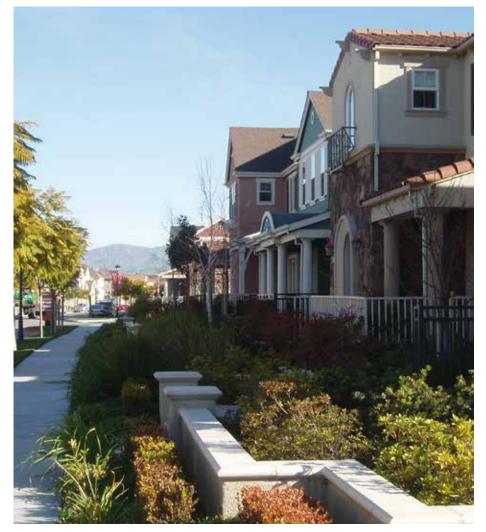
Building transparency/required openings

2.2.4 Areawide Urban Form Guidelines

The following urban form design guidelines apply to all districts within the Specific Plan area. The City should consider implementing Architectural Design Incentives for the Downtown Pomona Specific Plan to promote and preserve the historical integrity of existing visible streetscape.

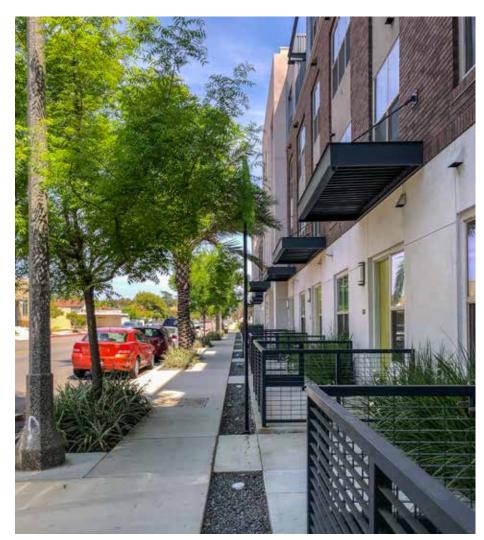
1. General Guidelines

- A. The following general urban form guidelines are desirable, and should be incorporated into all project designs where most applicable:
 - 1. Significant wall articulation.
 - 2. Variation in building height on larger buildings to give appearance of distinct massing elements.
 - 3. Multi-pitched roofs.
 - 4. Durable building materials and finishes.
 - 5. Richness of surface and texture.
 - 6. Awnings and roof overhangs.
 - 7. Regular or traditional window rhythm.
 - 8. Easily identifiable entries.



Distinct massing elements



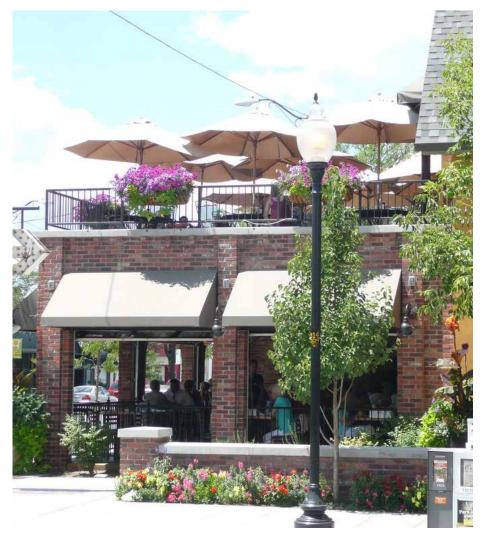


Buildings oriented towards street

- 9. Significant, sustainable, drought-tolerant landscape design elements arranged in a thoughtful, comprehensive manner.
- 10. Enhanced paving and other elements giving attention to pedestrian scale.
- 11. Adequate landscape and screening for parking areas.
- B. The following design elements are not desirable and should be avoided:
 - 1. Large, blank, unarticulated wall surfaces.
 - 2. Visible outdoor storage, loading and equipment areas.
 - 3. Exterior security bars, gates, and grills are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
 - 4. Chain link fencing is prohibited.
- C. The following design elements are encouraged only if incorporated into an architecturally interesting or artistic, unique development:
 - 1. Unpainted, plain color concrete block walls.
 - 2. Highly reflective surfaces.
 - 3. Metal Siding.

2. Site Planning Guidelines

- A. Buildings should be oriented toward streets, plazas and open spaces to embrace intersections and surrounding neighborhoods.
- B. Buildings should be placed at the setback line(s) to define and enliven the street edge and maximize access from the public sidewalk.
- C. Buffer residential uses from incompatible commercial and office development by providing intensified landscaping, increased setbacks, appropriate building orientation, or combination thereof, where mixed use development abuts a residential use.
- D. Orient commercial and office buildings, wherever feasible, to minimize shading of adjacent residences and impairing privacy.
- E. Outdoor dining and cafes are encouraged, particularly along Second and Third Streets.
- F. Units and buildings should be sited with view opportunities to open space as much as possible.
- G. Buildings should be sited in a manner that will be compatible with other buildings in the vicinity. Long rows of garages that create a monotonous streetscape should be avoided.



Outdoor dining



Streeet parking along Garey Avenue

- H. Private open space should be provided adjacent to each unit, at least partially screened from adjacent common areas, to the extent feasible.
- I. Create public spaces and plazas for social gathering and activities.
- J. Provide open space and recreation areas.
- K. Buildings with ground level restaurants should have spaces for outdoor patios and sidewalk dining.
- L. Entry nodes should include low walls with signage, identification signage or monument signage, where adequate space allows for their construction.

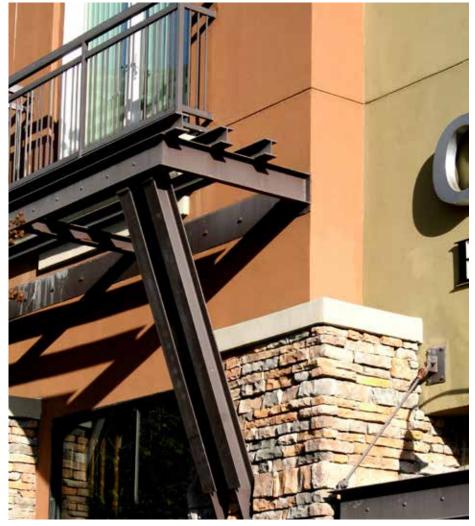
3. Parking Guidelines

- A. Parking should not dominate street frontages and should be provided within commercial, office, and residential structures.
- B. Parking structure facades should be articulated in similar character as surrounding uses.
- C. Parking for new development should either be underground or hidden in the interior of the block, with retail, or office space on the ground level facing the sidewalks.

- D. All parking areas visible from the public right-of-way should be screened by hedges, shrubs or other plant materials. Large expanses of parking should be broken up by planting trees, particularly at the ends of and along the length of aisles.
- E. Parking lot landscaping should accent driveways, frame the major circulation aisles, and highlight pedestrian pathways.

4. Architectural Guidelines

- A. Attached residential units should include design elements to add visual interest and to avoid "box-like" appearances. Elements such as balconies, porches, arcades, dormers, and cross gables should be considered.
- B. Wall and roof planes should be varied and articulated into smaller modules that reduce the overall massing and scale of the building and add richness and variety. Long, unarticulated building facades are discouraged.
- C. Renovated buildings should retain historic architectural features of existing structures. New construction should make a strong new architectural contribution.
- D. Balconies, porches and projecting windows are encouraged to help contribute to eyes on the street.
- E. Primary building materials should include brick, brick façade, stucco, stone, stone veneer or other similar materials.



Varying building textures



Art installation

- F. Varying building textures are encouraged.
- G. Signage should be integrated into architectural design.
- H. Awnings and canopies are encouraged.
- I. Orient windows in non-residential structures to avoid direct line of sight into adjacent residential uses.
- J. All rooftop and mechanical equipment should be screened from public and pedestrian view using parapets or other architecturally acceptable methods.
- K. Trellises, pergolas, gazebos, patios/courtyards and other outdoor structures are encouraged.
- L. To the extent feasible, avoid windows facing directly onto a neighboring window or private outdoor space.
- M. Exterior window bars, grills and gates are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
- N. Street trees, landscaping, street furniture and architecture should contribute to the human scale.
- O. Provide art in public gathering areas for visual appeal and placemaking.
- P. Provide lighting to create safe environments.
- Q. Special consideration should be given to lighting of entry areas, courtyards, and other pedestrian gathering places.

5. Landscaping Guidelines

- A. Landscape treatments should incorporate native colorful plant material and evergreen trees.
- B. The use of trees to define and enclose exterior space, and to provide physical protection from the sun and wind, is encouraged.
- C. Provide for landscape open space areas that enhance the building design and public views, provide buffers/ screening and transitions between adjacent uses.
- D. Screen undeveloped, vacant land intended for future phases with plants that prevent wind and water erosion.
- E. Existing healthy mature trees should be preserved and incorporated into the overall landscaping design, where appropriate.
- F. Use of vines and climbing plants on buildings, trellises and privately-owned perimeter walls is encouraged.
- G. Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.
- H. Minimize the use of water through the selection of plants that are drought-tolerant when they are mature.



Use of vines and potted landscaping



Drought-tolerant planting



2.2.5 Districts

This Specific Plan establishes a district-based zoning approach with each district guided by a tailored set of form standards and allowable uses that support the desired future desired character and user experience. This hybrid approach combines form-based development standards with a selection of compatible uses that have been tailored for each Specific Plan District resulting in a mix of uses, whereby the desired building forms and uses dictate what is permitted, conditionally permitted, or not permitted. The form standards regulating each district constitute the constraints for a project's building envelope in which new construction or a remodel is permitted. In addition, design guidelines are provided to ensure high-quality design that reflects the area's envisioned character. Form standards and design guidelines are captured within this section, Urban Form, and allowed uses are located in the second part of this chapter, Land Use.

Individual district boundaries, **Figure 2-1 Districts Map**, within the Specific Plan have been organized by land use and zoning regulations and have been updated to reflect and reconcile the Specific Plan relationship to the Pomona Corridors Specific Plan boundaries.

The following districts have been established:

- Mixed Use Central Business District (MU-CBD)
- Mixed Use Institutional (MU-I)
- Mixed Use High Density Residential (MU-HDR)
- Residential Multifamily (R-MF)

For each district, the following sets of policies are provided:

- Vision and Character
- Purpose and Form Standards



Figure 2-1, Districts Map



Cafe in the Central Business District

1. Mixed Use – Central Business District (MU-CBD)

A. Vision and Character

The Mixed Use-Central Business District is envisioned to allow for either horizontal or vertical mixed-use development to support a vibrant, pedestrian-friendly urban environment providing a variety of employment, entertainment, cultural, and shopping opportunities for both residents and non-residents of Pomona. An influx of daily transit users and weekend visitors entering and exiting the adjacent Amtrak/Metrolink Station are envisioned to be greeted by inviting plazas and informative wayfinding signage that connect to the Central Business District through enhanced pedestrian linkages. Home to the expanding arts colony, the MU-CBD district is also intended to support arts-related uses.

New, mixed use developments reflecting the character of Pomona will cater to the pedestrian by incorporating active and articulated front facades. Cafes with outdoor dining, coffeehouses, pubs, and retail shops will be located at street-level, enlivening the district environment and providing commuters and residents access to early morning coffee and after work dining opportunities. Structured parking, located above ground and hidden from public view, will provide commuters a safe place to park their vehicles with convenient access to the nearby Amtrak/Metrolink Station.

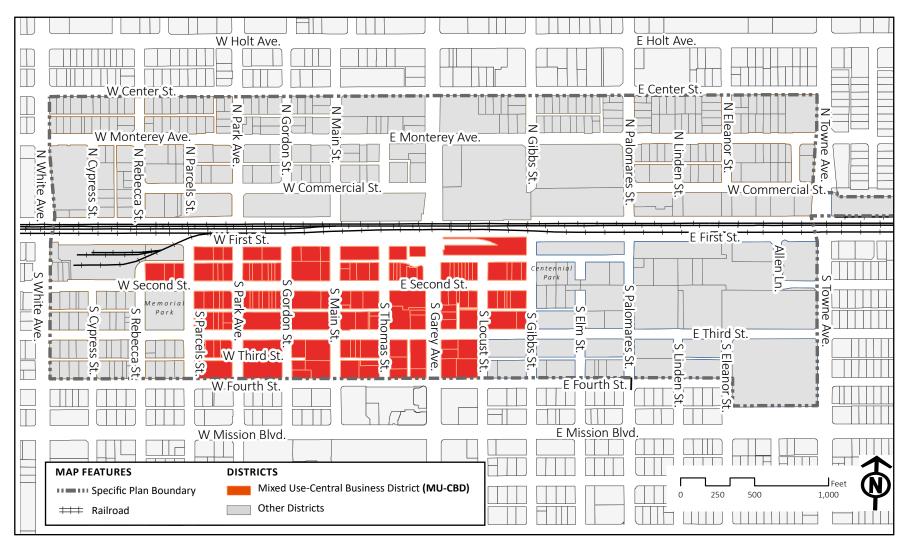


Figure 2-2, Mixed Use Central Business District (MU-CBD) Map

B. Purpose and Form Standards

1. Purpose

Projects located within the MU-CBD should strive to accomplish the following objectives:

- A. Provide for the orderly, well-planned, and balanced development of commercial and mixed-use districts;
- B. Designate adequate land for a full range of local- and regional-serving economic and cultural resources consistent with the General Plan to maintain and strengthen the city's economic resources;
- C. Provide appropriately located areas for a range of commercial uses that provide for a variety of good and services for residents, employees, and visitors;
- D. Provide opportunities for a mix of complementary uses that may combine residential and nonresidential uses or combine a variety of nonresidential uses on the same site; and
- E. Promote pedestrian-oriented, mixed-use commercial centers at appropriate locations.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-CBD district are contained within Table 2-1.

3. Street Frontage

A minimum six-foot unobstructed walkway shall be provided between a building facade and the street/curb along any street frontage within this district. Sidewalks located within the public right-of-way may be included.

Table 2-1: Mixed-Use Central Business District (MU-CBD) Form Standards			
Standard	Requirement		
Density (units/acre)	East of Park Street: 50 to over 100 maximum		
	West of Park Street: 40 to 70		
Number of Stories	3 minimum to 12 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks of the R-MF District		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential	Total/unit: 150 sf		
unit)	Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		

2. Mixed Use – Institutional (MU-I)

A. Vision and Character

The Mixed Use-Institutional District is intended to serve as the major governmental, educational and office spine for the Downtown while still incorporating mixed use developments with residential and retail uses. The MU-I zone is also intended to allow for higher education development and, in particular, for Western University campus expansion including classrooms and administrative offices, dormitories, other student/faculty housing, ancillary retail/service and office uses, such as a college bookstore, educational materials and supply store, tutorial services, etc. In general, single-user office buildings, and multitenant office buildings are encouraged in this zone with the intent of providing economic and employment base opportunities. It is projected that office and related service/support uses and ancillary development could locate in proximity to the MU-CBD zone and, thus, complement the entertainment commercial development within the "core" area as employees of MU-I zone patronize the entertainment and associated retail uses provided.



Western University





Figure 2-3, Mixed Use Institutional Map

B. Purpose and Form Standards

1. Purpose

The specific purposes of the Mixed Use-Institutional District are to:

- A. Provide areas for educational facilities, cultural and institutional uses, health services, parks and recreation, general government operations, utility and public service needs, and other similar and related supporting uses;
- B. Provide for the orderly, well-planned, and balanced development of institutional, commercial and mixed-use land uses compatible with the surrounding districts including the MU-CBD district;
- C. Provide opportunities for attached and multi-family residential development; and
- D. Promote pedestrian-oriented, mixed-use development at appropriate locations.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-I district are contained within Table 2-2.

Table 2-2, Mixed Use-Institutional District (MU-I) Form Standards			
Standard	Requirement		
Maximum Density (units/acre)	West of Palomares: 40 to 100 East of Palomares: 40 to 80		
Number of Stories	3 minimum to 6 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks of the R-MF District		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential unit)	Total/unit: 150 Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		



Surrounding the activity and intensity of the Downtown core, the Downtown neighborhood will be a neighborhood with a difference. It will feature the City's widest range of contemporary housing types, and a wide mixture of uses that are compatible with the district's housing, all concentrated within walking distance of the Downtown Core's theater, shops, restaurants, cafes, nightlife, and amenities.

-Pomona General Plan

3. Mixed Use-High Density Residential (MU-HDR)

A. Vision and Character

This Mixed Use-High Density Residential District includes residential, retail, service and office uses and is intended to function as a pedestrian precinct complementing the Downtown area. One of the Specific Plan objectives is to provide housing opportunities for households of varying economic means and social needs. To allow for variations in the marketplace, the land use plan has been designed with flexibility allowing shifts in residential densities and housing types, including housing for artists, educators, and work-live units, in addition to assuring a range of choices in price and lifestyle for future residents. This will result in a greater mixing of residents, incomes and lifestyles and a less insular neighborhood identity. With the location of the transit center within the MU-HDR district, this area is envisioned as a pedestrian friendly, high-density commuter residential neighborhood with ancillary retail, service and office uses. Residential products in this area will be a mix of lofts, apartments, town homes and condominiums at a variety of medium to high densities.



Figure 2-4, Mixed Use High Density Residential Map

B. Purpose and Form Standards

1. Purpose

The specific purposes of the Mixed Use - High Density Residential District are to:

- A. Provide for the orderly, well-planned, and balanced development of commercial, residential and mixed-use districts;
- B. Designate adequate land for a full range of localand regional-serving retail and commercial services consistent with the General Plan to maintain and strengthen the city's economic resources;
- Provide appropriately located areas for a range of commercial uses that provide for a variety of good and services for residents, employees, and visitors;
- D. Provide opportunities for a mix of complementary uses that may combine residential and nonresidential uses or combine a variety of nonresidential uses on the same site; and
- E. Promote pedestrian-oriented, mixed-use commercial centers at appropriate locations.

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Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-HDR district are contained within Table 2-3.

Table 2-3, Mixed Use-High Density Residential Form Standards			
Standard	Requirement		
Density (units/acre)	40 to 100		
Number of Stories	3 minimum to 6 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks from R-MF district		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential unit)	Total/unit: 150 Private: minimum 50 sf for 50% of units Common: minimum 100 sf/unit		

4. Residential Multifamily (R-MF)

A. Vision and Character

The Residential Multifamily District allows multifamily dwellings at densities that range between 70-80 maximum dwelling units per acre. The R-MF District is intended to provide areas for a wide variety of medium to high-density residential development. Housing types emphasize multiple units and include townhouses, condominiums, and apartment buildings. This district also provides for uses such as schools, daycare centers, parks, and community facilities that may be appropriate in a higher-density residential environment.







Figure 2-5, Residential Multifamily Map

B. Purpose and Form Standards

1. Purpose

The purposes of the Residential Multifamily District are to:

- A. Provide for a full range of multi-unit housing types consistent with the General Plan:
- B. Preserve, protect, and enhance the character of the City's different residential neighborhoods and the quality of life of City residents.
- C. Ensure adequate light, air, privacy, and open space for each dwelling.
- D. Ensure that the scale and design of new development and alterations to existing structures are compatible with surrounding homes and appropriate to the physical characteristics of the site and the area where the project is proposed.
- E. Provide sites for public and semi-public land uses such as parks, schools, daycare, and other uses that will serve City residents and will complement surrounding residential development.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the R-MF district are contained within Table 2-4.

Table 2-4, Residential Multifamily Form Standards			
Standard	Requirement		
Maximum Density (units/acre)	South of Railroad Corridor: 40 to 70		
	North of Railroad Corridor 40 to 80		
Number of Stories	South of Railroad Corridor: 4 maximum		
	North of Railroad Corridor 6 maximum		
Minimum Setbacks (ft.)	For attached single-unit dwellings, required setbacks apply to the ends of rows of the dwellings		
Front	0		
Interior Side	10		
Street Side	10		
Rear	10		
Minimum Building Separation	See subclassifications below		
Between Residenital Buildings	15		
Between Residential Building and Accessory Building	6		
Between Accessory Building	6		
Open Space (sf/unit)	Total/unit: 150		
	Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		

3. Incentives

A. Affordable Artist Work/Live Units

Developers that voluntarily provide affordable artist work/ live units for new development within the Downtown Pomona Specific Plan may take advantage of development concessions that are intended to make such projects more financially viable.

- i. To qualify, the proposed work/live unit must:
 - a. At least 25% of all residential units must meet the standards of the Work/Live ordinance of the City of Pomona, including unit size and operations.
 - Work/Live units must be rented at fair market value, to be recorded against the property through a housing covenant.
- Upon qualifying as an affordable artist work/live unit, the following development concessions shall be granted:
 - a. A 50% reduction in total parking demand for proposed development.
 - b. A 25% reduction in required square footage of common open space for proposed development.

B. Privately Owned Public Open Space (POPOS)

Developers that voluntarily provide privately owned public open space (POPOS) as part of new development within the Downtown Pomona Specific Plan may take advantage of development concessions that will aid the City of Pomona in providing accessible public open space across Downtown Pomona.

To qualify, the proposed POPOS must:

- i. Be accessible to the general public.
 - a. For open plazas that do not require entry through a building, public access shall be 24 hours a day.
 - b. For public open spaces that require entry through a building or gate, public access shall be from sunrise to one hour after sunset.
 - c. For all proposed public open spaces, alternative hours requested by a private property owner are subject to review and approval by the Director of Development Services.
- Follow the design guidelines for POPOS as indicated in 3.2.2—Downtown Placemaking Standards and Guidelines.

Upon qualifying as privately owned public open space, the following development concessions shall be granted:

- i. A 75% reduction in total common space requirements for proposed development.
- ii. POPOS will be considered common open space.

2.3 Land Use

An important component of this Specific Plan process is the provision of a mix of land uses that reflect the intended vision for each District. The uses shown in Table 2-5 as Permitted (P) are acceptable anywhere in the land use designation in which they are located. They will not require special conditions when developed in accordance with this Specific Plan. Several uses are shown as Conditionally Permitted (CUP) as a result of potentially incompatible characteristics. Conditional uses require special consideration so they may be properly located with respect to their effects on surrounding properties. This may be because of their hours of operation or their potential nuisance aspects such as light, odors, glare, or noise. Conditional Use Permits are subject to public review and require a public hearing before the Planning Commission.

2.3.1 Applicability and Thresholds

1. Applicability

Any use to be established on a property located within the Downtown Pomona Specific Plan is subject to the provisions of this section. All nonconforming uses are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions

When a use is not specifically listed in the Land Use Matrix it shall be assumed that such uses are expressly prohibited. Proposed uses that are not explicitly listed in the Land Use Matrix may be permitted through the Substantial Conformance process, see Section 1.7.

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2. Building Use Regulations

Table 2-5 specifies the various land uses that are allowed within each of the land use districts of the Downtown Pomona Specific Plan. The letter in the columns beneath the district designations describe the allowed uses category described as follows:

- *P:* The use is permitted by right within the specified district (i.e., no discretionary application required).
- P2: The use is permitted only in upper story levels, lower levels/basement or in the rear of a ground-floor building subject to the following provisions:
 - In no event shall the rear use occupy more than 65% of the groud story area.
- M: The use is only allowed within the specified district with the approval of a Minor Conditional Use Permit consistent with applicable provisions of the DPSP and Pomona Zoning Code.
- CUP: The use is only allowed within the specified district with the approval of a Conditional Use Permit consistent with applicable provisions of the DPSP and Pomona Zoning Code.
- N/A: Not Allowed in this zone district. A symbol of " " also indicates a not-allowed use.

3. Review and Approval Process

A. Minor Conditional Use Permit

- i. Uses subject to a Minor Conditional Use Permit shall be approved, approved with conditions, or disapproved by the Planning Division. To approve a use, the Planning Division shall make the same findings as for a Conditional Use Permit as contained in the Pomona Zoning Ordinance. The Planning Division may impose conditions deemed reasonable and necessary to ensure that these findings are met.
- ii. An application for a Minor Conditional Use Permit shall not require a public hearing.
- iii. The action taken by the Planning Division is final unless appealed to the Planning Commission by either the applicant or a member of the Planning Commission within 20 calendar days of the action.

B. Conditional Use Permit

i. Uses subject to a Conditional Use Permit shall be approved, approved with conditions, or disapproved by the Planning Commission, and are subject to the regulations and procedures pertaining to Conditional Use Permits contained in Section .580 of the Pomona Zoning Code.



Pomona Fish Market

2.3.2 Land Use Matrix

Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Residential Uses				
Single-Unit Dwelling, Detached	-	-	-	-
Single-Unit Dwelling, Attached	-	-	-	Р
Two-Unit Dwelling	-	-	-	-
Multi-Unit Development	Р	Р	Р	Р
Work/Live Units	Р	Р	Р	Р
Family Day Care				
Small	Р	Р	Р	Р
Large	-	-	-	М
Group Residential	-	-	М	М
Residential Care Facility				
Small	Р	Р	P	Р
Large	-	-	М	М
Residential Facility, Assisted Living	-	-	M	М
Supportive Housing	Transitional and supportive housing			
Transitional Housing	constitute a residential use and are subject only to those restrictions that apply to other residential uses of the same type in the same district.			

Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Public/Semi-Public Uses				
Colleges and Trade Schools	С	С	-	-
Convention Center	-	С	-	-
Cultural Institutions	М	М	М	М
Day Care Centers	М	М	М	М
Government Offices	P2	Р	-	-
Hospitals and Clinics				
Hospitals	-	С	-	-
Clinics	-	Р	Р	-
Instructional Services	М	М	М	М
Park and Recreational Facilities	Р	Р	Р	Р
Parking Lots and Structures	М	М	М	-
Performing Arts, Fine Arts, Dinner Theater	М	С	С	-
Public Assembly	С	С	С	С
Public Safety Facilities	С	С	С	С
Schools	С	С	С	С
Social Service Facilities	М	М	М	М
Tutoring Facilities	Р	Р	Р	-

Table 2-5, Land Use Mati	Гable 2-5, Land Use Matrix			
	MU-CBD	MU-I	MU-HDR	R-MF
Commercial Uses				
Alcohol Sales (On-sale only)	М	М	М	-
Alcohol Sales (Off-sale only)	С	С	С	-
Animal Care, Sales, and Services				
Animal Sales and Grooming	Р	Р	Р	-
Veterinary Services	-	Р	Р	-
Art Sales and Services				
Art Gallery	Р	Р	Р	Р
Artist Studio	Р	Р	Р	Р
Artisanal Manufacturing	М	М	М	М
Artist Cooperative (Co-op)	Р	Р	Р	Р
Banks and Financial Institutions	P2	Р	-	-
Commercial Entertainment and Recreation				
Cinema/Theaters	Р	Р	Р	-
Indoor Sports and Recreation	Р	Р	Р	-

Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Eating and Drinking Establishments				
Bars/Night Clubs/Lounges	М	М	М	-
Restaurant, Full Service	Р	Р	М	-
Restaurant, Limited Service	Р	Р	Р	-
Restaurant, Take-Out Only	Р	Р	М	-
Drive Through	-	-	-	-
Outdoor Dining & Eating	Р	Р	Р	-
Food and Beverage Manufacturing (Small Scale)	Р	Р	-	-
Hotels	С	С	С	-
Maintenance and Repair Services	-	-	-	-
Nurseries and Garden Center	-	-	-	-
Offices				
Business, Professional, and Technology	P2	P2	Р	_



Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Medical and Dental	P2	P2	Р	-
Walk-In Clientele	-	Р	Р	-
General Personal Services	Р	Р	Р	-
Private Garden	Р	Р	Р	Р
Recharging Station	Р	Р	Р	Р
Retail Sales				
Convenience Markets	Р	Р	Р	-
Food and Beverage Sales	Р	Р	Р	-
General Retail	Р	Р	_	-
Solar Energy Systems	Р	Р	Р	Р

2.3.3 Temporary Uses

1. Intent

The Downtown Pomona Specific Plan recognizes that a diversity of land uses and activity will contribute towards making Downtown Pomona a vibrant destination to live, work, and play. The purpose of this section is to encourage a variety of temporary uses on private property in Downtown Pomona while also establishing conditions to ensure that such temporary uses do not adversely affect the public health, safety, and welfare, are compatible with surrounding land uses, protect the rights of adjacent residents and land owners, and minimize any adverse effects on surrounding properties and the environment. This section will supersede any temporary use provisions in the Pomona Zoning Code for all parcels located within the Downtown Pomona Specific Plan. This section does not apply to any publicly-owned property, which is subject to a Special Event Permit.

2. Authority

The Development Services Director is authorized to approve, conditionally approve or to deny a Temporary Use Permit application. The Development Services Director may establish conditions and limitations to minimize detrimental effects on surrounding properties, including but not limited to hours of operation, provision of parking areas, signing, lighting, and traffic circulation access. A business license shall also be obtained.

3. Application

The property owner or the owner's authorized representative shall file an application for a Temporary Use Permit with the Planning division at least 30 days prior to the date the proposed use takes place. Non-profit organizations or other organizations with verifiable tax-exempt status are not subject to any fees associated with a Temporary Use Permit. For reoccurring temporary uses, the property owner or the owner's authorized representative shall file a single application for a Temporary Use Permit with the Planning Division with the predetermined dates at least ten days prior to the first predetermined date of the proposed use to take place. All reoccurring temporary uses may be approved for predetermined dates for up to three calendar months.

The Development Services Director may request additional documentation, such as traffic studies, operation plans, or other data necessary to adequately review a Temporary Use Permit application.

4. Approval

To approve a Temporary Use Permit application, the Development Services Director shall make the following findings:

- A. The proposed temporary use will be located, operated, and maintained in a manner consistent with the policies of the General Plan and the provisions of the Downtown Pomona Specific Plan.
- B. Approval of the application will not be detrimental to property or improvements in the surrounding area or to the public health, safety or general welfare.
- C. The proposed temporary use complies with the various provisions of this ordinance.
- D. All building, electric, plumbing, fire, encroachment or other permits required by city ordinances shall be obtained.

5. Permitted Uses

The following temporary uses are allowed on all private property located within the boundaries of the Downtown Pomona Specific Plan:

- A. Pop-up restaurant and retail. The temporary use permit shall be subject to the following conditions:
 - i. Limited to one restaurant or retail use for a 60-day day period.
 - ii. The sale of alcohol may be permitted subject to approval by Alcoholic Beverage Control (ABC).
 - iii. The proposed use shall obtain all necessary permits with the Planning and Building and Safety Divisions for interior tenant improvements proposed for the restaurant or retail use. Restaurant uses shall also obtain all necessary healthy and safety permits.
 - iv. The proposed signage shall abide by the Temporary Sign Permit regulations, with the exception to duration. The proposed signage shall be limited to the period approved for the temporary use permit.
- B. Other temporary uses which meet the required findings for Substantial Conformance.

2.3.4 Non-Conforming Provisions

1. Uses that legally exist and are permitted prior to the adoption of this plan, shall be classified as legal, nonconforming uses and are allowed to continue operation provided they do not cause any adverse health, safety, or aesthetic related impact(s) on the community. These uses are not allowed to expand to additional sites, locations or addresses in a zone where the use is not specified as a permitted use except as allowed herein and as approved by the Planning Commission under a Conditional Use Permit.

A legal non-conforming business may be sold or have a change of management, including a new building occupant or tenant with the same use as previously existed without having to revert to a use that is permitted by the applicable zone.

A legal non-conforming use may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.

2. Commercial or residential use. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone unless a continuation or reestablishment of that commercial or residential nonconforming use is approved by the Planning Commission under a Conditional Use Permit within three years of its closure as noted herein and said use is shown to be a use that will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole.

A revocation hearing will be held for a legal non-conforming use that displays a pattern of code violations or which create a public nuisance and if the use is revoked, it is deemed "closed" and must revert to a use that is permitted by the applicable zone. The continuation or reestablishment of a revoked non-conforming use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management, including a new building occupant and/or tenant, and that such continued or re-established use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation.

- 3. Residential structures. When a legal nonconforming, residential structure(s) is/are demolished, deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, the land use at that location must revert to a use permitted by the applicable zone. Where a legal nonconforming, residential structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of a Building Permit. A revocation hearing will be held for a legal non-conforming residential use that displays a pattern of code violations or creates a public nuisance and if the use is revoked, it is deemed "closed" and must revert to a use that is permitted by the applicable zone. The re-establishment of a revoked legal non-conforming residential use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management and that such re-established residential use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation. Applicability thresholds shall not apply to legally nonconforming single-unit residential, which shall be subject to R-1-10,000 standards.
- 4. Commercial or residential use within a nonconforming structure. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business or residence to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone, except that the same or substantially the same use may be reestablished or established within said non-conforming structure within three years of its closure as noted herein when that structure was specifically designed and built for, and historically occupied by, the same or substantially the same type of use.
- 5. Non-conforming structures. When a structure that was built to all applicable standards and codes at the time of construction and under an appropriately issued Building Permit becomes nonconforming due to a change(s) to the development standards of the Downtown Pomona Specific Plan (including setbacks, height limits, landscaping, parking, architecture, equipment screening, storage, fencing, etc.), said structure may continue to be occupied or may be re-occupied by a use permitted within the underlying applicable zone which required the same standards consistent with those for the structure as it was originally constructed. Said occupation or reoccupation may occur at any time provided that the structure has been maintained in a safe and sanitary condition, has not become a public

or attractive nuisance, has not been physically altered in a manner that would reduce conformance to the original development standards at the time of original construction and upon approval of a Conditional Use Permit by the Planning Commission. Legal non-conforming structures may be sold or have a change of management, including a new building occupant or tenant with the same or substantially similar use as historically existed without having to physically alter or modify the structure or site to conform to the current development standards of the underlying applicable zone. When a legal non-conforming structure is deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, any future structure(s) at that location must conform to the development standards of the underlying applicable zone. Where a legal non-conforming structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate and re-occupy (with the same or substantially similar use) said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of an appropriate Building Permit.

A legal non-conforming structure may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.

2.4 Signage

2.4.1 Intent

The signage regulations contained in this section are intended to promote the orderly development of new signage within the Downtown Pomona Specific Plan and to encourage the community to recognize, enhance, protect and promote Downtown Pomona's unique character and identity. The purpose of this section is to promote public safety and welfare by regulating signs with the following stated objectives:

- 1. Prioritize the pedestrian environment within the Downtown Pomona Specific Plan.
- 2. Encourage high quality sign design.
- 3. Assure that signs are sized and arranged to minimize clutter and prevent unsafe conditions.
- 4. Assure that signs harmonize with the underlying urban design district where they are located.
- 5. Assure that signs and their message are of sufficient size and design to be legible and easily read by the intended viewer.
- 6. Encourage creative and innovative approaches to signs within the context of Downtown Pomona.

- 7. Ensure that commercial signs are designed for the purpose of identifying a business in an attractive and functional manner, rather than to serve primarily as general advertising for a business.
- 8. Ensure that signs on the façades of buildings reinforce the existing character and are integrated into the architectural scheme of the building.
- 9. Promote cohesiveness among signs on a single building.
- 10. Ensure that new signs on historical structures are sensitive to the historic character of the structure.

2.4.2 Applicability

The sign requirements in this section apply to all new signage within the boundaries of the Downtown Pomona Specific Plan. Where the provisions of this section conflict with Section .503-K or any other provisions of the Zoning Code, the Downtown Pomona Specific Plan shall prevail. Where this section is silent regarding signs, the Zoning Code of the City of Pomona shall be reviewed by the Planning staff to determine the applicability of the development standards in the Zoning Code.

2.4.3 Application Procedure

Except as exempted in Section .503-K of the Zoning Ordinance, all new signs, reconstructed signs, and sign alterations, except change of copy, require approval of a sign permit from the City of Pomona Planning Division, with permit types defined in Table 2-6. Building permits and/or electrical permits shall be obtained as required in accordance with the Uniform Building Code and/or National Electrical Code.



Pomona Fish Market Neon Sign

Table 2-6, Permit Types, Applicability, and Review			
Type of Sign Permit	Applicability	Review Body	Apellant Body
Sign Permit	All new primary, secondary, and portable signs; repurposed signs; and alterations to existing signs beyond like-for-like replacement or repair.	Planning Division	Planning Commission
Master Sign Plan	As defined in .503-K C. 4	Planning Division	Planning Commission
Creative Sign Permit	Any new creative sign that cannot meet standards due to innovative features	Planning Commission	City Council
Minor Certificate of Appropriateness	All new signs on a registered historic landmark or district, as defined in .5809-13 (Historic Preservation)	Planning Division	Historic Preservation Commission
Temporary Sign Permit	All new temporary signs	Planning Division	Planning Commission

2.4.4 Sign Categories and Types

Commercial signage generally falls into one of the following four categories:

- 1. **Primary sign.** A business with one building frontage shall have only one primary sign, typically a wall sign, unless another type of sign is approved under a Creative Sign Permit. In cases where a Creative Sign Permit has been approved for a sign, that sign shall be designated as the primary sign. A business with multiple building frontages may have one primary sign on each building frontage.
- 2. Secondary sign. Secondary signs are usually installed in addition to the primary building sign. Typically they include awning signs, projecting signs and under-canopy signs; and are generally intended to be viewed by pedestrians. A business may have one secondary sign per storefront for the business.
- 3. Portable sign. Intended to be viewed by the pedestrian, portable signs are intended to be placed on the sidewalk in front of a business. They include sandwich board signs and "A"-frame signs. A maximum of one per business is permitted during hours of business operation only and is not counted as either a primary or secondary sign.
- **4. Repurposed sign.** A repurposed sign may advertise a business no longer in existence and/or is a sign that has been preserved for use in a new location for the purpose

- of improving the aesthetic quality of the location. Historic signs can add value to the Downtown area by providing artifacts of visual interest along building frontages.
- 5. Historic sign. A historic sign is one that is proposed to be located on a building that is registered as a federal, state, or local historic landmark. Such signs require a Minor Certificate of Appropriateness.
- 6. Creative sign. A creative sign cannot meet standards contained in this section due to innovative or creative elements unique to the sign. A creative sign, when approved, will take the place of a Primary sign for that respective building frontage. Creative signs are subject to approval by the Planning Commission.
- **7. Temporary sign.** Usually meant for a special purpose and are used for short periods. Only one per tenant is permitted and is not counted as either a primary or secondary sign.

2.4.5 Permitted Signs

The following types of signs are permitted within the DPSP area provided they are consistent with the stated criteria and have an approved sign permit. When the word "shall" is used or specific standards are given, the criteria are considered standards. If the words "may" or "should" are used, the criteria is a guidelines and compliance is strongly recommended unless a suitable alternative can be substituted that achieves the same purpose.

1. A-frame Signs (Sandwich Board)

- A. A-frame signs shall not obstruct or interfere with pedestrian traffic on the sidewalk.
- B. A-frame signs shall be removed from the sidewalk when the business it is advertising is closed.
- C. Maximum area is 12 square feet per sign face.
- D. Maximum size is four feet high by three feet wide.
- E. A-frame signs shall not be permanently affixed to any object, structure or the ground.
- F. A-frame signs shall be professionally constructed using one of the following materials: plastic, wood or metal suspended from a wire frame, wooden A-frame signs with open bases, or shaped silhouette signs made of plywood, metal, or similar wood-like material that can withstand various weather conditions.
- G. Glass, breakable materials, paper, laminated paper, vinyl, plastic, PVC pipe frames, or illumination are not permitted materials for A-Frame Signs.

2. Awning and Canopy Signs

- A. Lettering shall be permitted only on the valance and shall not exceed six inches in height.
- B. Awnings with a solid color that is compatible with the building colors are preferred.
- C. Striped awnings may be appropriate for buildings without ornamental facades. Striped awnings with highly contrasting, bright colors may be visually blaring and are inappropriate.
- D. Only permanent signs that are an integral part of the awning shall be permitted.
- E. Awning signs shall be permitted only for first floor tenants.
- F. Awnings shall not be lighted from under the awning (backlit) so that the awning appears internally lit. Lighting directed downwards that does not illuminate the awning is allowed.
- G. Awning and canopy materials are limited to matte finish canvas, glass, or metal.
- H. Translucent fabric and plastic are not permitted as awning or canopy material.

- I. Awnings shall be installed in a location that does not obscure transom windows, grillwork, piers, pilasters and other ornamental features. In openings with transoms, awnings shall be installed below the transom on the horizontal frame separating the storefront window from the transom.
- J. Shed awnings without end panels are the preferred awning style. Convex-shaped awnings may be appropriate for rounded arched window and door openings.
- K. Awnings should have simple horizontal valances. Scalloped or decorative valences are discouraged.

3. Building Edge Signs

- A. Building edge signs are permitted on buildings located on street corners.
- B. Edge signs shall include a graphic image relating to the business being advertised.

4. Exposed Neon Tube Signs

- A. Exposed neon signs are signs where the neon is exposed with or without a backing.
- B. Neon lighting shall not exceed 30 milliamps.

5. Hanging (under canopy) Signs

- A. Hanging signs shall only be used at ground floor locations and placed in front of the business frontage which it is advertising.
- B. Maximum area for a single face of a hanging sign is three square feet.
- C. The sign shall be a minimum of seven feet above the sidewalk.
- D. A maximum of one tenant per sign is permitted.

6. Low-Profile Monument Signs

- A. A low-profile monument sign shall have a base and sign face area above it.
- B. Use of architectural details such as cornices and materials that complement or reflect those on the building is required.
- C. Lettering shall consist of individual letters in a style and material that is consistent with the business use and/or the architectural style of the on-site building.
- D. Maximum height of the sign, including the base and sign face, shall be limited to four feet.
- E. Illumination shall consist of either external fixtures designed to complement the sign's appearance or halo-lighting.

- F. A maximum of one low-profile monument sign is permitted per property.
- G. The area around the base of the sign shall be covered with a minimum width of two feet of landscaping, rock, or other aesthetic material to soften the appearance of the sign.
- H. Low-profile monument signs may be located within any land use district in the Downtown Pomona Specific Plan, but are limited to properties where the main building is set back a minimum of ten feet from the front property line.
- I. Low profile signs shall only be used when other types of signage would not provide adequate identification.

7. Over-Sized Projecting Signs

Over-sized projecting signs shall only be permitted on buildings over two stories in height with approval of a Creative Sign Permit. Over-sized projecting signs shall be consistent with the following

- A. Project no more than two feet from a wall to which it is attached.
- B. Minimum of 10 feet above grade.
- C. One over-sized projecting sign is permitted per ground floor business with building frontage on a public street.
- D. Over-sized projecting signs shall be located directly in front of the business which it is advertising.

E. Located no higher than the height of the finished floor level of the fourth floor of the building.

8. Projecting Signs

- A. The total area of a sign face of a projecting sign shall not exceed 3.5 square feet.
- B. The bottom of a projecting signs shall be seven feet above the surface over which it projects.
- C. Projecting signs shall not project more than four feet into a public right-of-way and shall not project closer than two feet from a curb.
- D. Projecting signs shall have a minimum thickness of 18 inches.
- E. Internally lit projecting signs shall have opaque face panels so that only the letters, logos, numbers or symbols appear to be lit.
- F. Projecting signs shall be a minimum of 15 feet from another projecting sign and 5 feet from an interior property line or line dividing two separate business frontages.
- G. Projecting signs shall hang from decorative metal brackets of a size and design appropriate to the sign and the architectural character of the building. The design of the sign bracket shall not distract or compete with the design of the sign.

9. Restaurant Menu Signs

- A. Must be a permanently mounted display box or freestanding display box on one or two poles where the menu is placed within a display box. The sign shall be located outside adjacent to the main entrance of the restaurant.
- B. Freestanding restaurant menu signs shall be removed from the exterior located when the restaurant closes.
- C. High quality materials and artistic designs shall be used in the construction of menu signs.
- D. Menu signs are not included in the total allowable square footage for a business.
- E. Maximum allowable sign area is six square feet.
- F. Menu signs shall be appropriate in size, location and design to the character and architectural detail of the building as well as the restaurant.
- G. Only one menu sign is permitted per restaurant.

10. Roof Signs

- A. Roof signs may be permitted under a Creative Sign Permit with the following restrictions:
 - i. Such signs shall only be located on buildings six floors or higher.
 - ii. Location. Permitted on non-local streets.

11. Temporary Banner Signs

- A. Temporary banners signs in the Downtown Pomona Specific Plan area shall be consistent with the following:
 - i. A Temporary Banner sign shall be permitted for a maximum of 180 days per calendar year for each business storefront.
 - ii. Maximum size of a temporary banner is no more than 45 square feet in area.
 - iii. Shall require approval of a Temporary Sign Permit.
 - iv. Temporary Banner Signs shall be continuously maintained. Banners shall be replaced if tattered or worn.

12. Tenant Directory Signs

- A. Tenant directory signs shall be constructed and oriented to the pedestrian.
- B. Maximum height is seven feet.
- C. Sign copy shall include only the building or project name, project logo, address, business tenant names, and suite numbers or letters.
- D. The letter size of the building name, project name, or logo shall not exceed four inches in height. All other copy shall not exceed two inches in height.
- E. Tenant Directory signs should be constructed out of materials that compliment both the building structure and its use.

13. Wall Signs

- A. Wall signs shall be located only on a primary or secondary building frontage and shall not extend above an eave or parapet.
- B. Wall signs shall have individual letters and may be either internally (except as cabinet signs) or externally lit.

- C. Maximum area of a wall sign is limited to 1.5 square feet per linear foot of building frontage.
- D. Wall signs are only permitted for businesses located on the first floor of a building and businesses located on the other floors when limited to one square foot per lineal foot of building frontage.
- E. Wall signs should be designed to create a clearly defined edge, provide shadow relief, and a substantial appearance. This effect is generally difficult to achieve by painting the sign directly on the building. For this reason, painted signs are discouraged.
- F. Wall signs shall be designed to be compatible with the storefront in scale, proportions, and color.
- G. Wall signs shall be mounted flush and fixed securely to a building wall, projecting no more than 12 inches from the face of a building wall, and not extending sideways beyond the building face or above the highest line of the building to which it is attached.

14. Window Signs

Window signs shall be consistent with the following criteria:

- A. Permitted for ground floor commercial throughout the mixed use districts within the specific plan area.
- B. Placement is limited to ground floor storefront windows.
- C. Window signs above the first floor are prohibited.
- D. Maximum of 20 percent of the window area of each window on front of building occupied by business displaying signs.
- E. Sign copy shall not exceed eight inches in height.
- F. Window signs should be created from high-quality materials such as paint, gold-leaf, or neon. Appropriate techniques for application of window signs include sandblasting or etching.
- G. Window signs shall be applied directly to the interior surface of the window or hung inside the window, such that all mounting hardware and equipment is concealed. Window signs shall not be more than 12 inches from the window surface.

15. Prohibited Signs

- A. Cabinet signs
- B. Pole signs
- C. Off-site signs, except where otherwise permitted
- D. Inflatable signs
- E. Internally lit awnings
- Statues, sculptures, large three dimensional props and parked vehicles or vehicle trailers used specifically for advertising
- G. Telephone numbers
- H. Captive balloon
- I. Roof signs except as approved under a Creative Sign Permit
- J. Super graphics, except as approved under a Creative Sign Permit

- K. Electronic center displays, except time and temperature signs
- L. Electronic Signs
- M. LED signs
- N. Freestanding banner signs
- O. Spinners

2.4.6 General Criteria

1. Sign Illumination

The following types of sign illumination are permitted within the DPSP:

- 1. A projecting light source is recommended for signs within the DPSP area. Use of small, unobtrusive fixtures for external (projection) lighting is encouraged. Avoid use of oversized fixtures that are out of scale with the sign and structure. Light sources shall be shielded and directed away from adjacent residential uses.
- 2. Back-lit signs, also referred to as halo-lit signs, consisting of opaque individually cut letters installed directly on a building.
- 3. Neon signs

2. Electrical Raceways and Conduits

Use of electrical raceways and conduits shall be consistent with the following:

- 1. Electrical transformer boxes and raceways are required to be concealed from public view.
- 2. If a raceway cannot be mounted internally behind the finished exterior wall, the exposed metal surfaces of the raceway shall be finished to match the background wall or integrated into the overall design of the sign.
- 3. If a raceway is necessary, it should be as thin and narrow as possible and should never extend in width or height beyond the area of the sign's lettering or graphics.
- 4. All exposed conduit and junction boxes shall also be concealed from public view.

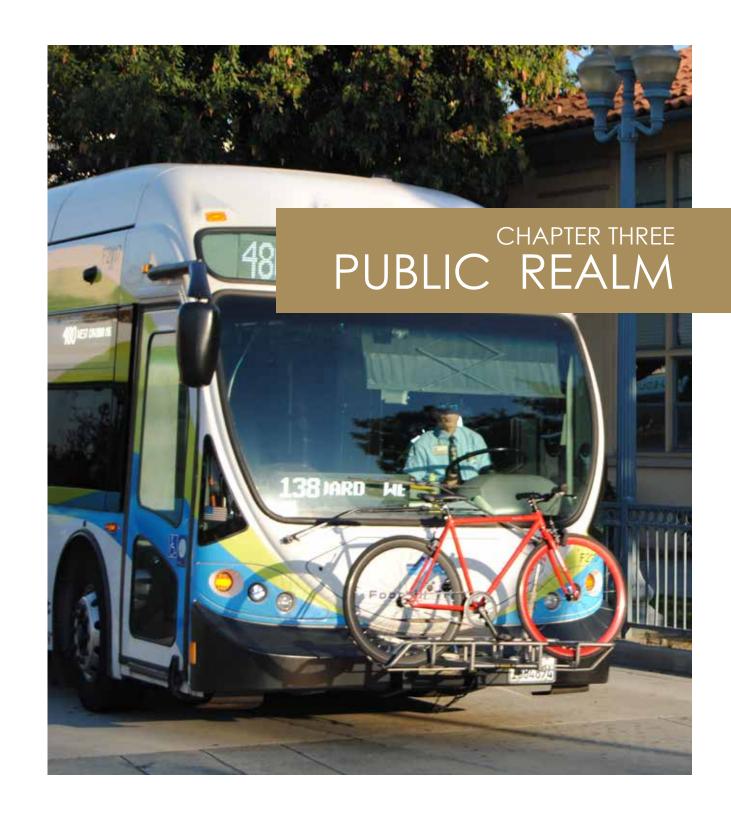
3. Sign Placement

Wall signs should be placed in the area of the façade historically used for this purpose such as the sign band between the storefront and the upper facade. Where no sign band exists, a wall sign shall be placed immediately above the storefront. For new construction, a sign panel should be included on the façade of the building directly above each storefront.



2.4.7 Nonconforming Signs

Minor repairs, maintenance, and upkeep of legal nonconforming signs are permitted but in no case shall the sign be modified or altered that would result in an increase of non-conformity. All signs that advertise a business that is no longer on site shall be considered abandoned and shall be removed within 60 days of the opening of a new establishment, unless the sign is approved under a repurposed sign permit. Any legal nonconforming sign destroyed by natural causes may be restored to its original state.



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3.1 Introduction

The Public Realm chapter provides direction for external spaces that are publicly accessible within the Specific Plan. The public realm should complement land uses and buildings (regulated by Chapter 2 – Private Realm) and consists of the 'everyday spaces' that are used by people to socialize, play, work, shop, traverse and use for range of activities and social opportunities. The provisions contained within this chapter apply where improvements are proposed in public rights of way as well as publicly accessible spaces. This chapter addresses a range of topics including mobility, placemaking, and infrastructure and utilities. The mobility section emphasizes providing a range of transportation options including pedestrian improvements, bicycle network enhancements, vehicular circulation considerations, and transit services as well as parking strategies. The placemaking section envisions an inviting and functional public realm and includes guidance relating



to historical landmarks, art in public places, parklets and plazas, pedestrian amenities, streetscapes, lighting and wayfinding. Lastly, the infrastructure and utilities section addresses public services and systems that provide backbone support for a successful downtown.

3.2 Mobility

3.2.1 Intent

One of the central directives of the Specific Plan is to create beautiful yet functional streets that include integrated transportation systems, provide pedestrian and bicycle links, and improve overall vehicular circulation. The beautification and mobility improvements for Downtown Pomona will strengthen the City's identity and move residents, visitors, goods, and services more efficiently. This chapter is organized into circulation, bicycle, pedestrian and transit networks followed by parking strategies.

3.2.2 Mobility Goals

Informed by the community engagement process, the mobility goals guide future multimodal transportation improvements.

- 1 Accommodate an effective public transportation system.
- 2 Improve accessibility for persons with disabilties.
- Provide connectivity and accessibility to the Commuter Rail Station from Downtown areas.
- Ensure an adequate supply of convenient parking for emerging development projects.
- Provide for parking and circulation of bicycles.
- Promote a pedestrian scale environment with enhanced pedestrian areas.







3.2.3 Circulation Network

Downtown Pomona is characterized by a traditional street system grid. The circulation network consists of streets (divided into arterial, collector and local classifications) that provides access to and from regional and local facilities and accommodates travel by various modes of transportation (automobiles, trucks, public transit, bicycles and walking). As shown in **Figure 3-1, Circulation Network**, streets are categorized using a hierarchical system of classification based on function, location, and design, and are assigned a classification based on the following descriptions.

Arterial

Arterial streets are generally high speed/high capacity roads that connect to major regional transportation facilities, such as the expressway system, and serve relatively long trips. Some designated arterials in Pomona are medium speed/medium capacity roads for intracommunity travel, usually providing direct connections from one side of the City to the other. Within Pomona high capacity roads are designated as "major" arterials and medium capacity roads are designated as "minor" arterials. Within the Specific Plan area, Holt Avenue is a prominent major arterial and Garey Avenue is considered a minor arterial.

Collector

Collectors are the 'bridge' between access and mobility in the functional classification system. These streets generally feature lower speeds and volumes than arterials and provide for circulation between neighborhoods (and not just within them). Their main function is to provide access for short trips and distribution to the arterial network.

Local

Local streets are low speed and low volume roadways that provide direct access to abutting land uses. Driveways to individual units, on-street parking, and pedestrian access are allowed.

The street grid system network supports convenient access to transit, with the Pomona Transit Center located centrally in the Specific Plan area. In addition, the downtown area benefits from lower speeds on arterial roadways. While the lower speeds reduce overall roadway capacity for vehicular traffic, lower speeds also provide an opportunity to increase the on-street parking supply and enhance the streetscape. Posted speed limits within the Specific Plan area range from 25 mph to 40 mph.

The Union Pacific Railway runs east-west through the Specific Plan area and can provide a barrier in the area. To address this challenge, an undercrossing is provided for Garey Avenue, White Avenue and Towne Avenue, while at-grade crossings are provided at Park Avenue, Main Street, and Palomares Street. A pedestrian bridge crosses the railway west of Garey Avenue near the Pomona Transit Center.

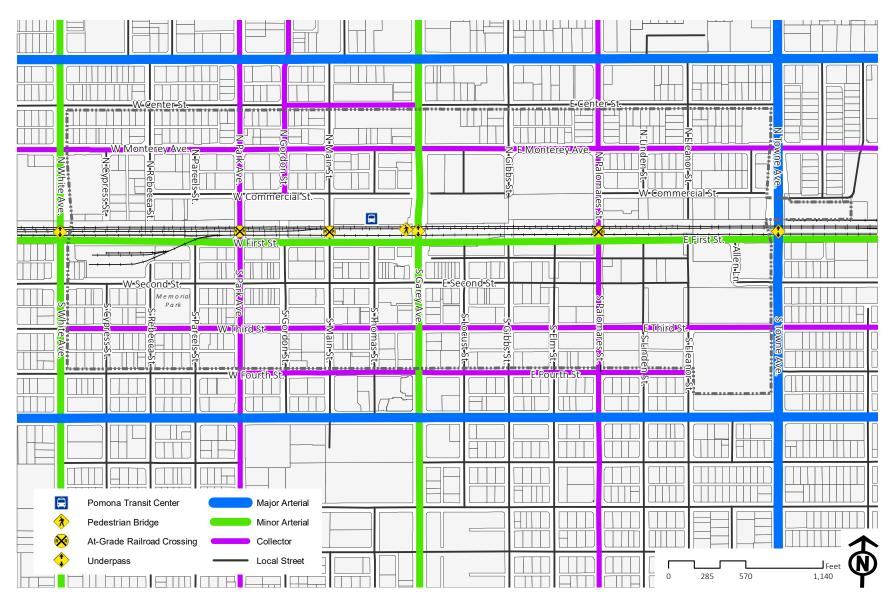


Figure 3-1, Circulation Network

A review of the Specific Plan area revealed a total of 409 driveway locations as shown in Figure 3-2, Driveway Locations. This includes oneway and two-way inbound and outbound driveway locations. Driveways create conflicts between vehicles entering or leaving a street and bicyclists and pedestrians riding or walking along the street. When possible, new driveways should be minimized and old driveways should be eliminated or consolidated. In addition, reducing the number of driveway locations will reduce conflicts and increase on-street parking opportunities. For this reason, the Specific Plan limits the maximum number of driveways/curb cuts with a single building along all streets to one two-lane curb cut or two one-lane curb cuts.

Public Realm Improvements

Streetscape Improvements

The City plans to implement phased streetscape improvements that will contribute significantly to the enhancement of the visual appeal and identity of key areas of the Downtown Pomona Specific Plan. Streetscape improvements have been designed to promote the type of change envisioned by the community by providing attractive and compatible environments for the desired types of new development, as well as for highly valued existing development.

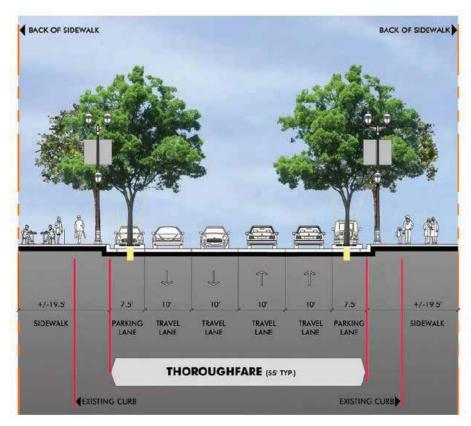
Implementation

Streetscape improvements will be installed and paid for by a combination of public and private investment. New private development in the Downtown Pomona Specific Plan boundaries will include the installation of sidewalk and landscaping improvements between property line and curb. Private investors will also provide payment for the costs of installing public improvements to the street centerline.

Public implementation of streetscape improvements will be staged over time, and as financial resources allow. Public implementation of streetscape improvements within the Downtown Pomona Specific Plan will provide improvements between curbs as well as improvements along public frontage areas of properties that have not yet been improved per the standards contained in this section.

Design

The design of specific streetscape improvements is integrated within the overall vision of Public Realm improvements within the Downtown Pomona Specific Plan. The integration of street design with building disposition and site improvements will result in the emergence of an increasingly cohesive and iconic Downtown Pomona.



Typical Downtown Boulevard Configuration

The following specific streetscape improvements shall be applicable to new development:

- **1. Garey Avenue**—improvements along Garey Avenue between Fourth Street and Center Street.
- **2. Second Street**—improvements to the portion of Second Street between South Main and Gibbs Streets.

Garey Avenue Public Realm Improvements

- **1. Streetscape.** Street improvements will feature an approximately 19.5 foot wide widened sidewalk with no planter strip. Infill street trees are planted in individual tree wells.
- 2. Street Lighting. Double-head/arm pendant boulevard-scale street lighting with double-head/arm post-top pedestrian-scale luminaires located at sidewalks, with a spacing of approximately 56 feet on-center, centered between street trees. The boulevard-scale light source should be located 25-30 feet above finished grade, with double-arms perpendicular to the street centerline; the pedestrian-height luminaires are mounted on double-arms parallel to the street centerline, located 12-14 feet above finished grade, with double-arms parallel to the street centerline.

Streetlight poles shall contain pairs of banner arms with wind relief mounting devices.

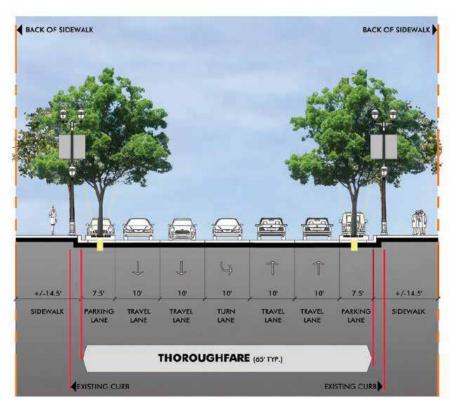
Where needed to supplement intersection lighting, singlearm pendant boulevard-scale street lighting located within the planter strip, at a sidewalk corner, or atop a traffic signal pole. Light source should be located 25-30 feet above finished grade.

Trees within parking lanes on curbed islands are each uplit from both front and rear with in-grade fixtures, on times to shut off after a late night hour designated by the City.

3. Street Trees. At sidewalk tree wells, trees are planted 28 feet on-center.

Parking lane trees are planted in curbed islands between every two parking stalls, within the parking lane at approximately 56 feet on center. Curbed islands are 6.5 feet wide by 8 feet long, offset from the existing face of curb by 12 inches to enable passage of existing drainage. Street tree trunks are asymmetrically positioned within the 8-foot length to allow for dissimilar front and rear parked vehicle overhangs, with the tree trunk centerline set back from face of curb by 3.5 feet at the vehicle front overhang and by 4.5 feet at the vehicle rear overhang.

4. Transit Stops. Special configuration of the streetscape at transit stops will require coordination with bus transit agencies for concept and final design.



Downtown Bouelvard Configuration with Turn Lane



Antique Row on Second Street

Second Street Public Realm Renewal (South Main to South Gibbs)

These four core blocks represent less than half the length of the original nine blocks of the historic 1962 pedestrian mall streetscape improvements. They represent only an initial phase associated with recommended improvements on Garey Avenue.

Improvements are intended only to renew the appearance and physical integrity of the current streetscape and are limited to: refurbishment of existing street lighting, tree uplighting, and ornamental street signs; addition of benches; infill of damages or missing street trees and associated irrigation and planting beds; and rehabilitation of worn, exposed paving or planter wall surfaces. The configuration of existing lanes, parking, paving and drainage will remain unchanged.

1. Street Lighting. Three-head, post-top pedestrian-scale decorative street lighting assemblies with luminaires, arms, and accessories to match boulevard fixtures, to replace existing 3-head decorative luminaires and arms at existing poles—or to also replace existing poles if required, at present locations. The luminaire shall be of a cutoff type with opaque cap, shall have a large prismatic globe lens to diffuse and mitigate glare, and feature capability for interchangeable convention or LED light sources. Paint color applied to metal finishes to match existing streetlight paint color.

Where needed to supplement intersection lighting, single-head/arm pendant boulevard-scale street lighting located at a sidewalk corner or atop a traffic signal pole. Light source should be located 25-30 feet above finished grade.

Existing street trees within raised concrete planters (historic) within parking lane are each uplit by uplight fixtures from within the planters at both front and rear, on timers to shut off after a late night hour designated by the City.

- **2. Furnishings.** Benches with arms may be mounted on the sidewalk-facing side of existing concrete planters. Refurbish and repaint existing decorative street name signs within existing raised concrete planters.
- **3. Pavement Surfaces and Planter Wall Surfaces.** At sidewalk, parking, and travel lane surfaces –replace existing surfaces in disrepair with same materials, or as closely matched as is feasible. Existing exposed vertical planter walls and fountain walls—repair and refurbish cracked, chipped or water materials and surfaces.
- **4. Street Trees.** Where existing street trees within raised planters are damaged or missing, infill with replacement trees of the original species at the largest practical size. Amend or repair planting pits, tree supports, soil media, and irrigation function as required.



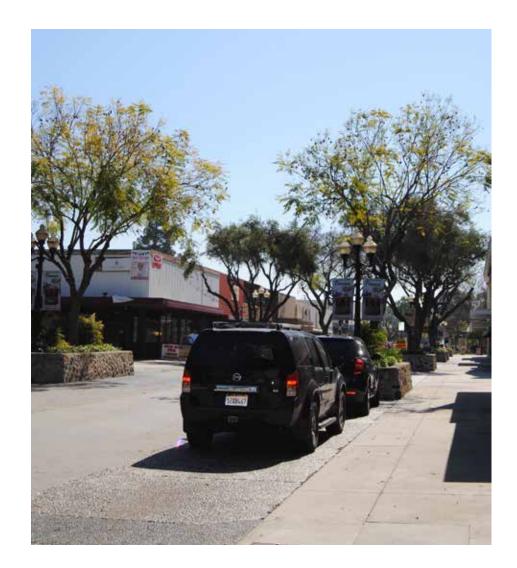
Existing street furnishings



Figure 3-2, Driveway Locations

Circulation Policies

- 3.2.1 Retain the grid circulation system.
- 3.2.2 Accommodate citywide/regional traffic on Holt Avenue, White Avenue and Towne Avenue.
- 3.2.3 Draw citywide/regional traffic into Downtown along Garey Avenue.
- 3.2.4 Provide appropriate pedestrian, bicyclist, and automobile circulation within and between Specific Plan Districts.
- 3.2.5 Accommodate the current pattern of bus stops on Holt Avenue, Garey Avenue, White Avenue and Towne Avenue.
- 3.2.6 Provide landscaped medians to reduce the scale of existing major arterials and other significant roads such as Towne Avenue and White Avenue, as appropriate.
- 3.2.7 Reduce the number of driveway locations. When possible, new driveways should be minimized and old driveways should be eliminated or consolidated.
- 3.2.8 Promote ridesharing opportunities by encouraging the use of drop-off/loading zones.



3.2.4 Bicycle Network

The size, topography and climate of Pomona make it an ideal setting for bicycling with a significant portion of the community within a 10-minute ride from Downtown, as demonstrated in the Mobility and Access Component of the Pomona General Plan where policies are provided for existing and planned bicycle facilities. In addition, the Pomona Active Transportation Plan (ATP) describes detailed direction on how to continue progress toward a better bicycling and walking environment. The ATP does this by proposing a system of bikeways to connect neighborhoods to key activity centers throughout the City, developing support facilities such as bike parking, and education programs, and by identifying recommendations for improving bicyclist safety.

Bicycle facilities are typically divided into three distinct types as described below:

Class I (Bicycle Paths): Class I bikeways are completely separated from motor vehicle traffic, as in the case of an off-street path along a river or railroad corridor. Class I bikeways have varying widths, generally between 8 and 12 feet. Pedestrians are allowed on Class I paths.

Class II (Bicycle Lanes): Class II bicycle lanes are located on streets and allow bicyclists to utilize a separate lane of travel, usually five to six feet wide, separated from motor vehicle traffic by a six-inch white stripe.

Class III (Bicycle Route): Class III bikeways are designated by signs only. Cyclists share the travel lane with motor vehicle traffic on these routes. Some Class III routes have a wide outer curb lane while others carry low volumes of motor vehicle traffic, making a separate bicycle lane or wide curb lane unnecessary.

Class IV (Separated Bikeway/Cycle Track): Class IV separated bikeways are often referred to as cycle tracks or protected bike lanes. Class IV facilities provide for the exclusive use of bicycles, and are separated from vehicular traffic by a physical barrier such as flexible posts, inflexible barriers, or parking.

Planned bicycle facilities are shown in **Figure 3-3**, **Bicycle Network**, and are proposed on the following streets:

North-south:

Palomares Street

East-west:

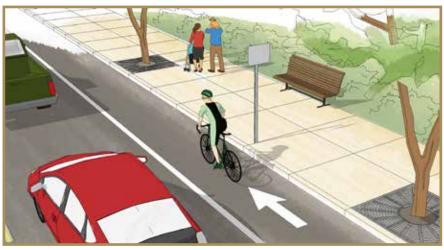
Second Street



CLASS I BIKE PATH



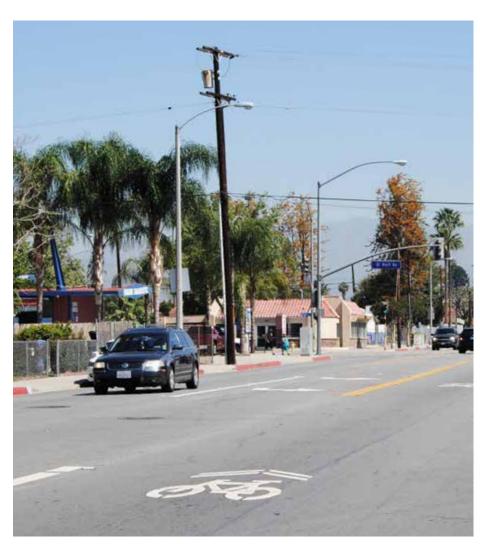
CLASS III BIKE ROUTE



CLASS II BIKE LANE



CLASS IV CYCLE TRACK



Bicycle facility

Recommended Bicycle Facility Treatments

The following bicycling-friendly treatments may be considered along bicycle routes. These treatments are described in detail in Chapter 8 (Design Guidelines) of the ATP.

- Five to six-foot bicycle lanes
- Physically separated bicycle lanes with buffer
- Colored bicycle lanes
- Sharrows
- Accommodation at large intersections and freeway interchanges
- Signage and wayfinding



Figure 3-3, Bicycle Network

To support bicyclists and the use of the bicycle network, establishing a bicycle rack program for the installation of bicycle parking facilities at key locations such as transit facilities, civic uses, and other bicycle generators. This strategy is recommended to complement private development bicycle parking requirements established in Chapter 2, Land Use and Urban Form. Identified locations for bicycle parking include:

- Pomona Transit Center
- Pomona Post Office (580 W. Monterey Avenue)
- The Glass House/Fox Theater
- Western University

It is recommended that the City enhance the accessibility of transit to bicyclists by installing secure short-term bicycle parking in the form of additional bicycle racks and long-term bicycle parking in the form of bicycle lockers. Bicycle lockers allow potential transit patrons to ride to a transit station and board transit service without needing to take their bicycle along. These should be acquired and established at the Downtown Pomona Metrolink station. In addition, support facilities such as restrooms, showers, and changing facilities should be provided at this location to further facilitate bike commuter comfort and willingness to bicycle to and from these facilities.

As plans become more fully developed for bicycle facilities improvements, coordination with programs (such as Safe Routes to School) and interested stakeholders (such as Pomona Valley Bicycle Coalition and Pomona Police Department) should be facilitated.



Figure 3-4, Bicycle parking locations

Bicycle Network Policies

- 3.2.9 Design and build recommended bicycle facilities:
 - Park Avenue Bike Route (north of 3rd Street), Bike Lane (south of 3rd Street)
 - Palomares Street Bike Lane
 - Monterey Avenue Bike Route
 - 2nd Street Bike Route (east of Gibbs Street), future facility type to be determined (between Gibbs and Towne Avenue)
- 3.2.10 Provide additional bicycle racks at the following locations:
 - Pomona Transit Center
 - Pomona Post Office (580 W. Monterey Avenue)
 - The Glass House/Fox Theater
 - Western University
- 3.2.11 Consider bicycle lockers and supporting restrooms, showers, and changing facilities at the Pomona Transit Center.





Bicycle parking

3.2.5 Pedestrian Network

Creating an inviting and safe environment for walking in Downtown Pomona is an important community goal. The pedestrian network is addressed in this section and is complemented by public realm recommendations under Section 3.3 Placemaking, that describe streetscape design and amenities to support pedestrian activity.

Sidewalks and crosswalks are the primary facilities for pedestrian travel in the Specific Plan area. An inventory of existing gaps in the sidewalk network is included in the Pomona Active Transportation Plan (ATP) and is graphically shown in **Figure 3-5**, **Pedestrian Network**. Existing improvements include current sidewalks and the pedestrian bridge over the railroad tracks by the Pomona Transit Center just west of N. Garey Avenue.

Crosswalks are typically located at intersection locations controlled by stop signs and traffic signals, additionally shown in **Figure 3-5**, **Pedestrian Network**. Existing crosswalks are present by law at all intersections, whether marked or unmarked, unless the pedestrian crossing is specifically prohibited. Application of marked crosswalks is inconsistent and includes a combination of unmarked, marked, textured and ladder style crosswalk pavement markings. Consistent with the recommendations in the Pomona ATP, improvements such as zebra-stripe crosswalks, advanced stop bars, high-visibility continental or diagonal crosswalk marking patterns should aim to increase pedestrian visibility and driver awareness. Typical intersection improvements are illustrated in **Figure 3-6**, **Typical Intersection Improvements**.



Sidewalks to accommodate pedestrians, dining, and bicycle parking



Landscaped medians



Figure 3-5, Pedestrian Network, Source: Urban Crossroads

Typical Intersection Improvements

Potential enhancements to intersections include the following elements to be further tailored based on individual intersection needs:

- Zebra-stripe crosswalks to all crossings
- Pedestrian countdown signals to all crossing
- Audio signals to all crossings
- Advanced stop bars
- Advanced yield markings
- Advanced yield signs
- **Bulbouts**



Figure 3-6, Typical Intersection Improvements , Source: Urban Crossroads

Pedestrian Network Policies

- 3.2.12 Construct missing sidewalk segments through the City's Capital Improvement Program and/or developer requirements.
- 3.2.13 Provide sidewalks with adequate widths (10-foot minimum on primary pedestrian corridors, 8-foot minimum on secondary pedestrian corridors and narrower sidewalks where it is required to be consistent with historic sidewalks) to allow for pedestrian travel, dining and auto transitional zones.
- 3.2.14 Upgrade crosswalks treatments with zebra-stripe crosswalks, advanced stop bars, high-visibility continental crosswalk marking patterns. Priority intersections include:
 - Pomona Transit Center
 - 2nd Street and Garey Avenue
 - 3rd Street and Garey Avenue
 - 4th Street and Garey Avenue
- 3.2.15 Provide pedestrian links and continuity between public gathering areas and other areas of interest, parking areas and attractions.

3.2.6 Transit Network

Public transportation in Downtown Pomona consists of both bus and rail service and can be characterized as the most transit-rich environment in the city. The Pomona Transit Center, centrally located within the Specific Plan area, provides a unique opportunity to promote transit-oriented development and land uses that benefit from access to regional transit service. The overall transit goal is to provide greater opportunity for residents and visitors to use public transit options by incorporating appropriate infrastructure in development plans to support transit as well as amenities to make these options appealing to users.

Regarding bus transit service, Foothill Transit, Omnitrans, and Los Angeles County MTA buses all operate within the City. Since the Pomona Transit Center is located centrally within Downtown Pomona, the Specific Plan area enjoys access to regional transit service from approximately ten routes according to schedules from Foothill Transit and Omnitrans. MTA operates two bus routes within Pomona connecting Downtown Pomona, Cal Poly Pomona, and the Lanterman Developmental Center with Downtown Los Angles, Union Station, Cal State Los Angeles, and Mount San Antonio College. Refer to Figure 3-7, Transit Network, illustrating existing transit routes and bus stops within the Specific Plan area.

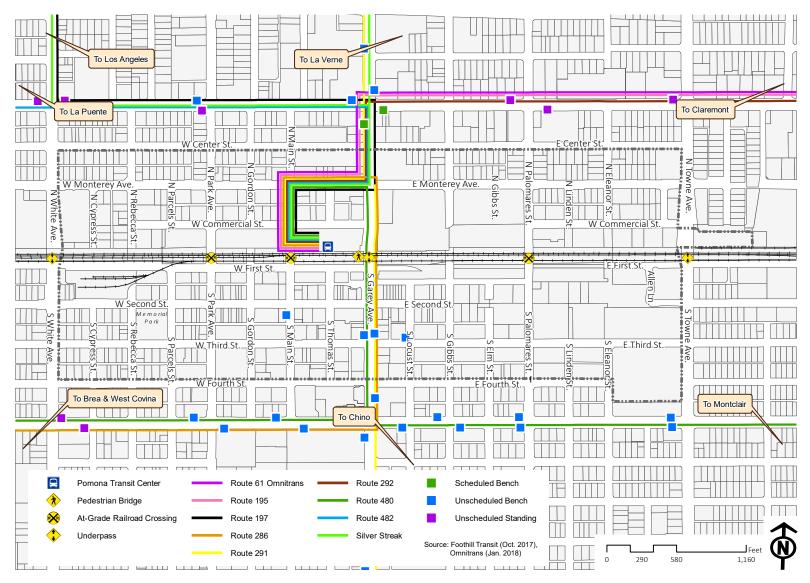


Figure 3-7, Transit Network, Source: Urban Crossroads

In addition to the transit services described above, the Pomona Valley Transportation Authority offers transportation programs to the area including the Pomona Valley Get About program for seniors and disabled and the Pomona Group Service which is open to the general public and available to groups of six or more rides. Further, Access Paratransit provides regional paratransit services in Los Angeles County for all locations within ¾ of a mile of an active bus line. This includes the entire City of Pomona.

In addition to the bus service described above, the Pomona Transit Center is served by rail service from Metrolink and Amtrak. The Metrolink Riverside line serves the Pomona Transit Center with destinations between Downtown Los Angeles and Riverside. Six trains stop in Pomona daily, with trains arriving every 30 minutes during the peak hour and peak direction, but there is virtually no "off-peak" service. No weekend trains currently operate on the Riverside line. Amtrak trains that stop at the Pomona Transit Center include the Sunset Limited and the Texas Eagle with service three times weekly in each direction. Providing rail service between Los Angeles and New Orleans, and between Los Angeles and Chicago, respectively.

Future transit infrastructure and service expansions are envisioned in the City's General Plan and by the transit agencies that serve the City. The MTA Long Range Transportation Plan envisions an expanded regional transit system that may help sustain the increasing mode share of transit in Pomona. By providing greater frequencies and better transit access to regional destinations, transit use may become a viable option for travel. Better coordination between bus and existing Metrolink rail service also offers the opportunity to improve the appeal of transit in Pomona.

Planning for future transit services for Downtown Pomona requires an understanding of existing services, those that are planned, and those that could be developed, and integrating those services with the anticipated land use patterns. The primary role of the Specific Plan is to establish the planned land use/development pattern in support of existing and future transit services. As transit service planning and implementation is ultimately the responsibility of transit agencies and other regional planning organizations, a detailed transit plan is beyond the scope of the Specific Plan. However, identifying goals and strategies for encouraging transit use and mandating the consideration or inclusion of significant elements is appropriate for the Specific Plan and can help regional service providers maximize service potential within Downtown Pomona.

The following section provides guidance in developing services that integrate existing and new transit services with redevelopment in the Specific Plan area. Transit recommendations for adjacent Holt Avenue (where future Bus Rapid Transit may be made available) are addressed in the Pomona Corridors Specific Plan. Flexibility in implementation is provided in order to adapt to changes in potential services and development scenarios.



Transit Network Policies

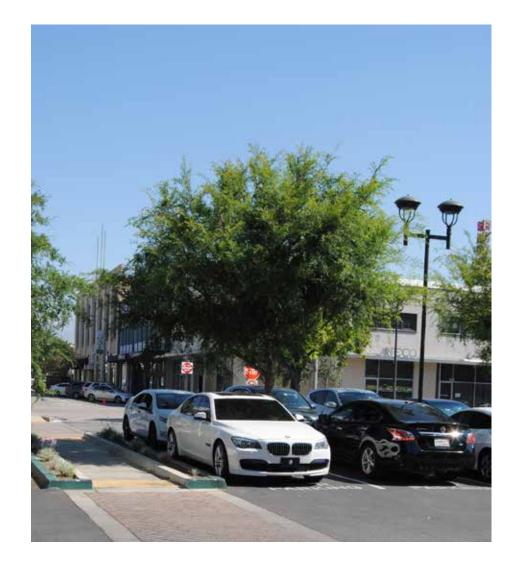
- 3.2.16 Incorporate transit stops and amenities into the public frontage areas along primary corridors (such as Garey Avenue) and within development along secondary arterials such as White Avenue and Towne Avenue. Certain transit services, such as bus rapid transit or express service may necessitate including a very limited number of stops within the mainline section of Holt or Garey Avenue to meet performance specifications for the individual service point at the Downtown Metrolink station and transit center.
- 3.2.17 Consider expansion of the Pomona Transit Center should increase service require it.
- 3.2.18 Continue to coordinate with local transit agencies to maintain a high level of bus (or equivalent) service running along Garey Avenue and connecting to the North Metrolink station and the Downtown Pomona Transit Center.

3.2.7 Parking Strategies

Parking Setting

Parking for the Downtown is either self-contained or located within parking facilities that serve groups of buildings. The majority of the Downtown area south of First Street, including Western University, has shared parking provided in public parking facilities with additional on-street parking accommodations. Chapter 2, Land Use and Urban form, establishes parking requirements for individual developments, while this section describes public parking strategies. The Specific Plan allows for the conversion of parking lots in this area into structured parking where appropriate.

Figure 3-8 depicts existing public parking lots in and near the Specific Plan area. Additional spaces can be provided by increasing the heights of existing parking structures or converting parking lots to structured parking as the parking demand grows over time.



Future Considerations

Changing economic and demographic conditions will continue to influence future parking needs. Self-driving cars, parking guidance, peer-to-peer vehicle sharing, telecommuting, transportation network companies and other forward-looking technologies will continue to shape future parking demands.

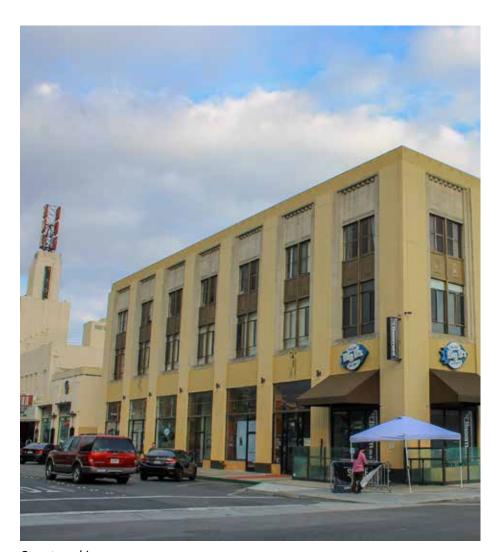
The recent rise in popularity of Transportation Network Companies (TNCs) such as Uber and Lyft, has the potential to significantly change the future parking demands in the Downtown. The TNC model, in which users can request rides through their smartphones, provides on-demand transportation in personal vehicles with direct point-to-point trips. The use rather than ownership model of TNCs may over time substantially reduce the need for dedicated parking. In addition, on-demand transportation increases the need to promote accessible drop-off and pick-up areas at key locations in the Downtown.

TNCs represent only one technology that is changing traditional parking demands. Other smart parking technologies will allow the City to better manage parking availability, occupancy and enforcement. Using smart vehicle detection sensors, smart parking management systems are available allowing the City to actively manage parking resources real-time. This smart parking management approach streamlines enforcement and reduces traffic congestion related to vehicles searching for available parking.

The City will prepare a parking management plan to create a long-term strategy that will address future parking needs within the Specific Plan area. The parking management plan should also prioritize TNC accessible drop-off and pick-up areas. To facilitate better utilization of existing parking facilities, parking strategies should include wayfinding improvements and application of smart parking technologies to inform users of available spaces and pricing.



Figure 3-8, Public Parking, Source: Urban Crossroads



Street parking

Parking Policies

- 3.2.19 Prepare parking management strategy for Downtown.
- 3.2.20 Incorporate improved wayfinding and technology to optimize utilizatino of existing parking supply.
- 3.2.21 Periodically evaluate funding sources and mechanisms for expanded/updated parking facilities or parking structures.
- 3.2.22 Evaluate parking lot design and parking lot management to ensure continued security for users.
- 3.2.23 Pursue the expansion of on-street parking throughout Downtown Pomona.

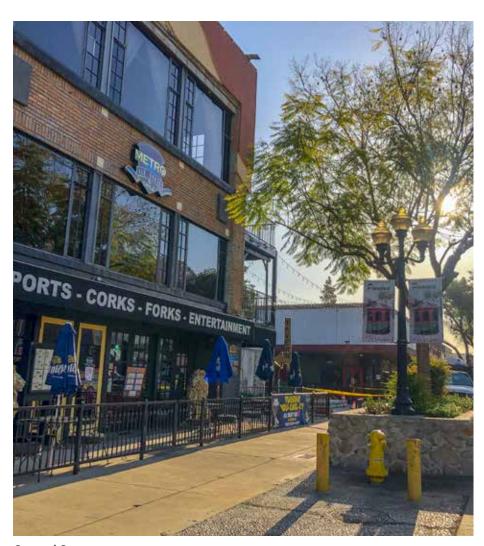
3.3 Placemaking

3.3.1 Vision

Downtown Pomona possesses a distinct identity within the larger landscape of the City of Pomona. It will serve as a diverse and dense hub of activity for businesses, families, and visitors that also embraces and encourages public art opportunities and preserves historic and cultural resources. Defining what makes Downtown Pomona unique is inherently a collective activity that involves the feedback and participation of many stakeholders. To that end, the process of placemaking, which establishes connections between people, community assets and public space, serves as a useful vehicle in establishing Downtown Pomona as a unique destination where people want to work, live, and play.

The vision of this section is twofold: first, to illustrate existing pathways and recommend new ones for stakeholders designing and pursuing placemaking strategies in Downtown Pomona; second, to provide guidelines and standards regarding historic preservation, temporary uses, privately owned public open space, and streetscapes in the Downtown Pomona Specific Plan.





The Downtown Pomona Specific Plan area is generally geographically divided into northern and southern sections by the Union Pacific Railroad right-of-way and into eastern and western sections by Garey Avenue. Historically, a relationship between bridging these portions of Downtown has not been established. In exploring placemaking strategies, the City of Pomona and stakeholders should consider improving key north-south connections, including at-grade crossings along Park Street, Main Street, and Palomares Street, underpass crossings at Garey, White, and Towne Avenues, and east-west connections running between Garey, White, and Towne Avenues, including Center, Monterey, First, Second, Third, and Fourth Streets. Activating these connections will allow for a more cohesive, connected Downtown Pomona.

Second Street

PUBLIC REVIEW DRAFT JUNE 2019

3.3.2 Downtown Placemaking Standards & Guidelines

Historic Landmarks

Downtown Pomona includes several designated historic landmarks and the Edison Historic District and in general is home to a wealth of historic and cultural features with distinct architecture and streetscapes. Future development of mixed-use, high density uses must remain sensitive to existing historic resources and should be designed to highlight those attributes when feasible. Parcels located within the Downtown Pomona Specific Plan will be subject to the standards and guidelines of Pomona Zoning Code Section .5809-13, Historic Preservation. Figure 3-9, Historic Areas and Landmark Structures illustrates historic resources in and near the Specific Plan.



Fox Theater



Renovated YMCA

Historic preservation within Downtown Pomona should consider the following guidelines:

- 1. Proposed demolition of pre-1945 structures that are identified as "contributing" in the City of Pomona historic resources surveys may require the preparation of a historic survey conducted by a certified architectural historian, at the discretion of the Development Services Director.
- 2. The City of Pomona should consult local historic groups and other interested stakeholders in establishing a list of historic resources that should be incorporated into a broader placemaking vision for Downtown Pomona.
- The design of new development located in Downtown Pomona neighborhoods with existing, historic housing stock should consider incorporating the architectural principles of these houses.
- 4. Whenever possible, existing historic landmarks within Downtown Pomona should be opened up to the general public through events and activities that increase community exposure to unique landmarks and history.
- 5. Placemaking strategies involving historic resources should be diverse and inclusive and represent the history and heritage of all people.



Figure 3-9, Historic Areas and Landmark Structures

Art in Public Places Program

The City of Pomona's Art in Public Places Program, adopted in 2011, encourages the development of public art citywide. As indicated in the Art in Public Places Manual, the mission of this program is to "increase citizens' appreciation of art, to improve the quality of life, and to enhance and identify Pomona as a unique community." The following principles should guide public art strategies in Downtown Pomona:

- 1. The vision of the Arts Colony which established Downtown Pomona as a place for the cultural arts extends to the entire boundary of the Downtown Pomona Specific Plan.
- 2. The Cultural Arts Commission and its Citizen's Advisory Committee, in conjunction with Development Services, should establish a strategic vision for implementing public art within Downtown Pomona, including identifying appropriate sites for the placement of murals and other public art.
- 3. The Art in Public Places program should increase understanding and enjoyment of art in Downtown Pomona and encourage interaction among residents, business owners, art patrons, and visitors across all socioeconomic and ethnic groups.
- 4. Downtown Pomona should serve as a destination that actively encourages an environment for artistic creativity and expression, which includes events, activities, and other initiatives.
- 5. All future development projects in Downtown Pomona should be encouraged to pursue public art on site that is accessible and visible. Development projects that also include a privately owned public space requirement should be designed in coordination with any Art in Public Places requirement.



Figure 4-6, Examples of Public Art

Public Property and Right-of-Way

The City of Pomona currently provides two pathways for stakeholders to activate public spaces in the city. The first is a Special Event Permit, issued by the Community Services Department for any event or activity taking place on public property or in the public right-of-way. The second is an Outdoor Dining Permit, issued by the Public Works Department for any restaurant that wishes to provide outdoor dining in the public right-of-way.

In an effort to further activate the public realm in Downtown Pomona and provide stakeholders additional opportunities to make use of unutilized or neglected spaces, the following uses are encouraged across all public property and public right-of-way within the Downtown Pomona Specific Plan boundary:

- **1. Parklets.** A mini-park within the street right-of-way which extends the sidewalk, usually created by eliminating one or two parking spaces. May include a variety of uses including seating, tables, planting, and play elements.
- **2. Temporary Pedestrian Plazas.** Temporary public open space created by closing a portion of the street to vehicular traffic and introducing street furniture and programming.
- **3. Street Furniture.** Chairs, tables, and benches placed on sidewalks or other public right-of-way. Placement can be temporary or permanent, fixed or movable, depending on location and use. Businesses with active storefronts, such as restaurants or cafes, can sometimes place seats and tables in the public realm outside of their business as part of an outdoor dining permit.
- **4. Active Alleys.** An alley comfortable for pedestrians and people on bicycles, as well as for vehicles. A green alley is built with materials and features that capture stormwater, reduce heat, and provide a more comfortable and aesthetically pleasing environment.
- 5. **Pop Up Music.** Concerts and other musical activities to enliven spaces for transit riders and pedestrians.

The City should consider implementing a series of **Public Realm Activation Guides and Applications for the Downtown Pomona Specific Plan** to establish new pathways for stakeholders to activate public property and public right-of-way.













Figure 4-5, Primary Streetscape Furnishings

Privately Owned Public Open Space (POPOS)

In addition to the City's parks and recreations facilities, privately owned public open space (POPOS) may be provided through development projects pursuing a development incentive (as defined in Urban Form—Incentives, Section 2.2.5.4.3). The following guidelines shall apply to all cases in which a development includes POPOS:

- 1. POPOS refers to spaces dedicated to public use and enjoyment that are also owned and maintained by private property owners.
- 2. POPOS should be open and inviting and visible to the general public, with clear sightlines and paths connecting to the public right-of-way.
- 3. POPOS should be located at the same elevation as the sidewalk and enhance pedestrian circulation, or, if at a higher elevation, provide visible access from the ground floor.
- 4. POPOS should be safe and secure with easily accessible paths for ingress and egress and sufficient lighting.
- 5. POPOS should be comfortable and engaging through the provision of amenities such as well-designed, comfortable seating, greenery and trees, and open areas.

Streetscapes

Streetscapes refer to the natural and built fabric of the street with a focus on the design quality of the public realm and its visual effect. The following components should be considered in the design and implementation of streetscape improvements carried out by Public Works. If streetscape improvements are proposed by stakeholders, it shall be subject to review and approval of the Public Works Department in coordination with Development Services.

Lighting & Furnishings

Site furnishings create a more comfortable and inviting pedestrian experience, and lighting improves safety and pedestrian circulation. Proposed lighting and furnishings should incorporate the following principles:

- 1. Outdoor furniture and hardscape such as trellises, planters, benches, bicycle racks, trash receptacles, etc., shall be provided within the public realm, and shall be coordinated in terms of colors, materials and design to reinforce a cohesive project identity.
- 2. Outdoor furniture shall be provided in public gathering spaces to encourage pedestrian activity. Design of materials and colors of outdoor furniture and hardscape elements shall complement surrounding building architecture.

- 3. Illumination shall be provided for entrances of buildings, in storefronts, parking areas, and pedestrian walkways to promote convenient, safe and easy identification.
- 4. Pedestrian-scale decorative light fixtures are encouraged. Lighting should highlight architectural elements and details.
- 5. The use of a variety of different lighting types and intensities at entries, public gathering areas, parking lots and other areas where evening activity occurs will help to create an exciting nighttime environment.
- 6. Public street lights should follow the adopted antique street light policy.

Tree Palette

A major unifying element of the Downtown design theme is the pattern of street trees. As part of the development of a City of Pomona Street Tree Planting Plan, the Public Works Department, in conjunction with the Parks and Recreation Commission, should develop specific tree palettes for major and minor streets in Downtown Pomona.

Intersections & Crossings

Areas where multiple modes of travel mix, including pedestrians, cyclists, and automobiles. In designing or improving intersections and crossings in Downtown Pomona, the following examples should be considered, among other best practices:

- 1. Provide crosswalk markings that identify a safe path for pedestrians to cross while also signaling to motorists to prepare to yield or stop.
- 2. Provide decorative pavement materials using different colors, textures, and patterns to distinguish crosswalk markings in different environments, which also helps to identify unique places within Downtown.
- 3. Provided raised crosswalks where feasible to provide pedestrian walkways at the same level of sidewalks, and which allows for traffic calming.
- 4. Provide corner bulbouts as curb extensions at intersection corners which improves vehicular turning movement safety and shortens pedestrian crossings.
- 5. Provide bicycle pavement markings to alert bicyclists and motorists of each other's presence as they approach intersections.

Wayfinding

Public signage in the right-of-way that helps orient pedestrians, cyclists, and automobiles around Downtown Pomona. Wayfinding initiatives should incorporate the following principles:

- 1. Signage should "explain the environment" around an individual and provide clear and accurate information to navigate.
- 2. Signage should only contain information that is relevant to the context of the area and not overburden an individual with unnecessary information.
- 3. Signage should be provided for local resident and transit rider experiences by providing information on "first mile-last mile" solutions, such as proximity to Metrolink, a bus station, bicycle stations, or other modes of public transit, as well as historic and cultural landmarks.
- 4. Signage should also be provided for visitor and tourist experiences by providing information on businesses, parking areas, and notable landmarks.
- 5. Signage should relate in design to adjacent streetscape improvements such as site furnishings and lighting.

The City of Pomona should consider implementing a Complete Streets Manual for the Downtown Pomona Specific Plan which provides strategies for implementing best practices in streetscape design. Two examples of streetscapes that should be considered for improvements include First Street between White and Towne Avenues, which lacks critical pedestrian and bicycling networks and building frontages facing the railroad, and Second Street between South Main and South Gibbs, which reflects the original 1962 pedestrian mall and the historic origins of the Arts Colony.

3.4 Infrastructure and Utilities

3.4.1 Introduction

Infrastructure and public services are essential to the success of the Downtown Pomona Specific Plan. Topics include water, wastewater, storm drainage, and dry utilities as well as public services including police protection, fire protection, schools, parks and recreation, and solid waste. This chapter describes infrastructure and public services and relevant policies that support the Specific Plan area.

3.4.2 Water

Water service within the City is provided by the Pomona Public Works Department with supply sources including groundwater, treated surface water, imported water and water conservation. Groundwater is the primary source of water supply for the City, supplemented by imported water from the Metropolitan Water District of Southern California (Metropolitan) via the Three Valleys Municipal Water District (TVMWD), local surface water from the San Antonia and Evey Canyon watersheds, and a small percentage of non-potable, recycled water.

Regarding water conservation, the City is signatory to the Memorandum of Understanding (MOU) regarding Urban Water Conservation in California and is a member of the California Urban Water Conservation Council (CUWCC). As a signatory, the City has pledged to implement 14 conservation Best Management Practices (BMPs). These BMPs include: water survey programs, residential

plumbing retrofit, system water audits, landscape conservation programs, public and school education programs, conservation pricing, and a low-flush toilet replacement program. In addition to the BMPs that are part of the MOU, the City plans to implement additional conservation programs to further discourage water waste and overuse.

Water Policies

- 3.4.1 Reduce water consumption through a program of water conservation measures, such as use of recycled water, water-saving features, and drought-tolerant landscaping.
- 3.4.2 Require that water saving devices, as required by the California Plumbing Code, be installed in all residential, commercial, industrial and institutional facilities within the Specific Plan area. Such devices are capable of reducing the amount of water used indoors, resulting in substantial wastewater flow reductions.
- 3.4.3 Explore opportunities to provide recycled water to irrigate all parks, plazas, community facilities, linear parks, landscaped front yards and buffer zones. Where reasonable and feasible, commercial uses, schools and non-residential mixed-use developments should provide with dual plumbing to enable indoor recycled water use for non-potable uses to the extent feasible.

The City's 2010 Urban Water Management Plan indicates that, due to its existing water rights and groundwater storage agreements, an adequate supply of water should be available, with normal conservation efforts, for projected demands through 2035. As individual development projects are initiated within the Specific Plan area, localized improvements to the water system would be required as part of project design and review.



Recycled water used for irrigation

3.4.3 Sanitary Sewer

Wastewater service within Pomona is provided by the City's Public Works Department, and collected wastewater is treated by the Los Angeles County Sanitation Districts (LACSD). The LACSD is a conglomerate of 23 independent special districts responsible for collecting, treating and recycling, and disposing of wastewater and industrial wastes. The City's wastewater is treated and disposed of at the LACSD Pomona Water Reclamation Plant located at 295 Humane Way in Pomona.

LACSD has adopted ordinances over time establishing legal mechanisms to protect and finance operations and facilities, including the Industrial Wastewater Discharge Permit, Connection Fee, and Discharge Programs. The Connection Fee Program requires all new uses of the LACSD sewerage system, as well as existing uses that significantly increase the quantity of strength of their wastewater discharge, to pay their fair share of costs for providing additional conveyance, treatment, and disposal facilities.

Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR. Existing treatment plant capacity and infrastructure can accommodate planned future growth. As individual development projects are initiated within the Specific Plan area, localized improvements to the wastewater system would be required as part of the project design and review.

3.4.4 Storm Drainage

The Specific Plan area, as well as the majority of the City of Pomona, lies within the eastern boundary of the San Gabriel River Watershed. Because Pomona is largely built out, water quality is mainly affected by the City's storm drain system which is comprised of gutters and storm drains designed to prevent flooding by moving rain water away from city streets and directly into local rivers (which flow to the ocean). The southern portion of the City (which includes the Specific Plan area) discharges to San Antonio Creek/Chino Creek which is a tributary to the Santa Ana River. Pomona does not lie within a 100- or 500-year floodplain, as delineated by the Federal Emergency Management Agency (FEMA).

The primary regulatory control for the protection of water quality is the Federal National Pollution Discharge Elimination System (NPDES) permit administered by the State Water Resources Control Board (SWRCB). Further, Pomona is within the jurisdiction of the Los Angeles Regional Water Quality Control Board (RWQCB). The SWRCB and Los Angeles RWQCB establish requirements prescribing the quality of point sources of discharge and establish water quality objectives. In addition, the City of Pomona protects water resources by reducing the impact of pollutants from urban runoff through implementation of its Storm Water Pollution Prevention Program as required by the NPDES permit program. Further, the Pomona Municipal Code, Chapter 18 (Environment), Division 3 (Discharge Regulations and Requirements) sets forth regulations associated with stormwater.



Flow-through planters collect stormwater and filter sediment and pollutants

Development envisioned under the Specific Plan is primarily infill in nature. Therefore, only an incremental expansion in the quantity of net new impervious surfaces is expected as a part of future development. In compliance with federal, state, regional and local requirements, contractors constructing new development or redevelopment projects are required to implement best management practices (BMPs). As individual development projects are initiated within the Specific Plan area, localized improvements to addressing storm water and water quality would be required as part of the project design and review.

Storm Drainage Policies

- 3.4.4 Provide storm drain infrastructure to adequately serve new development and meet City standards.
- 3.4.5 Coordinate site design and building systems to remove pollutants from stormwater runoff prior to discharge to the storm drain system or receiving waters by creating a project that attempts to mimic a natural hydrologic regime through the use of site design and treatment strategies including, but not limited to:
 - Landscaped Setback Area/Open Space
 - Detention Basin/Pond
 - Retention Basin/Pond
 - Vegetated Swale/Strip
 - Rain Garden
 - Landscaped Tree Well
 - Grated Tree Well
 - Permeable/Pervious Paving
 - Waterscape
 - Rainwater Harvesting/Vault

3.4.5 Dry Utilities

Electricity Service

Southern California Edison provides electric service in the Specific Plan area. The current transmission and distribution system is anticipated to meet the demands of the proposed land uses. The City requires undergrounding of utilities pursuant to Chapter 62 – Utilities of the Municipal Code (Section 62-31. – Undergrounding of Lines).

Natural Gas

The Southern California Gas Company provides natural gas service for the Specific Plan area. SoCal Gas has facilities in the area and service would be provided in accordance with SoCal Gas policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made (project-by-project basis). In addition, energy conservation measures required by applicable energy conservation regulations (e.g. the California Green Building Code) will improve efficiency of long-term energy use for the Specific Plan area.

Telecommunications

Telephone service is provided by Verizon and internet/cable service is provided by Frontier Communications in the Specific Plan area. The existing system is anticipated to meet the demands of the proposed land uses.

3.4.6 Police Protection

The City of Pomona Police Department (PPD) provides local police services for Pomona. The PPD provides services in crime investigation, offender apprehension, community awareness programs, traffic control, and other services. Seven facilities provide police services in Pomona. Of these facilities, three provide first-response services and include the Main Station at 490 W. Mission Boulevard, the Traffic Bureau at 100 W. Commercial, and the Aero Bureau at 1905 McKinley. Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR where policies are provided to reduce crime and ensure law enforcement as the City experiences future growth.

Police Policies

3.4.6 Consider public safety in the design of new development and public spaces by promoting the integration of Crime Prevention Through Environmental Design (CPTED) principles for new development and public spaces. The three key principles of CPTED are: Natural Surveillance, Natural Access Control, and Territorial Reinforcement.

3.4.7 Fire Protection

Fire protection services are provided by the Los Angeles County Fire Department (LACFD) and include fire, rescue, and hazardous materials prevention and emergency services. The LACFD uses national guidelines for response time targets which consist of five minutes for the first arriving unit for fires and basic life support and eight minutes for advanced life support (paramedic) in urban areas. Pomona is part of the LACFD Division III and the nearest fire department facility is Station 181 – Division and Battalion Headquarters located at 590 S. Park Avenue near the southern boundary of the Specific Plan area.

Any new development that would occur with the Specific Plan area would be required to comply with all applicable federal, state, and local regulations governing the provision of fire protection services, including adequate fire access, fire flows, and number of hydrants. LACFD has adopted the 2016 California Fire Code with certain local amendments. These project-specific requirements include construction standards in new structures and remodels, road widths and configurations designed to accommodate the passage of fire trucks and engines, and requirements for minimum fire flow rates for water mains.

3.4.8 Schools

The Specific Plan area is located within the Pomona Unified School District (PUSD) boundaries. Existing facilities include elementary, middle and high schools as well as opportunities for higher-level education. In the Specific Plan area, additional educational institutions range from The School of Arts and Enterprise (a charter school located at 375 S. Main Street and 925 N. Garey Avenue) and Western University of Health Sciences located in the southeastern quadrant of the Specific Plan area. Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR where policies are provided to address potential increases in student generation, including payment of the State Department of Education Development fee in conformance with AB 2926, which was enacted to provide for school facilities construction, improvements, and expansion.

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3.4.9 Parks and Recreation

The City of Pomona owns and operates a number of developed parks within the City. Two parks are located in the Specific Plan area: Centennial Park and Memorial Park. Pomona Civic Center Park is located just south of the Specific Plan area and several planned additions to this facility are identified in the General Plan Open Space Network.

As indicated in the 2014 General Plan, the City is striving to achieve a target of three acres of parkland per 1,000 residents. Municipal Code Section 70.69 Parks and Recreation Improvement requires dedication of park lands or payment of in-lieu fees, or a combination thereof, for the construction of new dwelling units. Funds collected are solely for the construction of park improvements. In addition, the Parks and Recreation Division in the Community Services Department provides recreation programs and services to the citizens of Pomona. As the City is largely built out, creative approaches are needed to increase available park space and improve the existing parkland system.

Policies provided below will support expansion of parks, recreation and public spaces within the Specific Plan area. Please refer to Section 3.3, Placemaking for additional discussion and policies regarding park space in the Downtown area.



Thomas Plaza

Parks and Recreation Policies

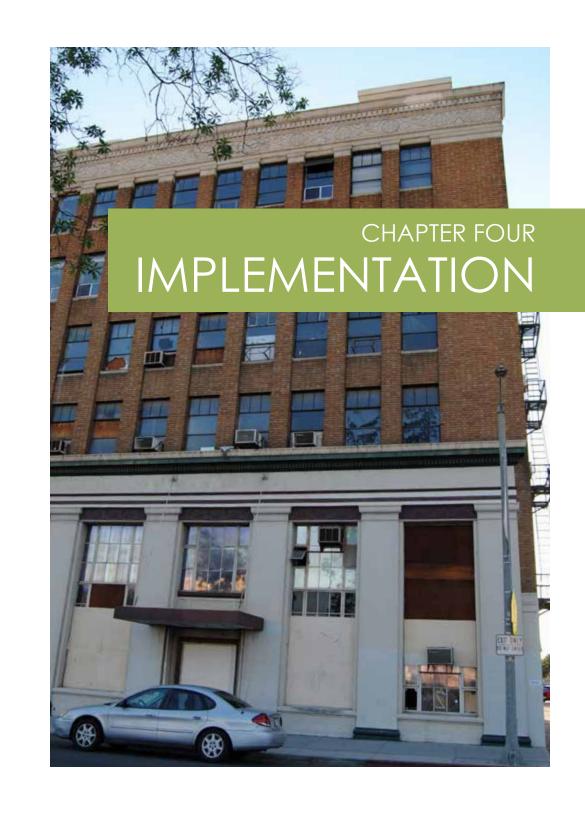
- 3.4.7 Support the development network of multi-purpose facilities to house visual and performing arts activities, such as exhibition, studio/class, performance, and theater/lecture space.
- 3.4.8 Consider allowing in-lieu fee payments based on the size of a project in higher intensity districts (such as Downtown) or for small developments.
- 3.4.9 Explore opportunities for adaptive reuse of historic or existing structures for performing and visual arts venues, as exemplified by the recently restored Fox Theater.

3.4.10 Solid Waste

The City of Pomona Public Works Department provides trash, recycling and special pickup services for the single-family residences, duplexes, triplexes and some fourplexes. Community cleanups coordinated by the City provide periodic neighborhood drop-off of bulky items for residents only. Franchise commercial waste haulers provide trash and recycling service for most fourplexes, all apartments with five or more units, as well as all commercial, governmental, and industrial facilities. Commercial haulers have non-exclusive, competitive franchise agreements and currently authorized franchise haulers include Athens Services, Burrtec Waste Industries, Valley Vista Services, and Waste Management.

Solid Waste Policies

3.4.10 All new development shall participate to the maximum extent practical in solid waste source reduction and diversion programs.



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4.1 Introduction

This chapter outlines the implementation program for the Specific Plan. The implementation program includes the following components:

- An overview of the ways the Downtown Specific Plan can facilitate economic investment in the plan area;
- A review of existing economic conditions influencing current and future development potentials in Downtown Pomona;
- · A summary of the types of new development that are likely to be successful in the Downtown area;
- Recommended high-priority action items for Specific Plan implementation;
- · A review of potential funding sources/mechanisms for implementation of key Specific Plan initiatives;
- An overview of the State of California's Community Revitalization and Investment Authority legislation. This legislation provides local
 jurisdictions with many of the development tools previously associated with redevelopment agencies and may be a useful mechanism
 for implementation of the Specific Plan; and
- Identification of methods to leverage Opportunity Zone legislation and incentives.

Note that within this chapter there are numerous strategies and other entries that are universally applicable to many different communities. However, the ways in which they are ultimately adapted to Pomona will be somewhat unique and will depend on numerous specific conditions including available resources, the interrelationship of various programs, preferences of the applicable stakeholder groups, and other considerations. The City should consider implementing Architectural Design Incentives for the Downtown Pomona Specific Plan to promote and preserve the historical integrity of existing visible streetscape.



4.2 Attracting Private Investment and Providing Public Benefits

Effective implementation of a development plan typically involves both the public and private sectors. Whereas development of envisioned land uses is often "kick started" by various public sector initiatives, the ultimate goal of this type of planning effort is to attract desired private investment. Broadly speaking, there are two major ways that a municipality can facilitate private development:

- By creating a "conducive development environment" that is consistent with prevailing market demand for various land uses. This may include the following types of actions or policies:
 - Zoning, design guidelines, etc. that are responsive to market needs at the individual-establishment level, while maintaining the overall character of the area that preserves and enhances its general marketability;
 - b. Information about the concepts, intent, etc. of the plan area to prospective investors/tenants;
 - Streamlined permitting and entitlement processes (i.e., minimizing the need for discretionary approval processes);
 - d. Area-wide "amenity" investments, including landscape and streetscape improvements;

- e. Focusing of available resources to address critical issues (e.g., homelessness) that would otherwise serve as impediments to attracting businesses and consumers to the plan area;
- f. Marketing programs to enhance the area's identity and recognition among consumers;
- g. Marketing efforts to attract targeted development and business types;
- h. Coordinating or supporting placemaking initiatives at the neighborhood level, as part of an overall branding effort; and
- i. Clearinghouse roles (e.g., coordination of funding resources and dissemination of information related to investment in the plan area).
- 2. By providing focused development support to area businesses, property owners, and key development projects. This can involve the following types of initiatives:
 - a. Facilitating financing of area-wide and/or project specific infrastructure requirements;
 - b. Facilitating the use of supplemental financing mechanisms to support needed services (e.g., security, landscape maintenance, etc.) in the plan area;
 - c. Assembling land for key "opportunity sites"; and
 - d. Providing loans and/or grants for various business and property improvement purposes such as building façade renovations.

4.3 Economic Conditions Influencing Development Potentials in Downtown Pomona

Downtown Pomona has made impressive progress since the original DPSP was adopted in 1994. Through growth of the Arts Colony – which was a prominent theme in the 1994 Plan – the area has developed a notable market niche for "nightlife" activities (attracting over one million visitors per year) and has experienced visible reinvestment in historic buildings. These accomplishments notwithstanding, the area has a number of remaining challenges to address. The area's key strengths and challenges from a market perspective are identified below.

4.3.1 Strengths

- Excellent "institutional" anchors, including:
 - Western University of Health Sciences a graduate school for the health professions offering degrees in osteopathic medicine, dental medicine, optometry, podiatric medicine, nursing, physician assistant studies, physical therapy, pharmacy, biomedical sciences and veterinary medicine;
 - ° The School of Arts and Enterprise a public, charter high school focused on visual and performing arts;
 - ° The Pomona Transit Center located in the heart of downtown (100 W. First Street), serving as a multi-modal Metrolink, Amtrak, Foothill Transit, and Omnitrans station;
 - Fox Theater an architectural landmark that was completely renovated in 2009 (after being mostly closed since 1977) to provide a state-of-the-art facility for concerts, cinema, performances, and parties.
- An eclectic mix of arts/entertainment oriented businesses, which have helped to position the downtown as a regional destination for nightlife and special events.
- An organized business community, spearheaded by the Downtown Pomona Owners Association (DPOA), which effectively coordinates marketing, special events, enhanced security, and public realm beautification for a portion of the downtown area.
- An emerging "urban residential" market (most notably the highly successful Monterey Station apartment community located at the southeast corner of Holt Avenue and Garey Avenue).
- Additional planned private investment (residential, movie theater, retail), which will potentially serve as an effective catalyst for accelerated development in the plan area.

4.3.2 Challenges

- The area's "daytime economy" is limited, making it difficult to attract and retain quality retail businesses.
- The historic Antique Row, which also featured prominently in the 1994 DPSP, has long struggled to remain viable and is currently in a state of decline.
- Local business stakeholders recognize a longstanding need to diversify the retail tenant mix (to complement the evening-oriented arts/ entertainment businesses).
- The limited (but gradually growing) housing stock in the immediate vicinity of the downtown limits the demographic potential for supporting additional neighborhood-oriented retail uses.
- Despite the progress made in terms of revitalization of key buildings, overall beautification (including almost-instant graffiti removal), and improved public safety, much remains to be done on the cleanup of vacant and underutilized properties and addressing larger social issues such as homelessness.

This section of the Implementation chapter describes several potential funding sources and financing mechanisms that may be important tools for implementation of the Specific Plan.

4.3.3 Reauthorization and Expansion of Property and Business Improvement District (PBID)

In California, there are two separate laws that authorize the formation of a Business Improvement District (BID): The Parking and Business Improvement Area Law of 1989 and the Property and Business Improvement District Law of 1994. Both laws enable a city, county, or joint powers authority (made up of cities and/or counties only) to establish a BID and levy annual assessments on businesses within its boundaries. Improvements which may be financed include parking facilities, parks, fountains, benches, trash receptacles, street lighting, and decorations. Services that may be financed include promotion of public events, furnishing music in public places and promotion of tourism. In addition to the above, the 1994 Act also allows financing of streets, rehabilitation or removal of existing structures, and security facilities and equipment.

The 1989 Act allows financing of marketing and economic development, and various supplemental municipal services such as security and sanitation. Neither law allows bonds to be issued by the BIDs.

Downtown Pomona has existing Property and Business Improvement District (PBID). The Downtown Pomona Owners Association (DPOA) and its associated funding mechanism – the PBID – have been instrumental in the significant progress the Downtown has made since the adoption of the original DPSP in 1994. Critical activities spearheaded by DPOA (and funded by the PBID) include:

- · Marketing and special events;
- Maintenance of public areas (pocket parks, streetscape, etc.);
- · A highly-effective graffiti removal program; and
- Private security patrols (to supplement City police services).

The existing PBID was established in 2004 and will need to be reauthorized by 2019. The existing PBID covers only a portion of the Specific Plan area. The DPOA is currently investigating options for expanding the area covered by the PBID as part of the reauthorization process. Alternatively, a new (separate) PBID could be established to cover the Downtown areas not included in the existing PBID. In either case, expanded PBID coverage will be a crucial element of implementing the updated DPSP.

4.3.4 Enhanced Infrastructure Financing Districts

Senate Bill No. 628, creating enhanced infrastructure financing districts (EIFDs) took effect on January 1, 2015. EIFDs are designed to fund infrastructure development and community revitalization, through issuing bonds, establishing a public financing authority, and adopting an infrastructure financing plan. EIFDs include a provision for using tax increment financing. A broad range of community development and revitalization projects can be funded through this mechanism. Given the limitations on PBIDs related to funding major infrastructure (since BIDs cannot issue bond), an EIFD could provide an important means of funding Downtown infrastructure projects that are beyond the means of the City's General Fund and normal Capital Improvement Program.

Along with the EIFD, the bill allows a city/county to adopt an infrastructure financing plan and issue bonds upon approval of 55% of the voters. As excerpted from the Bill's text, the bond funds would be used to "finance public facilities or other specified projects of communitywide significance, including, but not limited to, brownfield restoration and other environmental mitigation; the development of projects on a former military base; the repayment of the transfer of funds to a military base reuse authority; the acquisition, construction, or rehabilitation of housing for persons of low and moderate income for rent or purchase; the acquisition, construction, or repair of industrial structures for private use; transit priority projects; and projects to implement a sustainable communities strategy." Prior to the adoption of an EIFD and infrastructure financing plan, the bill requires the legislative body to establish a public financing authority, which would be comprised of members of the legislative body of the participating entities and of the public. In addition, the bill would require the adoption of a "resolution of intention" that, among others would include the following:

- District boundaries
- · Description of the proposed public facilities/development that would be financed or assisted by the EIFD
- Need for the EIFD and goals to achieve

The infrastructure financing plan, along with agreement from affected taxing agencies, would provide the mechanism to fund infrastructure projects through tax increment financing. Specifically, the bill authorizes the creation of an EIFD for up to 45 years from the date on which the issuance of bonds is approved. In addition, the bill would authorize a city, county, or special district that contains territory within an EIFD to loan moneys for projects/activities that are listed in the infrastructure financing plan. Finally, the city/county, through its infrastructure financing plan, could choose to allocate any portion of its net available revenue to the EIFD.

4.3.5 What is allowed

An EIFD can finance only public capital facilities or other specified projects of communitywide significance that provide significant benefits to the district or the surrounding community, including, but not limited to, all of the following:

- 1. Highways, interchanges, ramps and bridges, arterial streets, parking facilities, and transit facilities.
- 2. Sewage treatment and water reclamation plants and interceptor pipes.
- 3. Facilities for the collection and treatment of water for urban uses.
- 4. Flood control levees and dams, retention basins, and drainage channels.
- 5. Child care facilities.
- 6. Libraries.
- 7. Parks, recreational facilities, and open space.
- 8. Facilities for the transfer and disposal of solid waste, including transfer stations and vehicles.
- 9. Brownfield restoration and other environmental mitigation.
- 10. The development of projects on a former military base.

- 11. The repayment of the transfer of funds to a military base reuse authority pursuant to Section 67851 that occurred on or after the creation of the district.
- 12. The acquisition, construction, or rehabilitation of housing for persons of low and moderate income, as defined in Section 50093 of the Health and Safety Code, for rent or purchase.
- 13. Acquisition, construction, or repair of industrial structures for private use
- 14. Transit priority projects, as defined in Section 21155 of the Public Resources Code, that are located within a transit priority project area.
- 15. Projects that implement a sustainable communities strategy, when the State Air Resources Board, pursuant to Chapter 2.5 (commencing with Section 65080) of Division 2 of Title 7, has accepted a metropolitan planning organization's determination that the sustainable communities strategy or the alternative planning strategy would, if implemented, achieve the greenhouse gas emission reduction targets.

4.3.6 What is not allowed

Restrictions on EIFDs include the following:

- An EIFD can only be created after the City/County after the specified conditions related to the wind down of the former redevelopment agency (if one was created by the city/ county) have been satisfied.
- 2. Cannot divert property tax revenue from schools or from any non-consenting tax entity any taxing entity contributing tax increment must consent and opt into the EIFD.
- 3. A district may not finance routine maintenance, repair work, or the costs of an ongoing operation or providing services of any kind.

Category	EIFD	IFD
Created	Adopt infrastructure financing plan - city/county legislative body	2/3 vote of the electorate
Issue of bonds based on tax increment financing	55% vote of the electorate	2/3 vote of the electorate
Eligible projects	Broader range – e.g., transit, lmi housing, sustainable communities strategies, environmental remediation, etc., in addition traditional infrastructure projects	Public capital facilities (more limited)
Financing authority	Cad dedicate more revenue sources to the funding of infrastructure – e.g., can devote portions of their periodic distributions from the Redevelopment Property Tax Trust Fund, funds received from the Mello-Roos Community Facilities Act of 1982 and funds from the Benefit Assessment Act of 1982, among others	More limited
Longevity	45 years from date bonds are issued or loans are approved	30 years from initial formation
Eminent Domain	Can exercise eminent domain powers under the Polanco Redevelopment Act associated with the cleanup of environmentally impacted properties	Not available

Source: Holland & Knight. http://www.hklaw.com/Publications/Enhanced-Infrastructure-Financing-Districts-SB-628-Beall-11-12-2014/ (accessed on 5/20/15)

4.3.7 Landscaping and Light Act

The Landscaping and Lighting Act of 1972 created a flexible tool used by local government agencies to pay for landscaping, lighting and other improvements and services in public areas. As a form of benefit assessment, it is based on the concept of assessing only those properties that benefit from improvements financed, either directly, or indirectly through increased property values. Because it is considered a benefit assessment, a 1972 Act assessment is not subject to Proposition 13 limitations. The City of Pomona has several existing Landscaping and Lighting Districts (including, in the vicinity of the Specific Plan area, the South Garey Maintenance District).

4.3.8 Local Funding Opportunities Administered by the Los Angeles County Metropolitan Transit Authority.

The Specific Plan's focus on transit and transportation infrastructure provides an important nexus to potential funding opportunities administered by the Los Angeles County Metropolitan Transit Authority. Key funding programs are summarized in Table 6-2, Los Angeles County Metropolitan Transit Authority Key Funding Programs.

Table 4-2, Los Angeles County Metropolitan Transit Authority Key	
Funding Source	Eligible Uses Potentially Relevant to DPSP
Proposition A – voter enacted (1980) ½-cent sales tax in Los Angeles County.	Exclusively to benefit public transit (including fare subsidy programs).
DPSP-relevant component:	
 Local return program (25%) funds are distributed to cities on a per capita basis for public transit uses. 	
Proposition C – voter enacted (1990) ½-cent sales tax in Los Angeles County.	Capital costs of transit centers including facilities, access improvements, landscaping, bike lockers, rehabilitation, and other amenities.
DPSP-relevant components:	
 Commuter Rail/Transit Centers/Park-n-Ride (10% allocated on a call for projects basis) 	Commuter bikeways and bike lanes; fare subsidy programs.
 Local return program (20%) funds are distributed to cities on a per capita basis 	
Measure R – voter enacted (2008) ½-cent sales tax in Los Angeles County.	Major street resurfacing, rehabilitation and reconstruction; pothole
DPSP-relevant component:	repair; left turn signals; bikeways; pedestrian improvements; streetscapes; signal synchronization; and transit.
Local return program (15%) funds are distributed to cities on a per capita basis for public transit uses.	
Measure R – voter enacted (2016) ½-cent sales tax in Los Angeles County (increasing to one cent in 2039, at the expiration of Measure R).	Major street resurfacing, rehabilitation and reconstruction; pothole repair; left turn signals; bikeways; pedestrian improvements;
DPSP-relevant component:	streetscapes; signal synchronization; and transit.
Local return program (16%) funds are distributed to cities on a per capita basis for public transit uses.	
Transportation Development Act (TDA) – a Local Transportation Fund (LTF) for each county derived from ¼ cent of the 7/25 statewide retail sales tax.	Bicycle and pedestrian facilities.
DPSP-relevant component: TDA Article 3 (2% allocation for bicycle and pedestrian facilities), distributed to cities based on a population formula.	

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Measure A was passed by Los Angeles County voters in November 2016 and will replace funding under Proposition A passed in the 1990's. The measure adds a parcel tax of one-and-a-half cent per square foot of developed property. The measure is projected to raise \$94 million annually. The funds will go directly to cities and local communities to protect, enhance and maintain neighborhood parks, open space, trails, beaches, natural habitat and rivers, creeks and streams.

4.3.9 State of California, Active Transportation Program (ATP)

- Increase the proportion of trips accomplished by biking and walking;
- · Increase safety and mobility for non-motorized users;
- Advance the active transportation efforts of regional agencies to achieve greenhouse gas (GHG) reduction goals;
- · Enhance public health;
- Ensure that disadvantaged communities fully share in the benefits of the program; and
- Provide a broad spectrum of projects to benefit many types of active transportation users.

4.3.10 Community Development Block Grant (CDBG) Funds

Federal Community Development Block Grant (CDBG) funds are allocated to local public jurisdictions through HUD to provide opportunities for low-to-moderate income persons and areas. CDBG funds support: 1) development of viable urban communities, 2) preservation of existing housing stock, 3) the elimination of slums and blight, and 4) expanding economic opportunities for eligible persons. The CDBG program requires that funds be targeted at households that earn less than 80% of the area median income. Eligible activities include infrastructure improvements, rehabilitation, economic development programs, code enforcement, administration, fair housing and public services.

4.4 Overview of Community Revitalization and Investment Authorities

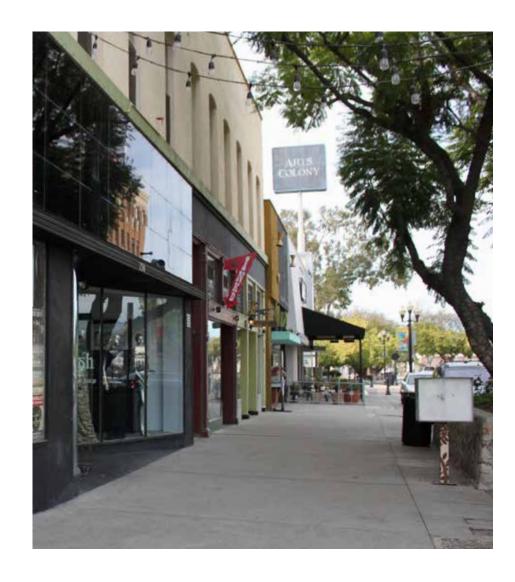
State legislation adopted in 2015 (amended in 2016) authorizes the formation of Community Revitalization and Investment Authorities (CRIAs). The CRIA legislation is intended to allow local government agencies to address the effects of blight, in effect addressing the role that was previously fulfilled by redevelopment agencies in California.

- 1. Acquire and transfer real property. The authority shall retain controls and establish restrictions or covenants running with the land sold or leased for private use for such periods of time and under such conditions as are provided in the plan. The establishment of such controls is a public purpose under the provisions of this part.
- 2. Issue bonds.
- 3. Borrow money, receive grants, or accept financial or other assistance or investment from the state or the federal government or any other public agency or private lending institution for any project or within its area of operation, and may comply with any conditions of the loan or grant.
- 4. Adopt a community revitalization and investment plan.
- 5. Make loans or grants for owners or tenants to improve, rehabilitate, or retrofit buildings or structures within the plan area.
- 6. Construct foundations, platforms, and other like structural forms necessary for the provision or utilization of air rights sites for buildings to be used for residential, commercial industrial, or other uses contemplated by the revitalization plan.
- 7. Provide direct assistance to businesses within the plan area in connection with new or existing facilities for industrial or manufacturing uses, except as specified in this division.

4.4.1 Criteria

One of the key general provisions is the requirement to implement a community revitalization plan within a community revitalization and investment area. As required by the Bill, the area is required to meet the following conditions:

- 1. An annual median household income that is less than 80 percent of the statewide annual median income.
- 2. Three of the following four conditions:
 - A. Nonseasonal unemployment that is at least 3 percent higher than statewide median unemployment, as defined by the report on labor market information published by the Employment Development Department in January of the year in which the community revitalization plan is prepared.
 - B. Crime rates that are 5 percent higher than the statewide median crime rate, as defined by the most recent annual report of the Criminal Justice Statistics Center within the Department of Justice, when data is available on the California Attorney General's Internet Web site.
 - C. Deteriorated or inadequate infrastructure such as streets, sidewalks, water supply, sewer treatment or processing, and parks.
 - D. Deteriorated commercial or residential structures.



4.4.2 Overview

As excerpted from the Bill's summary, the CRIA legislation authorizes a local agency to form a CRIA "within a community revitalization and investment area, as defined, to carry out provisions of the Community Redevelopment Law in that area for purposes related to, among other things, infrastructure, affordable housing, and economic revitalization. The bill would provide for the financing of these activities by, among other things, the issuance of bonds serviced by tax increment revenues, and would require the authority to adopt a community revitalization and investment plan for the community revitalization and investment area that includes elements describing and governing revitalization activities. The bill would also provide for periodic audits by the Controller. The bill would also require the Department of Housing and Community Development, advised by an advisory committee appointed by the Director of Housing and Community Development, to periodically review the calculation of surplus housing under these provisions. The bill would require certain funds allocated to the authority to be deposited into a separate Low and Moderate Income Housing Fund and used by the authority for the purposes of increasing, improving, and preserving the community's supply, as specified. The bill would, if an authority failed to expend or encumber surplus funds in the Low and Moderate Income Housing Fund, require those funds to be disbursed towards housing needs. The bill would require an authority to make relocation provisions for persons displaced by a plan and replace certain dwelling units that are destroyed or removed as part of a plan. The bill would authorize an authority to acquire interests in real property and exercise the power of eminent domain, as specified."

4.4.3 Authorized Actions

As indicated in Bill, an authorized Authority can do the following:

- 1. Provide funding to rehabilitate, repair, upgrade, or construct infrastructure.
- 2. Provide for low- and moderate-income housing.
- 3. Remedy or remove a release of hazardous substances pursuant to the Polanco Redevelopment Act.
- 4. Provide for seismic retrofits of existing buildings in accordance with all applicable laws and regulations.

4.4.4 Key Differences from Community Redevelopment Agencies

Although Authorities created under this law will function in a similar role compared to redevelopment agencies, which were previously authorized under Community Redevelopment Law, some of the key differences include the following:

- Public Members. Along with the three members of a legislative body of the city, county, or joint city-county that created the authority, the authority is required to include two public members who live or work in the plan area. Supporters contend that this requirement improves transparency.
- Low- and moderate-income housing. A minimum of 25% of the funds in a plan area are required to be set aside for affordable housing, compared to 20% under the previous redevelopment system.
- Tax increment. Previous redevelopment law "shifted" property tax increment from special districts (such as schools, fire districts, etc.) in effected plan areas. The existing Bill requires cities, counties, and special districts to agree to contribute a share of property tax increment to fund proposed improvements.
- Dissolution Mechanism. At 10-year intervals the public could stop the Authority's operations by filing a petition and putting the plan to a public vote.

4.5 Leveraging the City of Pomona's Opportunity Zones

The Opportunity Zone (OZ) program – a new federal tax incentive for real estate and business investment in economically-distressed areas – represents a significant and time-sensitive opportunity to catalyze implementation of the Downtown Specific Plan. The program has the potential to be a "game changer" in terms of enhancing the investment attractiveness of designated areas; virtually all development/business types are eligible to utilize the benefits (only "sin" businesses are excluded).

4.5.1 What are Opportunity Zones?

The Tax Cuts and Jobs Act of 2017 established Opportunity Zones as a mechanism to provide substantial tax incentives for investment in designated census tracts. Investments made by individuals through special funds in these zones are allowed to defer or eliminate federal taxes on capital gains.

An Opportunity Zone is an economically-distressed community where new investments, under certain conditions, may be eligible for preferential tax treatment. Localities qualify as Opportunity Zones if they have been nominated for that designation by the State and that nomination has been certified by the Secretary of the U.S. Treasury and the Internal Revenue Service.

An Opportunity Zone provides a new tool for investors, fund managers and communities by utilizing privately sourced funds into eligible economic development and community reinvestment projects.

4.5.2 Opportunity Zones in the City of Pomona and the Downtown Specific Plan Area

A total of 879 of the designated Opportunity Zones (census tracts) are in California. Of the 879 zones statewide, four census tracks (covering approximately 1,608 acres) are in the City of Pomona. One of Pomona's four census tracts overlaps into the Downtown Specific Plan Area.

City's Potential Roles in Maximizing Benefits of Opportunity Zones In defining potential City actions to maximize the benefits of the OZ program to Pomona, it is important to recognize the following aspects of the program:

- Local governments are not directly Involved in OZ administration. The actual OZ program, being part of the federal tax code, is administered by the U.S. Department of the Treasury. Participation in the program (by developers and investors) does not require interface with local governments. In some communities, investments in Opportunity Zones are already occurring absent any local effort to promote them. However, to fully maximize the advantages associated with having Opportunity Zones in a community, local jurisdictions can play a vital role in raising awareness of the program and, especially, in aggressively promoting investment-ready development and business opportunities in their designated zones.
- The clock is running on the availability of OZ tax benefits. The designated OZ census tracts were certified by the U.S. Treasury on April 9, 2018. Their certification is valid for ten years (i.e., through 2028), regardless of when qualifying investments are made. Investments initiated in 2019 will enjoy the full 10 years of tax benefits from the program. Investments made in subsequent years will be able to utilize the tax benefits for a reduced number of years. From the community's perspective, it will be critical to act fast in order to fully leverage the program benefits (in terms of attracting development and investment), Appropriately, the City of Pomona has already begun promoting its Opportunity Zones. The time-sensitivity of the program applies not only to marketing of opportunities but also to the need for the City to facilitate expedited development approvals in order for projects to be developed in time to take maximum advantage of tax benefits.
- "Opportunity Funds" are efficient vehicles for attracting investors. Private "Opportunity Funds" have been established throughout the U.S. in anticipation of the implementation of the OZ program. These funds provide a mechanism for qualified investors to pursue roll-overs of investment gains (e.g., from stocks or real estate) into Opportunity Zone investments, thereby taking advantage of the OZ program in terms of reductions in capital gains taxes. Opportunity Funds are not only efficient from the investor's standpoint, but also serve as a convenient marketing target for communities attempting to attracting investment to their zones.

4.5.3 OZ Action Plan for Pomona Downtown Specific Plan

The following specific steps are recommended for near-term action as part of the City's implementation of the Downtown Specific Plan:

- Showcase market-feasible, investment-ready development opportunities. Within the OZ census tract that overlaps with the Downtown Specific Plan area, identify specific development and investment opportunities than can be immediately marketed to prospective developers, investors and Opportunity Funds. The Downtown opportunities should be included in a larger list of OZ opportunities in the City (i.e., the list should also include development opportunities for the three census tracts not in the Downtown area).
- Fast-track approval processes. For the listed opportunity sites/projects, determine current entitlement status and identify any additional measures the City could take to expedite/ streamline development projects on these sites.
- 3. Prioritize infrastructure investments. Within the Downtown Specific Plan area, prioritize infrastructure and other public investments that would most directly enhance developability of prospective OZ projects/sites.

- 4. Prepare marketing materials. Market-based opportunities and supporting City policies/ programs should be succinctly summarized in print and online media. As part of the marketing materials, the City should consider developing an "OZ Prospectus" which makes a specific case (via a simple pro forma financial analysis) for investing in Pomona's OZ-designated census tracts.
- 5. Network through OppSites.com. This evolving platform provides a forum for communities with Opportunity Zones to connect and interact with prospective developers and investors. The City of Pomona should consider listing its development opportunities on this website.

4.6 Implementation Action Plan

The vision and goals presented in the Specific Plan are supported by actions outlined in Table 6.3, Implementation Action Plan Matrix. The action plan provides a summary of the key Specific Plan recommendations and is presented in a table format. The table is organized by topic, listing regulatory actions in the first section and improvements and programs in the second section. For each action item, the lead and support department at the City of Pomona is listed under the responsibility heading followed by an indication of priority of the action and funding sources that would be most closely associated with the activity. Outside agencies and organizations are identified as well where appropriate. It is intended that this list of public actions be incorporated in the City's annual budget setting process and included in the Capital Improvement Plan for execution.

Table 4-3, Implementation Action Plan Matrix					
Action Itom	T::	Responsibility		Potential Funding	
Action Item	Timing	Lead	Support/Coordination	Sources	

Timing:

- 1 = Early Implementation (1-3 years)
- 2 = Near Term Implementation (4-10 years)
- 3 = Long Term Implementation (10-20 years)

Funding Source Acronyms:

PBID - Property and Business Improvement District (existing and/or expanded).

Metro – various local funding opportunities administered by the Los Angeles County Metropolitan Transit Authority, including Proposition A, Proposition C, Measure R, Measure M, and the Transportation Development Act (TDA).

EIFD - potential Enhanced Infrastructure Financing District.

LLD - Landscape and Lighting District (new district or existing South Garey Maintenance District).

ATP – local funding opportunities through the State's Active Transportation Program.

VPD – Vehicle Parking District (existing district focused on parking maintenance and capital improvements in the Downtown business area.

CRIA – potential Community Revitalization and Investment Authority.

Measure A – Los Angeles County funding source (successful November 2016 ballot initiative) for local parks and open space.

CDBG – Community Development Block Grant funds.

PUC - Public Utilities Commission.

Regulatory Actions				
Downtown Pomona Specific Plan: Adopt Specific Plan Update	1	Development Services	Public Works, Economic Development	Metro Grant
Pomona Corridors Specific Plan: Amend Specific Plan map for identified parcels.	1	Development Services		General Fund

Table 4-3, Implementation Action Plan Matrix						
Action Item	Timing	Re	Potential Funding			
		Lead	Support/Coordination	Sources		
Development Code: Amend Zoning Map for identified parcels.	1	Development Services		General Fund		
Expand Property and Business Improvement District: Expand geographic scope of the existing Property Business Improvement District (PBID) administered by the Downtown Pomona Owners Association (DPOA). As an option to expanding the existing PBID, consider establishing a separate PBID that covers the portions of Downtown not covered by the existing PBID.	1	Economic Development	Development Services, Downtown Business Improvement District, Chamber of Commerce	General Fund, PBID		
Community Revitalization and Investment Authority: Investigate options for establishing a Community Revitalization and Investment Authority (CRIA) as a potential mechanism for economic development initiatives in the Downtown area.	1	Economic Development	PBID	General Fund		
Enhanced Infrastructure Financing District: Investigate options for establishing an Enhanced Infrastructure Financing District (EIFD) as a potential funding mechanism for capital improvements in the Downtown area.	1	Economic Development	PBID	General Fund		
Opportunity Zones: Develop marketing materials and related incentives/support policies to maximize developer/investor interest in the City's OZ-designated census tracts.	1	Economic Development	Development Services	General Fund		
Improvement Projects/Programs						
Public Realm Activation Guides and Applications for the Downtown Pomona Specific Plan: Series of guides to establish new pathways for stakeholders to activate public property and public right-of-way.	3	Development Services	Public Works	General Fund, PBID		

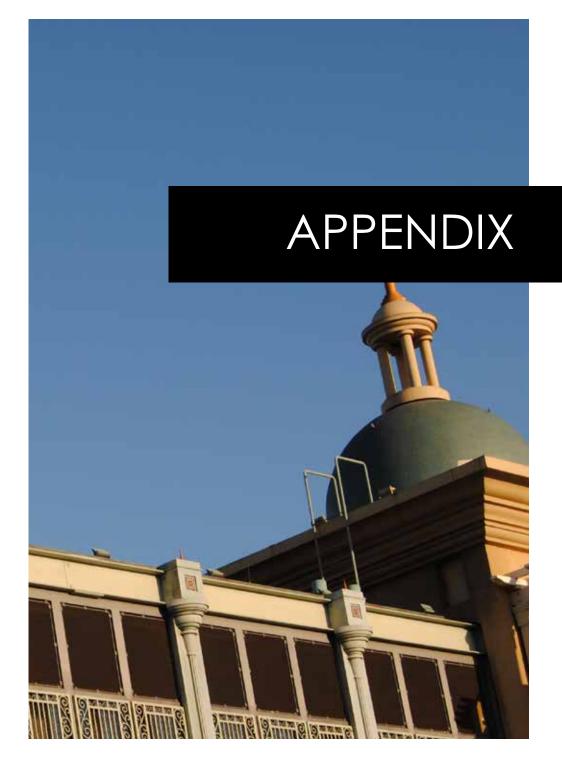
Table 4-3, Implementation Action Plan Matrix						
Action Item		Responsibility		Potential Funding		
Action Item	Timing	Lead	Support/Coordination	Sources		
Landscaped Medians: Install landscape medians on major arterials to reduce scale, including Towne Avenue and White Avenue.	1,2	Public Works	Development Services	Metro, EIFD, LLD		
Ridesharing Service: Develop program for coordination and location of ridesharing services (such as Uber, Lyft, Bird).	1	Public Works	Development Services	PBID		
Transit/Bus Stops: Continue to coordinate with transit providers to ensure adequate number and location of transit stops.	Ongoing	Public Works, Development Services	Omnitrans	Omnitrans, Metro		
Pomona Transit Center Expansion: Evaluate expansion of the Pomona Transit Center to adjacent properties if warranted by service increases.	Ongoing	Public Works	Metrolink, Omnitrans	Metro, Omnitrans, EIFD		
Bicycle Facilities: Implement bicycle facilities along Second Street and Palomares Street.	1, 2	Public Works	Development Services	ATP, Metro		
Bicycle Parking: Install bicycle parking facilities as recommended in the Active Transportation Plan for the following locations: Pomona Transit Center, Pomona Post Office, The Glass House/Fox Theater, Western University, Pomona Public Library/Superior Courthouse.	1, 2	Public Works	Development Services	ATP, Metro, Private Development		
Intersections: Install pedestrian improvements the following intersections pursuant to Active Transportation Plan recommendations: Pomona Transit Center, 2nd Street/Garey Avenue, 3rd Street/Garey Avenue, 4th Street/Garey Avenue.	1, 2	Public Works		ATP, Metro, Private Development, EIFD		

Table 4-3, Implementation Action Plan Matrix					
Action Item	Timing	Re	Potential Funding		
Action item		Lead	Support/Coordination	Sources	
Sidewalks: Install missing sidewalks segments along N. Parcels Street, N. Gibbs Street, N. Linden Street, N. Eleanor Street, W. Commercial Street, E. Commercial Street, S. Elm Street.	1, 2	Public Works	Development Services	EIFD, Private Development	
Parking Management Strategy: Prepare parking management Strategy for Downtown including incorporation of updated wayfinding and newer technologies to optimize utilization of parking supply.	1	Public Works	Development Services	VPD, PBID	
Metrolink Parking/Amenities: Coordinate with Metrolink to ensure adequate parking supply and supporting amenities such as information kiosks and directories.	Ongoing	Public Works, Economic Development	Metrolink	Metro	
Transit Technology: Implement transit operator and transit user ITS and technology advancements to improve transit/rideshare operations and user convenience	2	Omnitrans, Metrolink	Public Works	Omnitrans, Metro	
Parking Lot Design/Management: Evaluate parking lot design and parking lot management to ensure continued security for users.	Ongoing	Public Works		VPD, PBID	
Parking Funding: Periodically evaluate funding mechanisms for expanded/updated parking facilities and parking structures.	Ongoing	Public Works		VPD, PBID	
Branding Package: Develop a branding package including message, themes, and graphics	1	Economic Development	Chamber of Commerce	General Fund, PBID, CDBG, CRIA	
Wayfinding and Gateway Program: Prepare a comprehensive wayfinding and gateway program serving motorists, bicyclists and pedestrians	1	Economic Development, Public Works	Development Services	PBID, PVD	

Table 4-3, Implementation Action Plan Matrix						
Action Item	Timing	Res	Potential Funding			
		Lead	Support/Coordination	Sources		
Primary Gateway - Garey Avenue Entry Corridor: Design and install gateway treatment.	1	Economic Development, Public Works	Development Services	EIFD, PBID		
Secondary Gateways: Design and install gateway treatment at Garey Avenue/Monterey Avenue, Towne Avenue/Third Street, Second Street/White Avenue.	2	Economic Development, Public Works	Development Services	EIFD, PBID		
Pedestrian Street - Second Street: Design and install refurbishment improvements between Park Avenue and Gibbs Street.	1	Public Works	Development Services	EIFD, PBID		
Pedestrian Street - Locust Street: Between Mission Boulevard and Second Street design and install pedestrian-oriented improvements with limited vehicular access.	3	Public Works	Development Services	EIFD, PBID		
Memorial Park: Design and install enhancements to existing park.	2	Community Services/ Parks and Recreation	Development Services	Measure A, EIFD		
New Park: Design and install new park west of Eleanor Street between Third Street and Second Street.	3	Community Services/ Parks and Recreation	Development Services	Measure A, EIFD		
Streetscape Amenities: In areas adjacent to sidewalks provide sidewalks, bicycle racks, benches and other street furniture.	1, 2	Public Works	Development Services	Private Development, Metro		

Table 4-3, Implementation Action Plan Matrix						
Action Item	Timing	Re	Potential Funding			
		Lead	Support/Coordination	Sources		
Specific Street Tree Palette : Develop specific tree palettes for major and minor streets in Downtown Pomona as part of the development of a City of Pomona Street Tree Planting Plan.	1	Public Works	Community Services/ Parks and Recreation	General Fund, Grants, CIP, CRIA, PBID		
Pedestrian Lighting: Install pedestrian-scale lighting.	2	Public Works	Development Services	EIFD, Private Development, LLD		
Business Incentives: Develop a menu of business incentives such as streamlined application review or waive/defer fees	2	Economic Development, Development Services	Business Improvement District, Chamber of Commerce	General Fund, CDBG, CRIA		
Underground Utilities: Replace existing overhead utilities with underground utilities where practical.	3	Public Works	Utility Providers	PUC (Rule 20), Private Development		
Public Realm Activation Guides : Develop a series of public realm activation guides and applications to establish new pathways for stakeholders to activate public property and public right-of-way.	1, 2	Development Services	Public Works	General Fund, CRIA, PBID, Private Development		
Complete Streets Manual: Prepare a manual with strategies for implementing best practices in streetscape design.	2, 3	Development Services	Public Works	General Fund, CRIA, PBID, Private Development		

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A.1 Definitions

Land Uses

Residential Uses

Single-Unit Dwelling, Detached. A dwelling unit that is designed for occupancy by one household with private yards on all sides. This classification includes individual manufactured housing units.

Single-Unit Dwelling, Attached. A dwelling unit that is designed for occupancy by one household located on a separate lot from any other unit (except a second dwelling unit, where permitted), and is attached through common walls to one or more dwellings on abutting lots. An attached single-unit dwelling is sometimes called a "townhouse" or a "condominium".

Two-Unit Dwelling. A residential building containing two dwelling units, both of which are located on a single parcel (also referred to as a "duplex" or "two-flat".) The dwelling units are attached and may be located on separate floors or side-by-side.

Multi-Unit Development. Three or more attached or detached dwelling units on a single lot. Types of multi-unit residential include townhouses, multiple detached residential units, and apartment buildings.

Work/Live Units. An area comprised of one or more rooms or floors in a new building or in one originally designed for industrial or commercial occupancy which has been or will be remodeled or altered, which includes cooking space and sanitary facilities, sleeping space, and working space reserved for persons residing therein.

Family Day Care. A day care facility licensed by the State of California, located in a residential unit where resident of the dwelling provides care and supervision for children under the age of 18 for periods of less than 24 hours a day.

Small. A facility that provides care for eight or fewer children, including children who reside at the home and are under the age of 10.

Large. A facility that provides care for seven to 14 children, including children who reside at the home and are under the age of 10.

Residential Care Facilities. A facility licensed by the state of California to provide living accommodations, 24-hour care for persons requiring personal services, supervision, protection, or assistance with daily tasks. Amenities may include shared living quarters, with or without a private bathroom or kitchen facilities. This classification includes those both for and not-for-profit institutions, but excludes Supportive Housing and Transitional Housing.

Small. A facility that is licensed by the state of California to provide care for six or fewer persons.

Large. A facility that is licensed by the state of California to provide care for more than six persons.

Residential Facility, Assisted Living. A facility that provides a combination of housing and supportive services for the elderly or functionally impaired, including personalized assistance, congregate dining, recreational, and social activities. These facilities may include medical services. Examples include assisted living facilities, retirement homes, and retirement communities. These facilities typically consist of individual units or apartments, with or without kitchen facility, and common areas and facilities. The residents in these facilities require varying levels of assistance.

Supportive Housing. Dwelling units with no limit on length of stay, that are occupied by the target population as defined in subdivision (d) of Section 53260 of the California Health and Safety Code, and that are linked to onsite or offsite services that assist the supportive housing resident in retaining the housing, improving his or her health status, and maximizing his or her ability to live and, where possible, work in the community.

Transitional Housing. Transitional housing is housing that has a predetermined end point in time, and operated under a program that requires the termination of assistance, in order to provide another eligible program recipient to the service. The program length is usually no less than six months.

Public/Semi-Public Uses

Colleges and Trade Schools. Institutions of higher education providing curricula of a general, religious, or professional nature, granting degrees and including junior colleges, business and computer schools, management training, technical and trade schools, however excluding personal instructional services such as music lessons.

Convention Center. A facility accommodating large gatherings of people and used for any combinations of the following: conventions, conferences, seminars, product displays, recreational activities, entertainment functions, and accessory uses such as food preparation and service for on-site consumption.

Cultural Institutions. An institution and/or associated facility engaged in activities to promote aesthetic and educational interest among the community that are open to the public on a regular basis. This classification includes performing arts centers for performances and events; spaces for display or preservation of objects of interest in the arts or sciences; libraries; museums; historical sites; aquariums; art galleries; and zoos and botanical gardens, all of which are public or private. This does not include schools or institutions of higher education providing curricula of a general nature.

Day Care Centers. Establishments providing non-medical care for persons on a less than 24-hour basis other than Family Day Care. This classification includes nursery schools, preschools, and day care facilities for children or adults, and any other day care facility licensed by the State of California.

Government Offices. Administrative, clerical, or public contact offices of a government agency, including postal facilities and courts, along with the storage and maintenance of vehicles. This classification excludes corporation yards, equipment service centers, and similar facilities that require maintenance and repair services and storage facilities for related vehicles and equipment (see Public Utilities).

Hospital and Clinics. State-licensed facilities providing medical, surgical, psychiatric, or emergency medical services to sick or injured persons. This classification includes facilities for inpatient or outpatient treatment, including substance-abuse programs, as well as training, research, and administrative services for patients and employees. This classification excludes veterinaries and animal hospitals (see Animal Care, Sales, and Services).

Hospitals. A facility providing medical, psychiatric, or surgical services for sick or injured persons, primarily on an inpatient basis, and including supplementary facilities for outpatient and emergency treatment, diagnostic services, training, research, administration, and services to patients, employees, or visitors. The institutions are to be licensed by the state of California to provide surgical and medical services.

Clinic. A facility providing medical, psychiatric, or surgical service for sick or injured persons exclusively on an outpatient basis including emergency treatment, diagnostic services, administration, and related services to patients who are not lodged overnight. Services may be available without a prior appointment. This classification includes licensed facilities offering substance abuse treatment, blood banks and plasma centers, and emergency medical services offered exclusively on an out-patient basis. This classification does not include private medical and dental offices that typically require appointments and are usually smaller scale.

Instructional Services. Establishments that offer specialized programs in personal growth and development such as music, martial arts, vocal, fitness and dancing instruction.

Park and Recreation Facilities. Parks, playgrounds, recreation facilities, trails, wildlife preserves, and related open spaces, all of which are noncommercial. This classification includes playing fields, courts, gymnasiums, swimming pools, picnic facilities, tennis courts, golf courses, and botanical gardens, as well as related food concessions or community centers within the facilities.

Parking Lots and Structures. Surface lots and structures offering parking for a fee when such use is not incidental to another on-site activity.

Performing Arts, Fine Arts, Dinner Theater. Facilities for pubic assembly and group entertainment that provides on-stage theatrical and musical performances.

Public Assembly. A facility for public or private meetings or assembly, including community centers, banquet halls, union halls, meeting halls, religious facilities, and membership organizations. Included in this classification is the use of functionally related facilities for the use of members and attendees such as kitchens, offices, multi-purpose rooms, classrooms and storage.

Public Safety Facilities. Facilities providing public-safety and emergency services, including police and fire protection and emergency medical services, with incidental storage, training and maintenance facilities.

Schools. Facilities for primary or secondary education, including public schools, charter schools, and private and parochial schools having curricula comparable to that required in the public schools of the State of California.

Social Service Facilities. Facilities providing a variety of supportive services for disabled and homeless individuals and other targeted groups on a less than 24-hour basis. Examples of services provided are counseling, meal programs, personal storage lockers, showers, instructional programs, television rooms, and meeting spaces. This classification is distinguished from licensed day care centers (see Day Care Facility), clinics, and emergency shelters providing 24-hour care (see Emergency Shelter).

Tutoring Facilities. Facilities offering academic instruction to individuals or to groups in a classroom setting where an adult accompanies a minor. Facilities where minors are not accompanied by adults are classified as Day Care Centers.

Commercial Uses

Animal Care, Sales, and Services. Retail sales and services related to the boarding, grooming, and care of household pets including:

Animal Sales and Grooming. Retail sales of animals and/or services, including grooming, for animals on a commercial basis. Typical uses include dog bathing and clipping salons, pet grooming shops, and pet stores and shops. This classification excludes dog walking and similar pet care services not carried out at a fixed location, and excludes pet supply stores that do not sell animals or provide on-site animal services.

Veterinary Services. Veterinary services for small animals. This classification allows 24-hour accommodation of animals receiving medical services but does not include kennels.

Art Sales and Services.

Art Gallery. Retail sales and display of pieces of art such as paintings, sculptures, ceramics, crafts and other works of visual art. Art pieces may be made on site.

Artist Studio. Space primarily used as a work room for at least one artist that may be open to the public for demonstrations, classes, and retail sales and may involve the use of hand tools or small mechanical equipment. Examples include acting, dance, design, furniture restoration, film and animation, jewelry, screen printing, painting, photography, recording, sculpture. This does not include artist-related activities that involve heavy mechanical equipment, which are defined under "Artisanal Manufacturing".

Artisanal Manufacturing. Any establishment primarily engaged in on-site production of goods which involves the use of heavy mechanical equipment and the incidental direct sale to consumers of only those goods produced on site. Includes large format ceramic studios, carpentry, decorative ironworks, glassworks, printing, and woodworking.

Artist Cooperative (Co-Op). An organization or association jointly owned or controlled by its membership that provides professional facilities and services for artists, including studios, workshops, equipment, exhibition galleries, and educational resources.

Banks and Financial Institutions. Financial institutions providing retail banking services. This classification includes only those institutions serving walk-in customers or clients, including banks, savings and loan institutions, check-cashing services, and credit unions.

Cinema/Theaters. Any facility for the indoor display of films and motion pictures on single or multiple screens. This classification may include incidental food and beverage service to patrons as well as auditoriums within buildings.

Commercial Entertainment and Recreation. Provision of participant or spectator entertainment to the general public. These classifications may include restaurants, snack bars, and other incidental food and beverage services to patrons.

Indoor Sports and Recreation. Establishments providing predominantly participant sports, indoor amusement and entertainment services conducted within an enclosed building, including coin-operated electronic amusement centers. Typical uses include bowling alleys, billiard parlors, card rooms, health clubs, ice and roller skating rinks, indoor racquetball courts, athletic clubs, and physical fitness centers.

Eating and Drinking Establishments. Businesses primarily engaged in serving prepared food and/or beverages for consumption on or *off the premises*.

Bars/Night Clubs/Lounges. Businesses serving beverages for consumption on the premises as a primary use and including on-sale service of alcohol including beer, wine, and mixed drinks. This use includes micro-breweries where alcoholic beverages are sold and consumed on site and any food service is subordinate to the sale of alcoholic beverages.

Restaurant, Full Service. Restaurants providing food and beverage services to patrons who order and are served while seated and pay after eating. Take-out service may also be provided.

Restaurant, Limited Service. Establishments where food and beverages are consumed on the premises, taken out, or delivered, but where limited table service is provided. This classification includes cafes, cafeterias, coffee shops, delicatessens, fast-food restaurants, sandwich shops, limited-service pizza parlors, self-service restaurants, and snack bars with indoor or outdoor seating for customers. This classification includes bakeries that have tables for on-site consumption of products as well as bakery product plants. It excludes catering services that do not sell food or beverages for on-site consumption.

Restaurant, Take-Out Only. Restaurants where food and beverages are prepared on a customer-demand basis and can be taken out or delivered, but are not consumed on the premises. No seating or other facilities for on-premises dining are provided.

Drive-Through Facility. A motor vehicle drive-through facility which is a commercial building or structure or portion thereof which is designed or used to provide goods or services to the occupants of motor vehicles. It includes, but is not limited to, banks and other financial institutions, fast food establishments, and film deposit/pick-up establishments, but shall not include drive-in movies, service stations, or car-wash operations.

Outdoor Dining & Eating. The use of a public sidewalk and public right-of-way for the placement of tables, chairs, benches, planters, umbrellas, and related items for the purpose of serving food and/or beverages in conjunction with and adjacent to an eating establishment or food court.

Food and Beverage Manufacturing (Small Scale). A small scale food and beverage products manufacturing that must include a retail sales component. It is characterized by local or regional products, specialty or artisanal foods. Examples include small coffee roasters, micro-breweries, micro-distilleries, wine manufacturing, meat or fish processing, and wholesale bakeries.

Hotels. An establishment providing overnight lodging to transient patrons. These establishments may provide additional services, such as conference and meeting rooms, restaurants, bars, or recreation facilities available to guests or to the general public.

Maintenance and Repair Services. Establishments engaged in the maintenance or repair of office machines, household appliances, furniture, and similar items. This classification excludes maintenance and repair of motor vehicles (see Automotive/Vehicle Sales and Services) and personal apparel (see General Personal Services).

Nurseries and Garden Center. Any establishment(s) primarily engaged in retailing nursery and garden products, such as trees, shrubs, plants, seeds, bulbs, and sod that are predominantly grown elsewhere. These establishments may sell a limited amount of a product they grow themselves. This classification includes commercial and wholesale greenhouses and nurseries offering plants for sale.

Offices. Offices of firms, organizations, or public agencies providing professional, executive, management, administrative or design services, such as accounting, architectural, computer software design, engineering, graphic design, interior design, investment, insurance, and legal offices, excluding banks and savings and loan associations with retail banking services (see Banks and Financial Institutions). This classification also includes offices where medical and dental services are provided by physicians, dentists, chiropractors, acupuncturists, optometrists, and similar medical professionals, including medical/dental laboratories within medical office buildings, but excludes clinics or independent research laboratory facilities (see Research and Development) and hospitals.

Business, Professional, and Technology. Offices of firms, organizations, or agencies providing professional, executive, management, administrative, financial, accounting, or legal services, but excluding those that primarily provide direct services to patrons that visit the office (see Offices, Walk-In Clientele).

Medical and Dental. Offices providing consultation, diagnosis, therapeutic, preventive, or corrective personal-treatment services by doctors and dentists; medical and dental laboratories that see patients; and similar practitioners of medical and healing arts for humans licensed for such practice by the State of California. Incidental medical and/ or dental research within the office is considered part of the office use if it supports the on-site patient services.

Walk-In Clientele. Offices providing direct services to patrons or clients without prior appointments. This use classification includes employment agencies, insurance agent offices, real estate offices, travel agencies, utility company offices, and offices for elected officials. It does not include banks or check-cashing facilities, which are separately classified and regulated (see Banks and Financial Institutions).

General Personal Services. An establishment providing non-medical services to individuals as a primary use, of personal convenience, as opposed to products that are sold to individual consumers, or from/by companies. Personal services include barber and beauty shops, massage establishments, shoe and luggage repair, photographers, laundry and cleaning services and pick-up stations, copying, repair and fitting of clothes, and similar services.

Retail Sales.

Convenience Markets. Establishments primarily engaged in the provision of frequently or recurrently needed small personal items or services for residents within a reasonable walking distance. These include various general retail sales and personal services of an appropriate size and scale to meet the above criteria. Typical uses include neighborhood grocery stores, convenience markets, and drugstores.

Food and Beverage Sales. Retail sales of food and beverages for off-site preparation and consumption. Typical uses include food markets, groceries, liquor stores, and retail bakeries.

General Retail. The retail sale or rental of merchandise. not specifically listed under another use classification. This classification includes retail establishments with 25,000 square feet or less of sales area; including department stores, clothing stores, furniture stores, pet supply stores, small hardware stores (with 10,000 square feet or less of floor area), and businesses retailing the following goods: toys, hobby materials, handcrafted items, jewelry, cameras, photographic supplies and services (including portraiture and retail photo processing), medical supplies and equipment, pharmacies, electronic equipment, sporting goods, kitchen utensils, hardware, appliances, antiques, art galleries, art supplies and services, paint and wallpaper, carpeting and floor covering, office supplies, bicycles, video rental, and new automotive parts and accessories (excluding vehicle service and installation). Retail sales may be combined with other services such as office machine, computer, electronics, and similar small-item repairs.

Private Garden. A private food-producing garden that is accessory to the primary use of the site.

Recharging Station. A public or private parking space that is served by battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive means) to a battery or other energy storage device in an electric vehicle.

Solar Energy System. Roof- or building-mounted solar energy systems attached to the top of a building or structure, generally secured using racking systems that minimize impacts and is mounted either level with the roof or tilted toward the sun. Does not include ground-mounted or freestanding solar energy systems.

Signs

A-Frame (sandwich board) sign: A temporary sign that is moveable and not secured or attached to the ground and that is constructed so as to form an "A" or tent-like shape, hinged or not hinged at the top with each face held at an appropriate distance by a supporting member.

Architectural ledge sign: A sign with individual letters, numbers, symbols, etc. which stand on top of a horizontal projection from a building wall that forms a narrow shelf or architectural projection.

Animated Sign: A sign that uses movement or change of lighting to depict action or create a special effect or scene.

Awning sign: A sign that is printed on or attached to an awning valence.

Building edge sign: Sign placed on the corner of a building that is located at the intersection of two public streets (excluding alleys).

Building frontage: The building elevation that faces a street or major parking area, and that is roughly parallel to it.

Cabinet sign: Sign that contains all the text and/ or logo symbols within a single enclosed cabinet and may be illuminated.

Canopy: A protective roof-like covering, often of canvas, mounted on a frame over a walkway or projecting from a wall over a door or window.

Canopy sign: A sign that is printed on or attached to a canopy typically located above a building entrance or store window.

Captive balloon sign: Any object inflated with hot air or lighter-than-air gas that is tethered to the ground or a structure.

Channel letters: Individually cut three-dimensional letters, numbers or graphics that can be either illuminated or non-illuminated or solid backlit (reverse channel letters).

Commercial signage: Any non-residential signage.

Commercial use: A land use or other activity involving the sale of goods or services.

Directional sign: A freestanding on-site sign designed to direct or guide pedestrians or vehicular traffic that is non-advertising in nature, except for logo.

Electronic message center: A sign with a changing display/ message display: A sign capable of displaying words, symbols, figures or images that can be electronically or mechanically changed by remote or automatic means.

Freestanding banner sign: A sign that is supported by a base that rests on the ground and is not supported by or attached to a building.

Halo or halo-type sign. A sign where the light source is concealed behind an opaque face and the rays of illumination are projected outwards toward the edge of the sign forming a "halo" effect around the exterior of the sign. Light does not pass through the face of a halo sign.

Hanging sign (also under-canopy sign): Hanging signs are similar to projecting signs except that they are suspended below a marquee or under a canopy. Hanging signs are generally smaller than projecting signs due to their lower mounting height.

Historical sign: Sign listed on the DPSP register of historical signs.

Historical structure: Any building or structure located in the DPSP that is designated as a historic landmark, a contributor to a designated historic district, or identified in the DPSP as a potential historic landmark.

Inflatable sign: A sign that is a cold air inflated object, which may be of various shapes, made of flexible fabric, resting on the ground or structure and equipped with a portable blower motor that provides a constant flow of air into the device. Inflatable devices are restrained, attached, or held in place by a cord, rope, cable or similar method. The term inflatable device shall not include any object that contains helium, hot air or a lighter-than-air substance.

LED (*light emitting diode*) *sign*: A sign that uses LEDs to display an image or sign copy.

Marquee sign: A sign that is attached to or made part of any permanent roof-like structure projecting beyond a building or extending along a wall of a building.

Monument sign: A low-profile freestanding sign constructed on or supported solely by a pedestal base, or similar ground structure, and that has a horizontal dimension equal to or greater than its vertical dimension.

Nonconforming sign: A sign lawfully constructed and/or installed before the adoption of this Sign Guide that does not conform to the requirements of the Sign Guide.

Off-site signs: A sign advertising uses, facilities, products, or services, not located or sold on the premise on which the sign is located.

Over-Sized Projecting Sign: Vertically oriented projecting sign with curvilinear silhouettes faced with painted graphics and visible neon. These signs are double-sided and project from a building wall a portion of which may extend above the building roof. Sign copy is stacked vertically rather than horizontally. These types of signs were historically located on buildings built from the 1920s through the 1940s.

Painted wall sign: A sign that is painted or silk screened onto a building wall or freestanding structure.

Pedestrian sign: A sign near street or sidewalk level that is oriented and scaled to the pedestrian rather than the motorist. Projecting and under canopy signs are examples of pedestrian signs.

Permanent banner sign: Vertically oriented sign that hangs from projecting metal brackets.

Pole sign: A freestanding sign that is built or attached to one or more poles or posts that does not meet the requirements of a monument sign.

Principal building façade: The front or main part of a building facing a street or public space, especially one with enhanced architectural treatment.

Projecting sign: A sign attached to a building and extending from the wall at right angles to the wall.

Restaurant menu sign: A freestanding or wall sign that incorporates a menu for the restaurant on the same site.

Sans serif: Term used to describe typeface fonts without serifs. Serifs are small horizontal lines extending from the tops and bottoms of a typeface letter.

Sign: Any device (including, but not limited to, letters, words, numerals, figures, emblems, pictures or any part of combination) that is designed, intended, used or located so as to be attract the attention of the public.

Sign area: Area of the sign included within the outer dimensions of a sign face display area including all portions not part of the necessary supporting structure and excluding frame and supports. For wall signs that do not have a frame or separate background or discernable boundary, the area shall be calculated by measuring the area enclosed by straight lines drawn to enclose the extremities of the letters, numbers, symbols, graphics, symbols, background material, designs, and pictures that area part of the advertisement including those areas between letters, words intended to be read together and any device intended to draw attention to the sign message included in the computation of the sign area.

Temporary Sign. A category of signs intended to be displayed for a limited time.

Tenant directory Sign: The tenant directory sign is used to identify multi-tenant buildings and businesses that may not have direct frontage on a public street.

Under-Canopy Sign. See definition for hanging sign.

Wall Sign: A sign attached to or painted on a wall of a building or structure such that the wall is the supporting structure for or forms the background surface of the sign and which does not project more than 12 inches from the building or structure.

Window Signs: Interior signs that are painted, posted, displayed or etched on an interior transparent surface, including windows or doors, and that are located within two feet of a window.

Window Graphic Signs: Interior signs painted on vinyl or similar material that are attached to the back of a storefront window and that cover no more than 20% of the window.

Inland Valley Daily Bulletin

(formerly the Progress Bulletin) 9616 Archibald Avenue Suite 100 Rancho Cucamonga, CA 91730 909-987-6397 legals@inlandnewspapers.com

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POMONA CITY OF LEGAL ATTN: CITY CLERK PO BOX 660 POMONA, CA 91769

PROOF OF PUBLICATION (2015.5 C.C.P.)

STATE OF CALIFORNIA County of Los Angeles

I am a citizen of the United States, I am over the age of eighteen years, and not a party to or interested in the above-entitled matter. I am the principal clerk of the printer of INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation printed and published daily for the City of Pomona, County of Los Angeles, and which newspaper has been adjudged a newspaper of general circulation by the Superior Court of the County of Los Angeles, State of California, on the date of June 15, 1945, Decree No. Pomo C-606. The notice, of which the annexed is a true printed copy, has been published in each regular and entire issue of said newspaper and not in any supplement thereof on the following dates, to wit:

07/05/2019

I declare under the penalty of perjury that the foregoing is true and correct.

Executed at Rancho Cucamonga, San Bernardino Co., California, on this 8th day of July, 2019.

Evenglin almide

Signature

(Space below for use of County Clerk Only)

Legal No.

0011288751

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NOTICE OF PUBLIC HEARING

Project Title: Comprehensive Update to the Downtown Pomona Specific Plan

Project Applicant: City of Pomona

Area generally bounded by Holt Avenue to the North, Towne Avenue to the East, Mission Boulevard to the South, and White Avenue to the West. **Project Location:**

Introduction and First Reading of a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019). **Project Description:**

Lead Agency: City of Pomona, Development Services Department, Planning

Division

Public Hearing Date Location/Time:

The public hearing is scheduled for Monday, July 15, 2019 at & 7:00 p.m. in the City of Pomona City Council Chambers located at 505 S. Garey Avenue in the City of Pomona.

Environmental Review

According to \$15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified," except as might be necessary to determine whether there are project-specific significant effects. Staff has evaluated the proposed project's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether it would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Environmental impacts associated with the proposed project would be similar to those anticipated for the project area in the 2014 General Plan Final EIR and would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to \$15183 of the CEQA Guidelines, no additional environmental review or documentation is required under CEQA. This environmental review will be considered by the City Council at the public hearing for this project scheduled for July 15, 2019.

Public Hearing Notice

Any interested individual may appear in person or by agent at the Planning Commission hearing and be heard on any matter relevant to such proceedings. The staff report on this matter will be available in the Planning Division on or about July 11, 2019. For questions, please call Ata Khan, Senior Planner, Planning Division at (909) 620-7365.

PLEASE NOTE: If you challenge this matter in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Pomona, Planning Division at, or prior to, the public hearing.

Para Información en Español, Ilame (909) 620-2191.

Date: July 1, 2019

> Anita D. Gutierrez, AICP **Development Services Director**

Publication Date: July 5, 2019

Rosalia Butler City Clerk, City of Pomona

Inland Valley Daily Bulletin

Ad#11288751

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Downtown Pomona Specific Plan

Finding of Consistency

prepared for

City of Pomona

505 South Garey Avenue, Box 660 Pomona, California 91769

prepared with the assistance of

Rincon Consultants, Inc.

250 East 1st Street, Suite 1400 Los Angeles, California 90012

June 2019



This report prepared on 50% recycled paper with 50% post-consumer content.

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City of Pomona Downtown Pomona Specific Plan	n	
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1 Introduction

This Finding of Consistency evaluates the updated Downtown Pomona Specific Plan's consistency with the Final Environmental Impact Report (hereafter referred to as the 2014 Final EIR) for the City of Pomona General Plan Update. The 2014 Final EIR, which was certified in March 2014, establishes the City's vision for development of the City through the year 2035. This Finding of Consistency has been prepared pursuant to the applicable provisions of §15183 of the California Environmental Quality Act (CEQA) Guidelines.

While the City's General Plan establishes the framework for future development and provides comprehensive policies for the City, the Downtown Pomona Specific Plan (hereafter referred to as "Specific Plan") is a comprehensive document that implements the vision for the Specific Plan area as established by the General Plan. The Specific Plan was originally adopted in 1994 and has had several targeted updates. In 2017, the City of Pomona began a comprehensive update to the Specific Plan, funded by a grant from the Los Angeles County Metropolitan Transportation Authority (Metro) Round 4 Transit Oriented Development (TOD) Planning Grant Funds. The Specific Plan provides for more precise implementation of the goals, objectives, and policies outlined in the General Plan. As described in the updated Specific Plan, Downtown Pomona is centrally located near several freeways and major universities and the Specific Plan area encompasses approximately 232 acres with boundaries at Center Street to the north, Towne Avenue to the east, Fourth Street to the south, and White Avenue to the west (RRM Design Group 2019). Figures 1 and 2 show the regional context and boundaries of the Specific Plan area, respectively.

General Plan Vision

As envisioned in the General Plan, Downtown Pomona is the community's center of business, culture, and living. The Specific Plan area contains destinations such as the Pomona Transit Center and pedestrian bridge, the Arts Colony, Thomas Street, the Fox Theater, and Western University of Health Sciences. The General Plan reinforces Downtown as the mixed-use "heart of the City," and emphasizes available activities and transit service for locals and visitors. The greatest building heights and development intensities will be focused Downtown, which, when paired with the Pomona Transit Center, would encourage an environment where people can live and work without relying on automobiles. Streetscape and infrastructure improvements will enhance the City's walkability and the visual character while new parks and outdoor gathering spaces will make the district more community-oriented. Surrounding the Downtown core, the Downtown neighborhood will consist of a range of contemporary housing types and compatible uses, all concentrated within walking distance of the core's theater, shops, restaurants, cafes, nightlife, and amenities. As infill development proceeds and the region continues to invest in its transit infrastructure, the neighborhood will benefit from an increasing level of service and transit ridership generated by the Pomona Transit Center (RRM Design Group 2019).

Downtown Pomona Specific Plan

The updated Specific Plan would facilitate development in Downtown Pomona consistent with the vision for the area under the General Plan. As shown in Figure 1, the Plan area is in the City's center, where future mixed-use, streetscape, and infrastructure improvements would promote the Plan area as the "heart of the City". The Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height, setbacks) that reflect the area's visual character as envisioned by the City's General Plan. Figure 3 shows the individual district boundaries of the Plan area's four districts: Mixed Use – Central Business District (MU-CBD), Mixed Use – Institutional (MU-I), Mixed Use – High Density Residential (MU-HDR), and Residential – Multifamily (R-MF).

As discussed in the Specific Plan, the Mixed Use – Central Business District would allow for mixeduse development to support a pedestrian-friendly urban environment and provide for a variety of employment, entertainment, cultural, and shopping opportunities for City residents and visitors. The Mixed Use – Institutional District would serve as the major governmental, educational and office center for Downtown Pomona, but would still allow for mixed-use development with residential and retail uses. Institutional uses envisioned for this district would include support uses for the adjacent civic center (e.g., notaries, copy centers, legal counsel) and higher education development, particularly a campus expansion for Western University. With the location of the Pomona Transit Center within the Mixed Use - High Density Residential District, this district is envisioned as a pedestrian-friendly, high-density commuter residential neighborhood with ancillary retail, service, and office uses. The Residential – Multifamily District is intended to provide areas for a wide variety of medium to high-density residential development (e.g., townhouses, condominiums, apartment buildings) and schools, daycare centers, parks, and community facilities. These districts are consistent with the General Plan's land use designations for the Plan area, which consist of Activity Center, Transit-Oriented District: Core, Transit-Oriented District: Neighborhood, and Urban Neighborhood. Overall, consistent with the vision for the Plan area under the General Plan, the Specific Plan includes goals, policies, and regulations to facilitate an increase in mixeduses, pedestrian-friendly streets, and transit mobility that would encourage residential and commercial activity in Downtown Pomona.

Plan Area Refinements

As part of the Downtown Pomona Specific Plan update effort, City staff identified related refinements to the Plan area's boundaries and designations. Prior to the update effort, many parcels were included in both the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan. Associated refinements delineate just one governing specific plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. These refinements would increase the Plan area to approximately 380 acres with boundaries at Holt Avenue to the north, Towne Avenue to the east, Mission Boulevard to the south, and White Avenue to the west. Figure 4 and Table 1 provide an exhibit and matrix describing boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Nonetheless, these refinements would also facilitate an increase in land uses that would encourage residential, commercial, and open space activity in Downtown Pomona.

CEQA Guidelines Applicability

According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified", except as might be necessary to determine whether there are project-specific significant effects. Therefore, the following discussion evaluates the updated Specific Plan's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether the Specific Plan would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR.

Figure 1 Regional Context



Source: RRM Design Group, 2019

Figure 2 Specific Plan Area



Source: RRM Design Group, 2019

City of Pomona Downtown Pomona Specific Plan	1	
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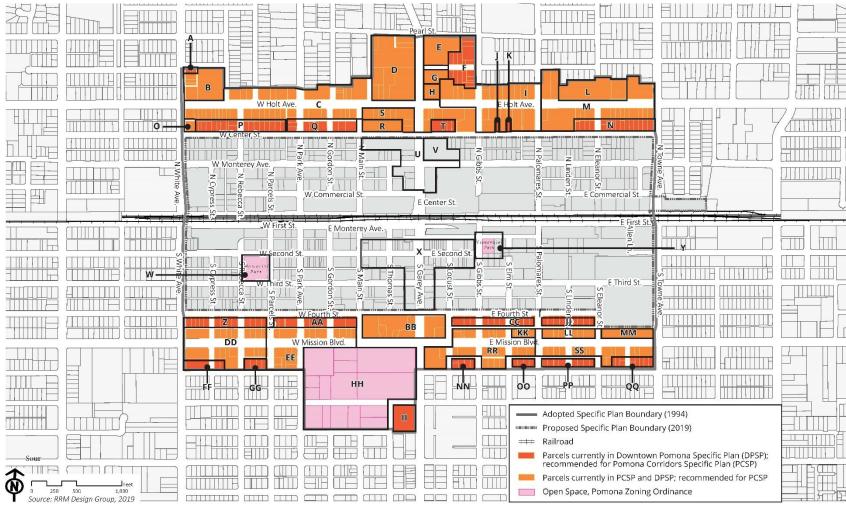
Figure 3 Specific Plan Districts



Source: RRM Design Group, 2019

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Figure 4 Specific Plan Area Refinements



As discussed under *Plan Area Refinements*, Table 1 shows a comparison between the existing and proposed land uses associated with the boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Associated refinements delineate just one governing specific plan between the 1994 Downtown Pomona Specific Plan and the 2014 Pomona Corridors Specific Plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. Nonetheless, these refinements would also facilitate an increase in land uses that would encourage residential, commercial, and open space activity in Downtown Pomona.

Table 1 Refinements Matrix

Area	Existing Land Use(s)	Proposed Land Use
А	DPSP Mixed Use-Institutional (MU-I)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
В	DPSP Mixed Use-Institutional (MU-I) PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
С	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
D	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
E	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
F	DPSP Mixed Use-Institutional (MU-I)	
G	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
Н	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
I	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	PCSP City Gateway Segment + Height Overlay (CG + HT)
J	DPSP Mixed Use-Arterial Retail (MU-AR)	_
K	DPSP Mixed Use-Arterial Retail (MU-AR)	_
L	DPSP Residential-Multi-Family (R-MF); PCSP City Gateway Segment (CG)	PCSP City Gateway Segment (CG)
M	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP City Gateway Segment (CG)	_
N	DPSP Mixed Use-Arterial Retail (MU-AR)	_
0	DPSP Residential-Single-Family (R-SF) PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
Р	DPSP Residential-Single-Family (R-SF)	_
Q	DPSP Mixed Use-Arterial Retail (MU-AR)	

Area	Existing Land Use(s)	Proposed Land Use
R	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	
S	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Neighborhood Center + Height Overlay (NC + HT)	PCSP Neighborhood Center + Height Overlay (NC + HT)
Т	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
U	DPSP Mixed Use-High Density Residential (MU-HDR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	DPSP Mixed Use-High Density Residential (MU-HDR)
V	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
W	DPSP Mixed Use-Institutional (MU-I); DPSP Residential-Single-Family (R-SF)	Pomona Zoning Ordinance Open Space (PZO OS)
X	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Downtown Core (DTC)	DPSP Mixed Use-Central Business District (MU-CBD)
Υ	DPSP Mixed Use-Institutional (MU-I)	Pomona Zoning Ordinance Open Space (PZO OS)
Z	DPSP Residential-Single-Family (R-SF)	PCSP Midtown Segment (MT)
AA	DPSP Mixed Use-Central Business District (MU-CBD)	-
ВВ	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Downtown Core (DTC)	PCSP Downtown Core (DTC)
СС	DPSP Mixed Use-Institutional (MU-I)	PCSP Midtown Segment (MT)
DD	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Midtown Segment (MT)	
EE	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Midtown Segment (MT)	-
FF	DPSP Mixed Use-Arterial Retail (MU-AR)	-
GG	DPSP Mixed Use-Arterial Retail (MU-AR)	-
НН	DPSP Mixed Use-Institutional (MU-I)	Pomona Zoning Ordinance Open Space (PZO OS)
II	DPSP Mixed Use-Institutional (MU-I)	PCSP Midtown Segment (MT)
IJ	DPSP Mixed Use-Institutional (MU-I)	
KK	DPSP Mixed Use-Institutional (MU-I); PCSP Midtown Segment + Height Overlay (MT + HT)	PCSP Midtown Segment + Height Overlay (MT + HT)
LL	DPSP Mixed Use-Institutional (MU-I) PCSP Midtown Segment (MT)	PCSP Midtown Segment (MT)
MM	DPSP Residential-Multi-Family (R-MF); PCSP Midtown Segment (MT)	-

Area	Existing Land Use(s)	Proposed Land Use
NN	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Midtown Segment + Height Overlay
00	DPSP Mixed Use-Arterial Retail (MU-AR)	(MT + HT)
PP	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Midtown Segment (MT)
QQ	DPSP Mixed Use-Arterial Retail (MU-AR)	
RR	DPSP Mixed Use-Arterial Retail (MU-AR);	PCSP Midtown Segment + Height Overlay
	PCSP Midtown Segment + Height Overlay (MT + HT)	(MT + HT)
SS	DPSP Mixed Use-Arterial Retail (MU-AR);	PCSP Midtown Segment (MT)
	PCSP Midtown Segment + Height Overlay (MT + HT)	-

As shown in Table 1, the 2014 Pomona Corridors Specific Plan would govern most of the proposed land uses, which are defined as follows (Pomona 2014):

- Downtown Core (DTC) Promote a vibrant mixed-use urban shopping district. Facilitate the augmentation of the retail core with additional daytime and nighttime activity anchors, shopping, eating, and entertainment venues, residences, offices, and hotel rooms, as well as a widening variety of pedestrian amenities, connected streets, and public spaces.
- Downtown Gateway Segments (DG) Provide for a mixture of townhomes, smaller scale multi-family buildings, and single and multiple tenant retail shops, services, offices, or hotels all oriented towards the street. Encourage streetscape improvements including park-like, treelined landscaped medians to help break up wide corridors, continuous sidewalk-side street tree canopies and planter strips to create a comfortable "buffer zone" for pedestrians, and broad sidewalk to promote walkability.
- City Gateway Segments (CG) Provide for new multifamily housing that would mix compatibly with commercial, office, and lodging neighbors. Encourage streetscape improvements that reinforce these segments as important, high volume, and high visibility concentrations of commercial sales and services, as well as potential locations for higher density housing.
- Midtown Segments (MT) Promote housing and housing compatible development that fronts onto the street and would create a neighborhood edge along streets lined with shade trees and decorative neighborhood-scale streetlights. In areas characterized by large scale blocks, new development would break up "superblocks" with new internally connected streets for buildings to face and for access to parking facilities. The resulting smaller blocks would increase neighborhood walkability.

2 Environmental Impacts

The following discussion addresses each of the environmental issues studied in the 2014 Final EIR for the City's General Plan Update, comparing the effects of the updated Specific Plan, including the boundary and designated refinements, to the effects of the adopted General Plan and overall consistency between the two documents. These environmental issues include aesthetics, air quality, biological resources, cultural resources, geology and soils, greenhouse gases, hazards and hazardous materials, hydrology and water quality, land use and planning, noise, population and housing, public services, recreation, transportation and traffic, and utilities and service systems. However, in December 2018, Appendix G (CEQA Checklist) of the CEQA Guidelines was amended and two new environmental issues were added: energy and wildfire. These topics are discussed under *Greenhouse Gases* and *Hazards and Hazardous Materials* in the following discussion.

Aesthetics

Scenic Vistas

As stated in the 2014 Final EIR, scenic views in the City include background mountain views and depend upon the viewer's vantage point and orientation. The 2014 Final EIR concluded that development of new urban uses would be regulated by the General Plan's policies and result in a less than significant impact to scenic vistas. Given the City's surrounding hillside and mountain views, new development facilitated under the Specific Plan would potentially affect scenic vistas from various Downtown locations. However, as envisioned in the General Plan Update, Downtown Pomona is currently slated for the greatest building heights and development intensities in the City, which would be further regulated by the Specific Plan. Nonetheless, the Plan area is flat and urbanized. Development facilitated by the Specific Plan would be consistent to what was considered in the 2014 Final EIR and would not significantly affect any identified scenic vistas. Therefore, the Specific Plan would not result in an impact to scenic vistas beyond that identified in the 2014 Final EIR.

Scenic Resources

As stated in the 2014 Final EIR, no officially designated State Scenic Highways traverse the City and development under the General Plan would not impact scenic resources. Although State Route 57 between State Route 90 and State Route 60 is considered eligible by the California Department of Transportation for this designation, the eligible highway segment is located approximately 2.5 miles southwest from City limits and existing development interrupts views from the from the highway of the City and potential future development that would occur under the Specific Plan. Therefore, similar to what was determined in the 2014 Final EIR, the Specific Plan would not impact scenic resources within a State-designated scenic highway.

Visual Character

As concluded in the 2014 Final EIR, the General Plan contains goals and policies designed to enhance the City's visual character through complementary development and result in a less than significant on the visual character of the City. The Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height) that reflect the area's visual character as envisioned by the City's General Plan. As

stated in the 2014 Final EIR, the General Plan emphasizes building form and character in districts and neighborhoods to allow a mix of land uses, a renewed Downtown Pomona, and redefined corridors. The Specific Plan would implement goals, policies, and regulations to facilitate an increase in mixed-uses, pedestrian-friendly streets, and transit mobility that would encourage residential and commercial activity in and around the Plan area. New development under the Specific Plan would change the visual character of Downtown; however, such development would be consistent with the General Plan's vision for this area and subject to existing building and development standards in the City's Municipal Code. The Specific Plan would not result in an impact to visual character beyond that identified in the Final EIR.

Light and Glare

As stated in the 2014 Final EIR, development under the General Plan would primarily result in infill of vacant lands, as well as intensification and the reuse of existing sites, and most new development would not be located adjacent to large pieces of undeveloped lands where added light and glare would result in an impact. Similarly, the Specific Plan area is primarily built-out, and a substantial amount of ambient light from urban uses (e.g., buildings, parking lots or structures, vehicle surfaces or headlights) already exists. Therefore, new development under the Specific Plan would not create a significant change compared to existing lighting conditions. Furthermore, the Specific Plan includes standards for sign illumination permitted in the Plan area, including the requirement for light sources to be shielded and directed away from adjacent residential uses. Consistent with the finding in the 2014 Final EIR for the General Plan Update, the Specific Plan would also not result in an impact from increased light and glare.

Air Quality

Air Quality Management Plan

As stated in the 2014 Final EIR, projects that are consistent with the growth projections in the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS) prepared by the Southern California Association of Governments (SCAG) are considered consistent with the South Coast Air Quality Management District (SCAQMD) Air Quality Management Plan (AQMP) growth projections. The 2014 Final EIR concluded that implementation of the General Plan would facilitate population growth consistent with SCAG forecasts. Overall, the Specific Plan would encourage higher density, mixed-use, walkable development around activity centers and transit stations in the Plan area as envisioned by the General Plan and would not generate growth beyond levels anticipated by the General Plan. By facilitating increased density, mixed-uses, and transit mobility, implementation of the Specific Plan would also reduce per capita air pollutant emissions associated with vehicle use, which is also consistent with the AQMP. Therefore, as with the General Plan, the Specific Plan would be consistent with the AQMP growth projections and air quality control measures.

Air Quality Emissions

Individual development projects facilitated by the Specific Plan would generate temporary construction and long-term operational air pollutant emissions (e.g., vehicle trips and stationary sources), including potential increases in carbon monoxide (CO) concentrations and odors. Nonetheless, development facilitated by the Specific Plan would be consistent to what was considered in the 2014 Final EIR and would not generate a new impact related to air quality when

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compared to buildout under the General Plan. Furthermore, the South Coast Air Basin has been in attainment of state and federal CO standards for several years. As determined in the 2014 Final EIR, future development associated with the General Plan would not result in traffic congestion at intersections that would cause an exceedance of CO standards. The Specific Plan would encourage development around activity centers and transit stations in the Plan area as envisioned by the General Plan. Therefore, because development intensity and associated trip generation would be consistent with the General Plan, the Specific Plan would not generate CO concentrations exceeding levels anticipated by the General Plan. Furthermore, odors from new residential, commercial, and open space uses would be consistent with existing odors throughout the Plan area. Overall, the Specific Plan would not result in air quality impacts from construction or operation emissions beyond those identified in the 2014 Final EIR.

Biological Resources

As discussed in the 2014 Final EIR, new or intensified development in Pomona could lead to the direct or indirect degradation of special-status species, sensitive habitats, nesting avian species, natural and man-made wetlands, that would be subject to potential impacts from construction (e.g., demolition, fugitive dust) and operation (e.g., noise, lighting) of urban uses in the Plan area. The Specific Plan would facilitate new and intensified development in Downtown Pomona, which does not support a wide diversity of biological resources, does not contain wildlife movement corridors, and does not pertain to a Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. Nevertheless, as with the General Plan, development under the Specific Plan would be subject to federal and state natural resources regulations and Mitigation Measure BIO-2 from the Final EIR, which would reduce impacts to nesting avian species. Because the Plan area is urbanized, impacts to special-status species, sensitive habitats, and wetlands would not occur under the Specific Plan. Furthermore, development under the Specific Plan would be subject to the City's Oak Tree Preservation Ordinance, which prohibits trimming, pruning, cutting, relocation or removal of any oak tree subject to the Ordinance unless a valid permit has been issued by the City. Based on these facts, the Specific Plan would not result in an impact to biological resources beyond that identified in the 2014 Final EIR.

Cultural Resources

Historic Resources

As discussed in the 2014 Final EIR, several designated historic resources are in or in proximity to Downtown Pomona and implementation of the General Plan would result in a significant, but mitigable, impact to historic resources. Similarly, any future development project facilitated by the Specific Plan that is located near an identified historical resource in Downtown would have the potential to result in a substantial adverse change to a historical resource. As with the General Plan, individual development projects would be subject to Mitigation Measure CUL-1 from the Final EIR, which would require historical evaluation of any affected property and implementation of methods aimed at reducing impacts to historic resources. The Specific Plan would not result in an impact to historic resources beyond that identified in the Final EIR.

Cultural Resources

As concluded in the 2014 Final EIR, ground-disturbing activities associated with development under the Specific Plan could result in damage or destruction to archaeological resources, paleontological resources, and human burials; however, compliance with existing federal, state, and local policies

would reduce impacts to a less than significant level. Although the Plan area is built-out, ground-disturbing activities that could occur under the Specific Plan would also have the potential to damage or destroy historic or prehistoric archaeological resources, paleontological resources, and human burials outside of formal cemeteries that may be present on or below the ground surface. However, as stated in the 2014 Final EIR, various goals and policies in the General Plan would require a qualified on-site archaeological and/or paleontological monitor whenever there is a potential to affect cultural resources during ground-disturbing as well as the scientific recovery and evaluation of any unearthed cultural resource. Furthermore, Section 5097 of the California Public Resources Code and Sections 7050.5, 7051, and 7054 of the California Health and Safety Code have specific provisions for the protection and treatment of human burials. Therefore, as with the General Plan, the Specific Plan would comply with existing federal, state, and local policies and result in less than significant impacts to cultural resources. Overall, the Specific Plan would not result in an impact to cultural resources beyond that identified in the 2014 Final EIR.

Geology and Soils

As concluded in the 2014 Final EIR, implementation of the General Plan would result in additional structures in the City with exposure to geologic impacts; however, compliance with applicable building standards would reduce impacts to less than significant levels. The Specific Plan would facilitate development in Downtown Pomona as envisioned by the City's General Plan, resulting in an increase in residential and mixed-use buildings in the City consistent to what was anticipated for the Plan area and considered in the 2014 Final EIR. Additional residents and employees would be potentially exposed to the effects of strong seismic ground shaking, seismic-related ground failure, liquefaction or landslides, erosion or loss of topsoil, expansive soils, subsidence, or collapse. Individual Downtown area developments would be required to comply with the California Building Code (CBC), which provides standards for excavation, grading, and earthwork construction; fills and embankments; expansive soils; foundation investigations; and liquefaction potential and soils strength loss. In addition, as with all new development under the General Plan, new development under the Specific Plan greater than one acre in size would be required to comply with the provisions of the General Construction Activity Stormwater Permit adopted by the State Water Resources Control Board (SWRCB), which would require the employment of Best Management Practices (BMPs) to limit the extent of eroded materials from a construction site. All development on sites of between one and five acres would be required to comply with the provisions of the National Pollution Discharge Elimination System (NPDES) Phase II regulations concerning the discharge of eroded materials and pollutants from construction sites. Furthermore, all developers would be required to obtain a grading permit prior to grading activities. In turn, as described in the 2014 Final EIR, all work requiring a grading permit would be required to have an approved Erosion Control Plan. Overall, the Specific Plan would not result in a geologic impact beyond that identified in the 2014 Final EIR and impacts would be less than significant.

Greenhouse Gases

As stated in the 2014 Final EIR, the General Plan would encourage compact development; promote the establishment and practice of alternative transit, such as walking and biking, as a mode of transportation; increase use of renewable energy resources; and reduce per capita energy consumption, which would be consistent with the goals of SB 375 and would contribute to long-term reductions in per capita greenhouse gas (GHG) emissions. The 2014 Final EIR concluded that the increase in per capita GHG emissions under the General Plan would be less than significant.

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Development facilitated by the Specific Plan would generate new GHG emissions through vehicle use and energy consumption. However, as discussed under *Air Quality*, the Specific Plan would encourage higher intensity, mixed-use, walkable development around activity centers and transit stations in the Plan area as envisioned by the General Plan. By increasing development intensity and encouraging mixed-uses and transit mobility, the Specific Plan would also reduce vehicle trips consistent with the General Plan. Furthermore, as stated the Specific Plan, energy conservation measures required by applicable energy conservation regulations, such as those in the California Green Building Code, would minimize long-term energy use. Overall, development facilitated by the Specific Plan would reduce per capita GHG and other air pollutant emissions, consistent with the goals of the SCAG RTP/SCS and applicable GHG reduction targets. The Specific Plan would not increase GHG emissions beyond what is anticipated in the 2014 Final EIR and all Downtown area development would be subject to regulations in place at the time the development is proposed that are aimed at achieving statewide GHG reduction targets, including the 2030 target of a 40 percent reduction from 1990 emission levels outlined in Senate Bill (SB) 32. The Specific Plan would not result in an impact from GHG emissions beyond that identified in the 2014 Final EIR.

Hazards and Hazardous Materials

Hazardous Waste and Materials

As concluded in the 2014 Final EIR, implementation of the General Plan would result in the transport, use, storage, and disposal of hazardous materials within the City; however, compliance with applicable federal, state, and local standards would reduce associated impacts to less than significant levels. Implementation of the Specific Plan would facilitate development in Downtown Pomona. As with development under the General Plan, construction and demolition activities that could result in the release of lead and/or asbestos would be conducted according to California Occupational Safety and Health Administrations (Cal/OSHA) standards. In addition, developers would comply with all applicable federal, state, and local laws and regulations pertaining to the transport, use, disposal, handling, and storage of hazardous waste, including but not limited to, Title 49 of the Code of Federal Regulations. Although unlikely with residential and mixed-uses, any development that handles or uses hazardous materials would be required to comply with the regulations, standards, and guidelines established by the Environmental Protection Agency (EPA), State, Los Angeles County, and City of Pomona related to storage, use, and disposal of hazardous materials. With compliance with existing applicable federal, state, and local regulations, the Specific Plan would not result in impacts associated with hazardous materials beyond those identified in the 2014 Final EIR.

Site Contamination

As discussed in the 2014 Final EIR, the City contains over 190 sites that have been identified as being contaminated from the release of hazardous substances in the soil, including landfills, sites containing leaking underground storage tanks, and large and small-quantity generators of hazardous waste. The 2014 Final EIR concluded that implementation of state and local regulations as well as General Plan policies would reduce impacts from known contaminated sites in the City to less than significant levels. As shown in Figure 4.6-1 of the 2014 Final EIR, various contaminated sites are were identified in the Specific Plan area. While implementation of the Specific Plan could lead to development of these sites, such development would be required to undergo remediation and cleanup under the Department of Toxic Substances Control (DTSC) and the Regional Water

Quality Control Board (RWQCB) prior to development. The Specific Plan would not result in impacts from contaminated sites beyond those identified in the 2014 Final EIR.

Airport Safety

While parts of the City are in the current airport land use plan of Brackett Field, a public use airport in the City of La Verne operated by Los Angeles County, the refined Downtown Specific Plan area is located approximately two miles south of the airport. Therefore, implementation of the Specific Plan would not result in the construction of residential and mixed-uses within two miles of an airport facility and would not result in a safety impact for people residing or working in the Plan area beyond that identified in the Final EIR.

Emergency Response and Evacuation Plans

The General Plan contains policies to provide for the proper implementation of adopted emergency response and evacuation plans. The 2014 Final EIR concluded that, with implementation of these policies, development under the General Plan would result in less than significant impacts to emergency response and evacuation plans. Because development envisioned for Downtown Pomona under the Specific Plan is consistent with the General Plan, the Specific Plan would not result in an impact to adopted emergency response and evacuation plans beyond that identified in the 2014 Final EIR.

Wildfire Hazards

Parts of Pomona are susceptible to wildland fires due to its hilly terrain, dry weather conditions and the nature of its plant cover. As concluded in the 2014 Final EIR, policies included in the General Plan would reduce the exposure of people or structures to significant risk of loss, injury, or death involving wildland fires to less than significant levels. However, the Downtown Specific Plan area is primarily built-out and located in the City's center (see Figure 1), which is fully urbanized and surrounded by other urban development. Therefore, development facilitated by the Specific Plan would not be directly susceptible to wildfires. The Specific Plan would not result in an impact involving wildland fires beyond that identified in the 2014 Final EIR.

Hydrology and Water Quality

Water Quality and Wastewater

As concluded in the 2014 Final EIR, compliance with the NPDES permit requirements, the City's Municipal Code, and General Plan policies would reduce water pollutants from the operation of new development to the maximum extent practicable, resulting in less than significant impacts to water quality and waste discharge requirements. Development under the Specific Plan could result in an increase in pollutants in stormwater and wastewater, although water quality standards and waste discharge requirements would not be violated. Water quality degradation from construction would be specific to each individual project, and thus would depend largely on the areas affected and the length of time soils are subject to erosion, and construction activities on-site. However, as stated in the Specific Plan, the primary regulatory control for the protection of water quality NPDES permit administered SWRCB. The SWRCB and Los Angeles RWQCB establish requirements prescribing the quality of point sources of discharge and establish water quality objectives. In addition, the City of Pomona protects water resources through implementation of its Storm Water Pollution Prevention Program as required by the NPDES permit program. Further, Chapter 18 (Environment) Division 3

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(Discharge Regulations and Requirements) of the City's Municipal Code sets forth regulations associated with stormwater. All these requirements would apply to development throughout the City, including the Downtown Specific Plan area. Therefore, the Specific Plan would not result in an impact regarding pollutants in stormwater and wastewater beyond that identified in the Final EIR.

Groundwater Supplies, Drainage, Flooding, and Runoff

The 2014 Final EIR concluded that compliance with the NPDES permit requirements, the City's Municipal Code, and General Plan policies, would reduce impacts to groundwater supplies, drainage patterns, downstream floods, and urban runoff to less than significant levels. As with the General Plan, new development facilitated by the Specific Plan would be primarily consists of infill in already urbanized areas, where increases in impervious surfaces would be minimal. As stated in the Specific Plan, only an incremental expansion in the quantity of net new impervious surfaces is expected as a part of future development. In compliance with federal, state, regional and local requirements, contractors constructing new development or redevelopment projects are required to implement BMPs. Furthermore, the Specific Plan includes storm drainage policies, which would provide for storm drain infrastructure and systems (e.g., detention basins, vegetated swales, rain gardens, permeable paving, rainwater harvesting) to remove pollutants from stormwater runoff prior to discharge. Therefore, the Specific Plan would not result in impacts related to groundwater supplies and quality, existing drainage patterns, downstream flooding, and polluted runoff beyond those identified in the 2014 Final EIR.

Hydrological Risks

As stated in the 2014 Final EIR, development under the General Plan would not place people or structures within a flood zone. While the City is within the San Antonio Dam inundation zone, compliance with the City's Municipal Code and General Plan policies would reduce flooding impacts to less than significant levels. The 2014 Final EIR also concluded that, due to the City's geographic location, people or structures in the City would not be impacted by a seiche, tsunami, or mudflow. As stated in the 2014 Final EIR, the potential for a flood event exists within Pomona in the form of dam inundation from the San Antonio Dam and Live Oak Reservoir. While new development under the Specific Plan could increase the quantity of residential uses with risk of inundation, Section 18-640 of the Municipal Code requires all new construction and substantial improvements within a flood-prone area to be designed and adequately anchored to prevent flotation, collapse, or lateral movement. The section also requires the use of flood-resistant building materials and construction methods. Sections 18-641 and 18-642 of the City's Municipal Code establish requirements for the construction of utilities in flood-prone areas. Overall, the Specific Plan would not increase risk of inundation or increase exposure of people, employees, or structures to a flood risk beyond that identified in the 2014 Final EIR.

Land Use and Planning

As stated in the 2014 Final EIR, the General Plan emphasizes building form and character in districts and neighborhoods to allow a mix of land uses, a renewed Downtown Pomona, and redefined corridors. The 2014 Final EIR concluded that no substantial land use or circulation changes that would physically divide an established community would occur under the General Plan. As discussed under *Aesthetics*, the Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height) that reflect the area's visual character as envisioned by the City's General Plan. Figure 3 shows the boundaries of the Plan area's four districts: Mixed Use – Central Business District (MU-CBD), Mixed

Use – Institutional (MU-I), Mixed Use – High Density Residential (MU-HDR), and Residential – Multifamily R-MF). Furthermore, as part of the Downtown Pomona Specific Plan update effort, City staff identified related refinements to the Plan area's boundaries and designations. Prior to the update effort, many parcels were included in both the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan. Figure 4 and Table 1 provide an exhibit and matrix describing boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Nonetheless, the Specific Plan includes goals, policies, and regulations to facilitate an increase in mixed-uses, pedestrian-friendly streets, and transit mobility that, in conjunction with the associated refinements, would encourage residential and commercial activity in and around the Plan area and improve overall connectivity. Therefore, as with the General Plan, implementation of the Specific Plan would not physically divide an established community.

Consistency with Applicable Plans

As concluded in the 2014 Final EIR, implementation of the General Plan would be generally consistent with applicable adopted plans, regulations, or policies. Several regionally and locally adopted land use plans, policies, and regulations are applicable to development under the Specific Plan, which include the SCAQMD AQMP, SCAG RTP/SCS, the City's Municipal Code, and the City's General Plan. As discussed under Air Quality, the Specific Plan would be consistent with development envisioned in the General Plan and within SCAG growth forecasts. By increasing the density, mixed-uses, and transit mobility, implementation of the Specific Plan would also be expected to generally reduce per capita air pollutant emissions associated with vehicle use, which is also consistent with the AQMP. The Specific Plan refines the uses set forth in the General Plan and provides further guidance for development in the Plan area but would not change the future land use pattern for the area relative to the General Plan. Furthermore, associated refinements delineate just one governing specific plan between the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. Therefore, the refined Specific Plan would not generate substantial conflicts with the City's Zoning Map or General Plan. The Specific Plan would not involve consistency conflicts with land use plans, policies, or regulations not identified in the 2014 Final EIR.

As discussed in the 2014 Final EIR, the City does not currently have a habitat conservation or natural community conservation plan. The Downtown Specific Plan area also is not subject to a habitat conservation or natural community conservation plan. Furthermore, development under the Specific Plan would be subject to the City's Oak Tree Preservation Ordinance. Therefore, the Specific Plan would not conflict with any habitat conservation plan or natural community conservation plan.

Farmland Conversion

As concluded in the 2014 Final EIR, the City does not contain land designated as Prime Farmland, Unique Farmland or Farmland of Statewide Important by the State's Farmland and Mapping Program. The Downtown Specific Plan area is built-out with urban uses. Therefore, as with the General Plan, the Specific Plan would not convert potential agricultural lands to urban uses or conflict with a Williamson Act contract.

Noise

Temporary and Permanent Noise

As concluded in the 2014 Final EIR, implementation of the City's existing noise regulations and standards, as well as goals and policies of the General Plan, would reduce potential noise impacts related to existing and future land uses to less than significant levels. Development facilitated by the Specific Plan would occur within Downtown Pomona and along redefined corridors and would generate temporary and permanent sources of noise. Under the Specific Plan, the primary source of temporary or periodic noise would be construction activity and maintenance work. However, Section 18-305 of the City's Municipal Code allows noise sources associated with construction, repair, remodeling or grading of any real property or during authorized seismic surveys, provided such activities do not take place between the hours of 8 PM and 7 AM on weekdays, including Saturday, or at any time on Sunday or a federal holiday, and provided the noise level created by such activities does not exceed the noise standard of 65 dB(A) plus the limits specified in section 18-311(b) of the Municipal Code as measured on residential property. With compliance with these restrictions, construction noise impacts associated with buildout of the Specific Plan would not create a significant adverse effect on sensitive receptors. Because development facilitated by the Specific Plan would be similar to that envisioned in the General Plan, operational noise associated with development under the Specific Plan would remain the same as what was considered in the Final EIR. Implementation of the Specific Plan would not result in temporary or operational noise impacts beyond those identified in the Final EIR.

Groundborne Vibration

Groundborne vibration in Pomona is generated primarily by two sources: temporary construction activities and permanent traffic on roadways and railways. As concluded in the 2014 Final EIR, construction vibration would result in less than significant impacts; however, vibration impacts associated with active rail lines would result in significant, but mitigable, impacts. Construction activities under the Specific Plan would also generate groundborne vibration. However, as with individual developments under the General plan, the City would review the potential for such impacts before it issues building permits and would require measures such that physical damage to neighboring buildings would not occur before issuing a building permit. Automotive traffic on roadways and train traffic on railways under the Specific Plan would also produce groundborne vibration. The Specific Plan would facilitate new residential uses in transit-oriented districts subject to potential vibration-related impacts from rail lines along First Street. However, as with the General Plan, the Specific Plan would comply with Mitigation Measure N-1 of the 2014 Final EIR, which would reduce the impact of groundborne vibration on new residential uses located near Pomona's active transit rail line along East 1st Street. Therefore, implementation of the Specific Plan would not result in impacts from groundborne vibration beyond those identified in the 2014 Final EIR.

Airport Noise

The 2014 Final EIR concluded that future development at the Fairplex could include residential uses that could be exposed to airport noise levels in excess of existing thresholds, which would result in a significant, but mitigable, impact. However, as discussed under *Hazards and Hazardous Materials*, the refined Downtown Specific Plan area is located approximately two miles south of Brackett Field. As determined in the 2014 Final EIR, most of the City is located outside of the airport's noise contours, including the Downtown Specific Plan area. While development facilitated by the Specific

Plan would be exposed to temporary and intermittent overflight noise, the Specific Plan would not result in exposure to airport noise.

Population and Housing

Growth

As concluded in the 2014 Final EIR, implementation of the General Plan would not generate growth in population, jobs, and housing in excess of SCAG's RTP/SCS forecasts. As discussed under *Air Quality*, the Downtown Specific Plan would encourage higher density, mixed-use, walkable development around activity centers and transit stations in Downtown Pomona as envisioned by the General Plan but would not generate growth beyond levels anticipated in the General Plan or the 2014 Final EIR. Furthermore, as envisioned by the General Plan, the Specific Plan would facilitate the development of a mix of residential and commercial uses in proximity to one another, which would encourage a balance of jobs and housing. Therefore, the Specific Plan would not result in an exceedance in population and housing projections or increase population growth beyond what is forecast in the 2014 Final EIR.

Displacement

As concluded in the 2014 Final EIR, implementation of the General Plan would not result in the displacement of substantial numbers of people or housing given that the General Plan would facilitate the development of new housing. As with the General Plan, new and intensified development under the Specific Plan would likely displace existing residences. However, consistent with the General Plan, future buildout under the Specific Plan would increase residential density in Downtown Pomona and offset any displaced residences. Therefore, the Specific Plan would not result in the displacement of substantial numbers of people or housing and would not increase impacts related to displacement beyond those identified in the 2014 Final EIR.

Public Services

Fire Protection

As concluded in the 2014 Final EIR, development facilitated by the General Plan increase demand for fire protection services and potentially create the need for new fire protection facilities; however, compliance with applicable codes and regulations and compliance with General Plan Update policies would reduce impacts to a less than significant level. Fire protection services are provided by the Los Angeles County Fire Department (LACoFD) and include fire, rescue, hazardous materials prevention, and emergency services. As stated in the Specific Plan, the LACoFD uses national guidelines for response time targets which consist of five minutes for the first arriving unit for fires and basic life support and eight minutes for advanced life support (paramedic) in urban areas. Pomona is part of the LACFD Division III and the nearest fire department facility is Station 181 - Division and Battalion Headquarters located at 590 South Park Avenue near the southern boundary of the Plan area. Any new development that would occur under the Specific Plan would be in the existing LACoFD service area and would be required to comply with all applicable federal, state, and local regulations governing the provision of fire protection services, including adequate fire access, fire flows, and number of hydrants. These project-specific requirements include construction standards in new structures and remodels, road widths and configurations designed to accommodate the passage of fire trucks and engines, and requirements for minimum fire flow rates

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for water mains. Based on these facts, the Downtown Specific Plan would not result in an impact to existing fire protection facilities nor result in the need for additional fire protection facilities and no impacts beyond those identified in the 2014 Final EIR would occur.

Police Protection

The 2014 Final EIR concluded that development facilitated by the General Plan increase demand for police protection services and potentially create the need for new police protection facilities; however, compliance with applicable codes and regulations and compliance with General Plan Update policies would reduce impacts to a less than significant level. The Pomona Police Department (PPD) provides police protection services in Pomona. As stated in the Specific Plan, the PPD provides services in crime investigation, offender apprehension, community awareness programs, traffic control, and other services. Seven facilities provide police services in Pomona. Of these facilities, three provide first-response services and include the Main Station at 490 W. Mission Boulevard, the Traffic Bureau at 100 West Commercial, and the Aero Bureau at 1905 McKinley. Implementation of the potential development allowed under the Specific Plan was addressed in the Final EIR, which includes policies to reduce crime and provide for law enforcement as the City experiences future growth. Nonetheless, police policies included in the Specific Plan would promote the integration of Crime Prevention Through Environmental Design principles (i.e., Natural Surveillance, Natural Access Control, and Territorial Reinforcement) for new development and public spaces. Overall, the Specific Plan would not result in an impact to police protection facilities or result in the need for additional police protection facilities. No impacts beyond those identified in the Final EIR would occur.

Schools

As concluded in the 2014 Final EIR, implementation of the General Plan would result in an increase in student enrollment; however, schools in the City have adequate capacity to serve the additional students and impacts associated with an increase in students were determined to be less than significant. The Specific Plan area is located in the Pomona Unified School District (PUSD). Existing facilities include elementary, middle and high schools as well as opportunities for higher level education. The increase in residential development under the Specific Plan would increase enrollment in local schools in the PUSD. Nonetheless, as with development under the General Plan, the payment of statutory fees pursuant to Section 65995 (3) (h) of the California Government Code (Senate Bill 50, chaptered August 27, 1998), "...is deemed to be full and complete mitigation of the impacts of any legislative or adjudicative act, or both, involving, but not limited to, the planning, use, or development of real property, or any change in governmental organization or reorganization." Therefore, the Specific Plan would not result in a significant impact related to schools and would have no impact related to schools beyond that identified in the Final EIR.

Recreation

As stated in the Final 2014 EIR, the City's target parkland to population ratio is 3 acres per 1,000 residents. Based on this target, there is an existing deficit in parkland and recreational facilities in the City. The 2014 Final EIR concluded that development of new parkland, bicycle facilities, and pedestrian facilities under the General Plan, as well as implementation of goals and policies associated with the General Plan, would reduce impacts associated with the demand for recreational facilities to less than significant levels. As with development under the General Plan, an increase in population resulting from implementation of the Specific Plan could increase demand on existing parkland and recreational facilities such that deterioration of these facilities would be

accelerated. However, Section 70.69 of the City's Municipal Code requires dedication of park lands or payment of in-lieu fees, or a combination thereof, for the construction of new dwelling units. In addition, improvement and expansion of the existing bicycle and pedestrian network envisioned under the Specific Plan would provide additional recreational facilities for use by City residents in addition to other planned parkland and recreation improvements in the General Plan. Furthermore, park and recreation policies included in the Specific Plan support the expansion of parks, recreation, and public spaces in Downtown Pomona. Based on these facts, the Specific Plan would not result in a significant impact related to recreation and would have no impact to parkland and recreational facilities beyond that identified in the 2014 Final EIR.

Transportation and Traffic

Transportation Facilities

As determined in the 2014 Final EIR, traffic associated with development facilitated by the General Plan would degrade operations at several intersections and three Congestion Management Program (CMP) facilities (i.e., two freeway segments and one intersection), to below identified City standards. While none of these intersections and CMPP facilities are in the Specific Plan area, development facilitated by the Specific Plan would contribute additional vehicle trips throughout the City, including intersections and CMP facilities that are expected to operate unacceptably under buildout of the General Plan. Nonetheless, while the Specific Plan would incrementally contribute to the significant and unavoidable impacts to these intersections and CMP facilities associated with the General Plan, the Specific Plan would not increase traffic impacts to the roadway network beyond those identified in the 2014 Final EIR since it would not increase development intensity or associated vehicle trips compared to the General Plan.

Air Traffic Patters

As discussed in the 2014 Final EIR, there are no airports in the City and implementation of the General Plan would not impact air traffic pattern. As discussed under *Hazards and Hazardous Materials* and *Noise*, the Downtown Specific Plan area is located approximately two miles south of Brackett Field. Therefore, the Specific Plan would not affect air traffic patterns and would have no impact beyond that identified in the 2014 Final EIR.

Traffic Hazards and Site Accessibility

As concluded in the 2014 Final EIR, development under the General Plan would be required to meet City design standards and comply with General Plan policies aimed at improving traffic safety and accessibility, which would reduce impacts associated with traffic hazards to less than significant levels. As with development associated with the General Plan, individual projects under the Specific Plan would be required to meet all applicable local and State regulatory standards for site design and emergency access, including those in the CBC, Municipal Code, and Fire Code. Furthermore, projects under the Specific Plan would be required to undergo independent project-level design review by the City. Potential traffic hazards or inadequate emergency access from a design feature or incompatible use associated with individual projects would be reviewed during this process. Therefore, the Specific Plan would not generate traffic hazards or site accessibility issues and would have no impact beyond that identified in the 2014 Final EIR.

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Alternative Transportation

As concluded in the 2014 Final EIR, implementation of the General Plan would enhance the use of alternative transportation modes in the City and would result in less than significant impacts. Implementation of the Specific Plan would enhance the use of alternative transportation modes, including transit, bicycling, and walking consistent with the General Plan. One of the central directives of the Specific Plan is to create pedestrian-friendly streets that include integrated transportation systems, provide pedestrian and bicycle links, and improve overall vehicular circulation. The General Plan includes a range of policies aimed at enhancement of alternative transportation mode opportunities throughout the City. These and Specific Plan policies aimed at improving transit mobility would reduce impacts related to alternative transportation to a less than significant level such that the Specific Plan would not result in impacts beyond those identified in the Final EIR.

Utilities and Service Systems

Water Supply

As stated in the 2014 Final EIR, implementation of the General Plan would increase demand for water supply; however, the City's 2010 Urban Water Management Plan (UWMP) projects that adequate water supply would be available to serve population growth in the City through 2035. The 2014 Final EIR concluded that development under the General Plan would have less than significant impacts to water supply. As with the General Plan, development facilitated by the Specific Plan would increase demand for water supply. However, water demand projections in the City's Urban Water Management Plan (UWMP) are based on SCAG population growth forecasts for the City. As stated in the 2014 Final EIR, the UWMP projects that the City will have adequate water supply, with normal conservation efforts, to meet demand through 2035 in average year. As discussed under Air Quality, development facilitated by the Specific Plan would be consistent with that envisioned by the General Plan and within SCAG growth forecasts. Therefore, water supplies would be adequate to serve population growth facilitated by the Specific Plan. Furthermore, as discussed in the Specific Plan, the City is signatory to the Memorandum of Understanding regarding Urban Water Conservation in California and is a member of the California Urban Water Conservation Council. As a signatory, the City has pledged to implement 14 conservation BMPs, which include water survey programs, residential plumbing retrofit, system water audits, landscape conservation programs, public and school education programs, conservation pricing, and a low-flush toilet replacement program. In addition to these BMPS, the Specific Plan includes water polices to reduce water consumption, require water saving devices for all development in the Plan area, and explore recycled water opportunities for irrigation. The Specific plan would not result in significant impacts to water supply or involve any impact beyond that identified in the 2014 Final EIR.

Wastewater Generation

The 2014 Final EIR concluded that implementation of the General Plan would not violate wastewater treatment requirements of the RWQCB and impacts associated with wastewater would be less than significant. Implementation of the Specific Plan would result in an increase in wastewater generation, and wastewater generated by new residential growth would require treatment. Wastewater service in Pomona is provided by the City's Public Works Department, and collected wastewater is treated by the Los Angeles County Sanitation Districts (LACSD). As discussed in the Specific Plan, LACSD has adopted ordinances over time establishing legal mechanisms to protect and finance operations and facilities, including the Industrial Wastewater Discharge Permit,

Connection Fee, and Discharge Programs. The Connection Fee Program requires all new uses of the LACSD sewerage system to pay their share of costs for providing additional conveyance, treatment, and disposal facilities. Therefore, as stated in the 2014 Final EIR, existing treatment plant capacity and infrastructure would accommodate planned future growth. The Specific Plan would not result in a significant impact to wastewater treatment facilities and would have no impact beyond that identified in the 2014 Final EIR.

Solid Waste Generation

As stated in the 2014 Final EIR, implementation of the General Plan would increase solid waste generation; however, landfills serving the City have adequate capacity to accept the additional waste and future development would comply with General Plan policies aimed at increasing recycling in the City. The 2014 Final EIR concluded that impacts from solid waste generation under the General Plan would be less than significant. Development facilitated by the Specific Plan would also increase the amount of solid waste sent to area landfills. However, as stated in the 2014 Final EIR, landfills serving the City have adequate capacity to accept the additional waste. Furthermore, as with development under the General Plan, individual developments facilitated by the Specific Plan would also be reviewed on a project-by-project basis and solid waste impacts would be evaluated based on existing and planned disposal facilities and capacities available. The Specific Plan also includes solid waste policies requiring all new development to participate in solid waste source reduction and diversion programs to the maximum extent practical. The Specific Plan would not result in significant impacts related to solid waste generation and would have no impact beyond that identified in the 2014 Final EIR.

Conclusion

As discussed in *Environmental Impacts*, development facilitated by the updated Downtown Specific Plan would be consistent with the development intensity established by the City's General Plan. Environmental impacts associated with the Specific Plan would be similar to those anticipated for the Downtown Plan area in the 2014 General Plan Final EIR and the Specific Plan would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required for the Specific Plan under CEQA.

3 References

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CITY OF POMONA PLANNING COMMISSION REPORT

DATE: June 26, 2019

TO: Chairperson and Members of the Planning Commission

FROM: Development Services Department, Planning Division

SUBJECT: COMPREHENSIVE UPDATE TO THE DOWNTOWN POMONA SPECIFIC

PLAN

A comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and

Pomona Zoning Ordinance (ZONE 12104-2019).

STAFF RECOMMENDATION

The Planning Division recommends the Planning Commission adopt the attached PC Resolutions recommending City Council approval of the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019), as defined in the draft resolution (Attachment 1) and related exhibits (Attachment 2).

PROJECT DESCRIPTION & BACKGROUND

The Downtown Pomona Specific Plan ("DPSP") was first adopted in 1994 and has had several targeted updates since its initial adoption, however the Plan remained in need of a complete update. In 2016, the Planning Division seized an opportunity to pursue grant funding to update the DPSP. The City was awarded a grant from the Los Angeles County Metropolitan Transportation Authority ("Metro Los Angeles") in the amount of \$220,000 to create a regulatory environment supportive of transit-oriented development ("TOD") around station areas and adjacent transit corridors in Downtown Pomona ("Downtown"). The Planning Division selected a consulting firm, RRM Design, to draft the comprehensive update ("Plan update"). Beginning in 2017, RRM Design conducted research, including stakeholder interviews and community forums, and delivered a draft of the Plan update in June 2018. Due to significant turnover in staffing, the Planning Division initiated a draft review six months later in December 2018. As the grant agreement with Metro Los Angeles was set to expire at the end of 2018, a six-month extension request was submitted and approved, giving the Planning Division until July 2019 to finalize the Plan update. To streamline this process, the following four key priorities for the Plan update adoption were established:

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- 1. Reconcile Downtown Pomona Specific Plan boundaries with the Pomona Corridors Specific Plan
- 2. Implement the 2014 City of Pomona General Plan goals and policies
- 3. Achieve Metro's TOD goals
- 4. Streamline the DPSP as a document and the development review process

ANALYSIS

GOAL #1: Reconciling Boundaries

Cleaning Up Dual Zoning

The Pomona Corridors Specific Plan ("PCSP") was adopted in March 2014, affecting parcels along Holt Boulevard, Garey Avenue, and Mission Boulevard, which historically were included in the DPSP. As part of that action, a cleanup was approved to reconcile boundaries between these two plans. However, as the approval only identified rough boundaries, a parcel level cleanup has not been conducted. This has left parcels along these corridors with dual zoning, and in some cases split zoning, which has led to code applicability challenges for entitlement and design review. Moreover, several parcels on the edges of the PCSP remain entirely within the DPSP but are noncontiguous with the rest of Downtown. The Plan update will establish parcel level boundaries (Attachment 2, Exhibits A-C) to clarify code applicability and eliminate non-contiguous Downtown parcels.

Establishing Complementary Boundaries

When adopted, the PCSP included multiple parcels along Garey Avenue identified in either the "Downtown Gateway" or "Downtown Core" segment. This includes land use classifications, development standards, and public right-of-way improvement recommendations. In the "Downtown Core" segment, land use classifications defer to the DPSP, further blurring the lines of the two plans (**Figure 1**). This has effectively minimized the reach of the DPSP along Garey Avenue and diminished boundary identities and land use classifications for either plan in Downtown.

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Figure 1. Conflicting PCSP & DPSP boundaries diminish each Plan's identity



Furthermore, the land use classifications of "Downtown Core" segment in the PCSP will be updated and will no longer rely on the DPSP. Parcels south of Mission Boulevard currently zoned in the DPSP will change to the "Midtown Segment" of the PCSP, with updated land use classifications to mirror the classifications currently allowed (**Attachment 2, Exhibit B2**). In effect, the PCSP running north along Garey Avenue will pause at Fourth Street and resume at Center Street, leaving the DPSP along this stretch of Garey Avenue uninterrupted.

Figure 2. Proposed DPSP Boundary establishes a clear geographic Downtown identity



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Expanding Open Space

The proposed boundary adjustment also necessitated a re-designation of several parcels particularly at Civic Center Plaza and at two City parks within the existing DPSP (Memorial Park and Centennial Park) that are currently zoned mixed-use designations to prevent the creation of any new non-contiguous parcels. To prevent the creation of any new non-contiguous parcels, and to preserve the land development potential of these two City parks, the zoning of these parcels will change to Open Space in the Pomona Zoning Ordinance (Attachment 2, Exhibit C). This change of zone will officially add Memorial and Centennial Parks into the Open Space acreage total of the City, and will shift the identity of Civic Center Plaza from a Downtown destination to a public park that complements both the PCSP and DPSP. This action is consistent with Section .38-1 of the Pomona Municipal Code, which technically identifies Civic Center Plaza as a public park.

GOAL #2: Implementing the General Plan

Updating Transect Density

The Pomona General Plan ("General Plan") was adopted in 2014. In the Land Use chapter, new density thresholds were established citywide, using the theory of transect zoning, which establishes development potential based on geographical context and street patterns. The highest densities are proposed for Downtown, however the DPSP includes density thresholds that are significantly less than what the General Plan has contemplated, creating an inconsistency in development potential (**Figure 3**).

Figure 3. Proposed densities in the General Plan that correlate to DPSP boundaries

	T6-A DOWNTOWN CORE	T6-B TYPICAL	T5 TYPICAL	T4-A TYPICAL
Height	2 floors min. 12 floors max.	2 floors or 24 ft. min. 6 floors max.	6 floors max.	4 floors max.
Housing Density	50 du/ac min. over 100 du/ac max.	20 du/ac min. 100 du/ac max.	80 du/ac max.	70 du/ac max.

In some cases, existing DPSP districts, such as Mixed-Use Central Business District, or Mixed-Use High Density Residential, need only an adjustment in density, as the intent of these districts is still consistent with the General Plan and transect. In other cases, such as Residential Single-Family, or Mixed-Use Light Industrial, the intent and densities of these DPSP districts are irreparably inconsistent with the General Plan and transect, and will need to be eliminated entirely. The proposed Plan streamlines the existing districts into four districts, as illustrated in **Figure 4**.

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Figure 4. Proposed DPSP Districts



In addition to the transect provisions of the General Plan, the Housing Element of the City of Pomona identified a portion of the existing Downtown Pomona Specific Plan as a transit-oriented district, which could accommodate additional growth in housing units (Attachment 2, Exhibit D). To be certain that such growth could be achieved, the Housing Element included an amendment to establish a minimum density of 40 units per acre for this area. When the transit-oriented district was first identified, it did not include the aforementioned single-family and mixed-use light industrial districts. However, the proposed Plan densities, and the elimination of low-density districts, which are consistent with the General Plan and transect, effectively increase the boundary of the transit-oriented district to be the entirety of the DPSP. Therefore, the Plan update expands upon the intent of the Housing Element update by establishing a minimum density of 40 units per acre in each of the four DPSP districts.

Expanding Arts Colony

Downtown Pomona has a long history of supporting artists and the arts. A major impetus for the establishment of the DPSP in 1994 was to formalize this support through land use permissions and design standards. Such permissions primarily focused on the core of Downtown, in the Mixed-Use Central Business District, which includes the historic Second Street Antique Row and Arts Colony. The General Plan built upon this vision by explicitly creating policies and goals to support the arts in Downtown (Figure 5). The Plan update intends to implement this vision through an expansion of the Arts Colony to include the entire DPSP, with by-right permissions for many art-related uses in all four districts, and a streamlined administrative review for more intense art-related uses, such as cultural institutions or heavy-equipment studios. Moreover, as discussed in Goal 3: Achieving Metro's TOD Goals, the Plan update establishes a new incentive for developers to provide affordable artist live/work lofts in exchange for concessions on parking requirements, and

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establishes a new Placemaking chapter, which calls for the implementation of the Art in Public Places Program, and the promotion of public art and the cultural arts across Downtown.

Figure 5. Selected General Plan policies related to arts in Downtown

Policy #	Policy
6E.P3	Permit a range of development types in the
	Downtown annex areas that support the creation of
	incubator space and districts of arts and innovation
6F.P22	Permit a range of development types in the eastern
	Downtown annex area that support the creation of
	incubator space, and districts of arts and innovation

Refining Design Standards

The General Plan added policies and goals to support improved standards for architectural design and site design citywide (**Figure 6**). The DPSP does not contain any mandates for design, instead relying on design guidelines that are optional and encouraged during plan review. To implement these General Plan design goals, the Plan update brings forward new mandatory urban form standards pertaining to building length, building articulation, street frontage, window transparency, and site pedestrian circulation. In addition, the design guidelines have been overhauled with new guidelines that encourage innovative building materials and encourages public art on blank walls. Additional policy provisions have been added that encourage the Planning Division to explore the feasibility of architectural design incentives in the future to protect historic and iconic building frontages in Downtown.

Figure 6. Selected General Plan policies related to design in Downtown

Policy #	Policy
7F.P4	Establish an appropriate relationship between public
	space and private development with building entrances
	oriented towards streets, utilizing shopfronts, porches,
	patios, or outdoor spaces that overlook or interact with
	front yards or sidewalks.
7F.P5	Promote developments that fit with the scale and
	character of their district or neighborhood by:
	-Utilizing varied massing, roof types, and floor plans.
	-Articulating building facades with distinctive
	architectural features such as windows, doors,
	chimneys and other such elements. Use articulation of
	building massing to reveal internal organization of
	building elements such as stairs and elevators, atriums,
	internal gathering spaces and major interior spaces.
7F.P6	Provide visual interest and express the human scale in

	building design with: -Architectural building base treatments -Varied building colors, materials, and site landscaping treatments
	-Pedestrian-scale signage and ornamental lighting
7F.P9	Maintain an open relationship between buildings and street edge, avoiding fencing and significant landscape barriers, except for street trees and sidewalk plantings. Along major collectors and corridors, allow fencing, low walls, and/or landscaping that maintains visibility and visual interaction between residences and the street edge. Limit materials to wood, stone, decorative metal, or low hedges.

Revisiting Land Use Permissions

The General Plan envisions Downtown as a destination that supports a mix of uses, including retail, residential, artist studios, live/work lofts, and educational institutions and offices (**Figure 7**). In addition, proximity to the Downtown Pomona Metrolink Station encourages walkable neighborhoods which require street activation and active storefronts. This intent is implemented in the revised land use permissions in the Plan update. Land uses are classified into one of four permission types: by-right ("P"), by-right to the rear of tenant spaces or on second or higher floors ("P2"), by minor conditional use permit ("M"), and by conditional use permit ("C"). Use permissions were analyzed against their potential to activate Downtown with pedestrian activity and to encourage arts, entertainment, restaurants, and retail. For example, on-sale alcohol uses, such as restaurants serving beer and wine, will now require a minor conditional use permit without a public hearing, while off-sale permits, such as liquor stores, still require a conditional use permit and public hearing. Indoor sports and recreation spaces are permitted by-right, and cultural and arts related institutions and spaces can be processed as a minor conditional use permit. Such changes are intended to streamline the development process and encourage investment in Downtown.

Figure 7. Selected General Plan policies related to land uses in Downtown

Policy #	Policy
+	Make Downtown the centerpiece of the economic
	development strategy. Focus on increasing housing and
	employment opportunities to generate activity increases
	throughout the Downtown area.
7B.P25	Develop a strong concentration of mixed use activities
	Downtown as a major economic draw for residents and
	visitors.

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GOAL #3: Achieving Metro's Transit-Oriented Development Goals

The Plan update is funded entirely through grant funding by Metro Los Angeles. The intent of the grant award is to encourage transit-oriented developments across Los Angeles County. To facilitate grantees with this process, Metro Los Angeles established a *Transit Supportive Toolkit*, which identifies various ways in which local jurisdictions can enact Specific Plan tools to build transit-oriented developments. The following are examples of such policies.

Placemaking

Borne out of both conversations with Pomona stakeholders and analysis of Metro Los Angeles transit policies, the new Placemaking chapter is intended to offer the community an open door into exploring ways to activate public spaces in Downtown. This includes policies for historic preservation and art in public places; pathways to activate public property and public right-of-way with features such as parklets, temporary pedestrian plazas, street furniture, active alleys, and popup music; tree palettes customized to Downtown; wayfinding considerations; improving intersections and crossings; and standards for privately owned public open space. The chapter encourages the City to pursue additional documents to guide placemaking in Downtown, including a Complete Streets manual and adopting a placemaking permit fee and application to facilitate creative uses of the public realm.

More Permissive Parking Standards

The DPSP defers to the Pomona Zoning Ordinance for parking standards. These standards are intended for zoning districts of far less intensity and density, and do not consider adjacency to the Downtown Pomona Metrolink Station. Moreover, Downtown is served by the Vehicle Parking District, which oversees City-owned parking lots. New parking provisions have been added to the Plan update that are in line with transit-oriented development, and provide a more permissive structure that does not overly rely on parking for land uses, and instead considers the ability for multiple modes of transportation to serve these uses, such as light rail, passenger bus, ride share, bicycling, and walking. Moreover, a new provision has been added to grandfather parking requirements for existing buildings up to two stories in the Mixed-Use Central Business District and Mixed-Use Institutional, and waive such standards up to two stories for new development in these districts. The equivalent parking demand generated by such a waiver can be accommodated by the multiple existing City-owned lots managed by the Vehicle Parking District. Parking relief can also be achieved through an applicant-provided parking demand study, certified by a licensed traffic engineer, subject to review and approval of the Development Services Director.

Incentives for Affordable Artist Housing and Public Open Space

As mentioned in **Goal 2: Implementing the General Plan**, the Plan update envisions the entire DPSP boundary to function as the Arts Colony, and to provide for land uses and policies that encourage the cultural arts and artist spaces. In addition, a new incentive has been added to encourage new developers to provide affordable artist housing in Downtown. A developer that

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provides at least 25% of their total proposed units as live/work units, meets Pomona Municipal Code live/work requirements, and is rented at fair market value, may receive a 50% reduction in total parking requirements, and 25% reduction in common open space requirements. The incentive is intended to increase the supply of affordable work/live units across all four districts in an effort to address affordability and viability of artist uses in Downtown.

A second incentive has also been added to address public open space in Downtown. All new development is subject to common open space requirements, with 100 square feet of common open space required for every new residential unit proposed. An applicant that provides such open space as "privately owned public open space," meeting the standards outlined in the Placemaking chapter, may receive a 75% reduction in total common open space requirements. For example, a proposed 100-unit development requires 10,000 square feet of common open space. Utilizing this incentive would bring this requirement down to 2,500 square feet, but would be publicly accessible and function as a community asset that is privately maintained. This incentive is intended to increase the availability of publicly accessible open space such as pocket parks and public plazas in Downtown.

GOAL #4: Streamlining the DPSP as a document and development process

A key component of the Plan update is to ensure that the document is easy to use, accessible, and clear for all stakeholders, including the community, developers, business owners, and City staff. To streamline the provisions of the DPSP, the document has been divided into four chapters: *Introduction, Private Realm, Public Realm, and Implementation*. All requirements that affect private property owners in Downtown are found in the Private Realm chapter, including applicability for new development or significant additions ("Urban Form") and land use permissions ("Land Use"). All requirements that concern public property or the public right-of-way are contained in the Public Realm chapter, including mobility, placemaking, and infrastructure and utilities.

Another key component of the Plan update is to streamline the development process and establish application processes that are tailored to the type of project proposed. To this end, a new "Development Plan Review" process has been established for all parcels within the DPSP. A Development Plan Review is an application that requires a public hearing at the Development Services Director level, and is currently used in the PCSP. Such a review would only apply to any new construction, significant additions, or major façade changes. All proposed projects that solely concern land use, such as the siting of a new land use or expansion of land uses, would not go through a Development Plan Review, and instead remain governed by either a minor conditional use permit or conditional use permit that requires a public hearing at the Planning Commission.

COMMUNITY ENGAGEMENT

Pre-Draft Engagement

RRM Design initiated community outreach prior to providing the first draft of the Plan update. This included key stakeholders meetings conducted in January 2017 with Downtown business

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owners, educational institutions such as Western University, and land owners. In March 2017, a community workshop was conducted to solicit input on opportunities and constraints that could be addressed in the new plan. Key themes that emerged from this engagement include:

- Economic revitalization and business friendly approaches
- Access, mobility and connectivity
- Cleaner, safer and more welcoming environment
- Address undesirable uses and homelessness
- Community character and beautification
- Mix of commercial, residential and institutional uses
- Importance of arts and education
- Family-orientation and need to accommodate all ages
- Protect historic resources
- Streamlined processes and clear policies

Draft Review and Revisions

In February and March 2019, the Planning Division conducted a series of presentations with the Planning Commission, Historic Preservation Commission, and Cultural Arts Commission to provide an update on the Plan and next steps. Staff also met with Building and Safety, Public Works, and other city departments to ensure the viability and accuracy of Plan update policies and standards. Staff also presented the same update to the Downtown Pomona Owner's Association.

The Planning Division circulated a public draft of the Plan update in May 2019. The draft was made available on the City of Pomona website, promoted on social media, and available at the Planning counter for review. As part of this public review period, Staff conducted an open house in Council Chambers to engage the community on the document, and subsequently discussed the draft with the Planning Commission and Historic Preservation Commission at its regularly scheduled meetings, and again with the Downtown Pomona Owner's Association.

The final draft recommended for approval incorporates the comments and feedback received during this period.

ENVIRONMENTAL REVIEW

According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified", except as might be necessary to determine whether there are project-specific significant effects. A Finding of Consistency was prepared to conduct this analysis (Attachment 4).

This Finding of Consistency evaluates the updated DPSP's consistency with the Final Environmental Impact Report for the City of Pomona General Plan Update. The 2014 Final EIR,

Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-206, SPA 12106-2019, ZONE 12104-2019 Page 11 of 11

which was certified in March 2014, establishes the City's vision for development of the City through the year 2035. The evaluation determined whether the Plan update would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. The Finding of Consistency has been prepared pursuant to the applicable provisions of §15183 of the California Environmental Quality Act (CEQA) Guidelines.

The Findings of Consistency concluded that development facilitated by the Plan update would be consistent with the development intensity established by the City's General Plan. Environmental impacts associated with the Plan update would be similar to those anticipated for Downtown in the 2014 General Plan Final EIR and the Plan update would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required for the Plan update under CEQA.

NOTICING

Public noticing was completed as required by Government Code 65091. A hearing for public notice was published on Friday, June 14, 2019 in the Inland Valley Daily Bulletin (**Attachment 5**).

CONCLUSION

The comprehensive update to the Downtown Pomona Specific Plan will reconcile boundaries with the Pomona Corridors Specific Plan, implement the goals and policies of the 2014 Pomona General Plan, achieve the transit-oriented development goals set forth by Metro Los Angeles, and streamline the implementation of the Plan as a document and development process.

Respectfully Submitted: Prepared By:

Anita D. Gutierrez, AICP Ata Khan Development Services Director Senior Planner

ATTACHMENTS

- 1. Draft Resolution
- 2. Exhibits A-D
- 3. Draft Downtown Pomona Specific Plan Update
- 4. Environmental Findings of Consistency
- 5. Proof of Publication of Public Hearing Notice

PC RESOLUTION NO. 19-031

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF POMONA, CALIFORNIA RECOMMENDING CITY COUNCIL APPROVAL OF A COMPREHENSIVE UPDATE TO THE DOWNTOWN POMONA SPECIFIC PLAN (SPA 6409-2016) AND RELATED UPDATES TO THE POMONA CORRIDORS SPECIFIC PLAN (SPA 12106-2019) AND POMONA ZONING ORDINANCE (ZONE 12104-2019).

WHEREAS, the City of Pomona was awarded a grant in 2016 in the amount of \$220,000 from the Los Angeles County Metropolitan Transportation Authority ("Metro Los Angeles") to create a regulatory environment supportive of transit-oriented development around station areas and adjacent transit corridors in Downtown Pomona;

WHEREAS, the City of Pomona subsequently initiated an application for a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) to fulfill the obligations of the Metro Los Angeles Grant;

WHEREAS, the City of Pomona selected RRM Design ("Consultant") to complete the comprehensive update to the Downtown Pomona Specific Plan;

WHEREAS, the Consultant conducted outreach between 2017 and 2019 with the City of Pomona to understand opportunities and constraints in Downtown Pomona;

WHEREAS, the City of Pomona subsequently initiated an application for related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) to reconcile boundaries with the Downtown Pomona Specific Plan and clarify land use permissions across various zoning documents;

WHEREAS, the proposed boundaries of the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) are generally bounded by Center Street to the North, Towne Avenue to the East, Fourth Street to the South, and White Avenue to the West, as defined in Exhibit A;

WHEREAS, the changes in boundaries and land use classifications to the Pomona Corridors Specific Plan (SPA 12106-2019) related to the comprehensive update of the Downtown Pomona Specific Plan are as defined in Exhibit B1 and B2;

WHERAS, the proposed change of zone from the Downtown Pomona Specific Plan to the Open Space Zoning District of the Pomona Zoning Ordinance (ZONE 12104-2019) related to the comprehensive update of the Downtown Pomona Specific Plan is as defined in Exhibit C;

WHEREAS, the Planning Commission of the City of Pomona has, after giving notice thereof as required by law, held a public hearing on June 26, 2019, concerning a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors

PC Resolution No. XX-XX Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-2016, SPA 12106-2019, ZONE 12104-2019 Page 2 of 6

Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019); and

WHEREAS, the Planning Commission has carefully considered all pertinent testimony and the staff report offered in the case as presented at the public hearing.

NOW, THEREFORE, BE IT HEREBY RESOLVED by the Planning Commission of the City of Pomona, California:

SECTION 1. The Planning Commission, exercising independent judgment, finds that the project will not have an adverse impact on the environment. According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified," except as might be necessary to determine whether there are project-specific significant effects. A Findings of Consistency was prepared to evaluate the proposed project's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether it would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Environmental impacts associated with the proposed project would be similar to those anticipated for the project area in the 2014 General Plan Final EIR and would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required under CEQA.

SECTION 2. Based on consideration of the whole record before it, including but not limited to, the staff report, public testimony received at the public hearing on this matter, and evidence made part of the public record, the Planning Commission hereby finds that the proposed comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) is in the public interest and in the interest of the furtherance of the public health, safety, and welfare and is consistent with the goals, objectives, policies, and programs of the Pomona General Plan.

SECTION 3. Based on consideration of the whole record before it, including but not limited to, the staff report, public testimony received at the public hearing on this matter, and evidence made part of the public record, The Planning Commission hereby finds and determines that the comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019) meet the findings outlined in Section .580(J) of the Pomona Zoning Ordinance:

1. The proposed Specific Plan Amendment systematically implements and is consistent with the General Plan.

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The comprehensive update to the Downtown Pomona Specific Plan, along with related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance are in conformance with the policies and goals outlined in the 2014 Pomona General Plan. In particular:

- 6E.P3 Permit a range of development types in the Downtown annex areas that support the creation of incubator space and districts of arts and innovation
- 6F.P22Permit a range of development types in the eastern Downtown annex area that support the creation of incubator space, and districts of arts and innovation

The comprehensive update to the Downtown Pomona Specific Plan expands the notion of the Arts Colony to the entirety of Downtown, and brings in more permissive land use classifications that encourage arts and innovation.

- 7F.P4 Establish an appropriate relationship between public space and private development with building entrances oriented towards streets, utilizing shopfronts, porches, patios, or outdoor spaces that overlook or interact with front yards or sidewalks.
- 7F.P5 Promote developments that fit with the scale and character of their district or neighborhood by:
 - *Utilizing varied massing, roof types, and floor plans.*
 - Articulating building facades with distinctive architectural features such as windows, doors, chimneys and other such elements. Use articulation of building massing to reveal internal organization of building elements such as stairs and elevators, atriums, internal gathering spaces and major interior spaces.
- 7F.P6 Provide visual interest and express the human scale in building design with: Architectural building base treatments

 Varied building colors, materials, and site landscaping treatments

 Pedestrian-scale signage and ornamental lighting
- 7F.P9 Maintain an open relationship between buildings and street edge, avoiding fencing and significant landscape barriers, except for street trees and sidewalk plantings. Along major collectors and corridors, allow fencing, low walls, and/or landscaping that maintains visibility and visual interaction between residences and the street edge. Limit materials to wood, stone, decorative metal, or low hedges.

The comprehensive update to the Downtown Pomona Specific Plan establishes new

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standards for urban form across Downtown, including building length, window transparency, interior pedestrian circulation, and building articulation.

7B.P3 Make Downtown the centerpiece of the economic development strategy. Focus on increasing housing and employment opportunities to generate activity increases throughout the Downtown area.

7B.P25Develop a strong concentration of mixed use activities Downtown as a major economic draw for residents and visitors.

The comprehensive update to the Downtown Pomona Specific Plan and related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance bring forward new land use classifications that are intended to generate economic development and pedestrian activity and street engagement in Downtown. Moreover, all four proposed Downtown districts are intended to support a concentrated mixed use of activities and represent the central hub of activity in the City of Pomona.

2. The proposed Specific Plan Amendment will not be detrimental to the public interest, health, safety, convenience, or welfare of the City.

The comprehensive update to the Downtown Pomona Specific Plan will not be detrimental to the public interest, health, safety, convenience, or welfare of the City, as it is implementing goals and policies previously identified in the 2014 Pomona General Plan, is intended to provide a mix of residential and commercial opportunities in the Downtown area that is subject to land use and urban form standards and guidelines, has been zoned to concentrate densities specifically in the Downtown and to be mindful of density transitions to low density neighborhoods immediately adjacent, and will generate activity and opportunity that is in the interest of all residents in the City of Pomona.

3. The subject property is physically suitable for the requested land use designations and the anticipated land use developments.

The comprehensive update to the Downtown Pomona Specific Plan and related updates to the Pomona Corridors Specific Plan and Pomona Zoning Ordinance has been reviewed by the Public Works Department for infrastructure, utilities, and mobility capacity. It has been determined that the proposed plans are suitable for the type of density and land use designations anticipated in Downtown Pomona and in the Pomona corridors.

4. The Specific Plan Amendment provides for the development of a comprehensively planned project that is superior to development otherwise allowed under conventional zoning

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classifications.

The comprehensive update to the Downtown Pomona Specific Plan is bringing forward a set of tools related to density, land use classifications, urban form, privately owned public open space, work/live units, placemaking, temporary use permits, parking standards, and development review processes that could not be achieved under conventional zoning classifications under the Pomona Zoning Ordinance.

5. The proposed Specific Plan Amendment will contribute to a balance of land uses so that local residents may work and shop in the community in which they live.

The comprehensive update to the Downtown Pomona Specific Plan will provide a healthy mix of land uses that include multi-unit residential, commercial retail and professional office space, cultural uses, public assembly, institutional uses, and other entertainment and recreation that will encourage Pomona residents to thrive in the community in which they live.

SECTION 4. Based upon the above findings, the Planning Commission recommends the City Council of the City of Pomona take the following actions:

- 1. Adopt a comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016), with boundaries and parcels as defined in Exhibit A;
- 2. Adopt an amendment to the Pomona Corridors Specific Plan (SPA 12106-2019), with parcels to be added or removed as defined in Exhibit B1, and with updated land use categories as defined in Exhibit B2; and
- 3. Adopt a change of zone (ZONE 12104-2019) for parcels to be changed from Downtown Pomona Specific Plan designations to the Open Space Zoning District of the Pomona Zoning Ordinance as defined in Exhibit C.

SECTION 5. The Secretary shall certify to the adoption of this Resolution and forward the original to the City Clerk.

APPROVED AND ADOPTED THIS 26TH DAY OF JUNE, 2019.

DR. KYLE BROWN PLANNING COMMISSION CHAIRPERSON PC Resolution No. XX-XX Comprehensive Update to the Downtown Pomona Specific Plan SPA 6409-2016, SPA 12106-2019, ZONE 12104-2019 Page 6 of 6

ATTEST:	A	TTEST:
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ANITA D. GUTIERREZ, AICP
PLANNING COMMISSION SECRETARY

APPROVED AS TO FORM:

MARCO A. MARTINEZ

INTERIM ASSISTANT CITY ATTORNEY

STATE OF CALIFORNIA) COUNTY OF LOS ANGELES) ss. CITY OF POMONA)

AYES: Brown, Grajeda, Camacho-Gonzalez, Urey, Kercheval, Vander Molen

NOES: ABSTAIN:

ABSENT: Bunce

Pursuant to Resolution No. 76-258 of the City of Pomona, the time in which judicial review of this action must be sought is governed by California Code of Civil Procedure Section 1094.6.

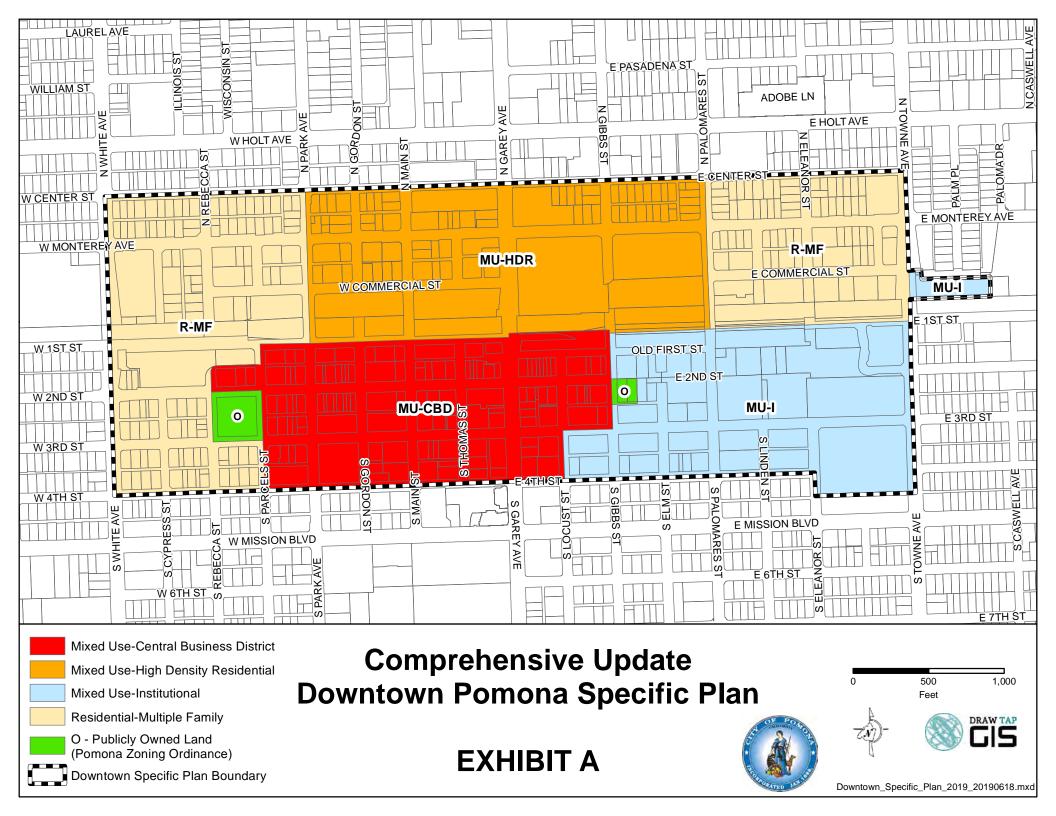


Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8335010013	CSP DownTown Core	DT-SP MU-CBD
8335010906	CSP DownTown Core	DT-SP MU-CBD
8335009005	CSP DownTown Core	DT-SP MU-CBD
8335009009	CSP DownTown Core	DT-SP MU-CBD
8335010014	CSP DownTown Core	DT-SP MU-CBD
8335009904	CSP DownTown Core	DT-SP MU-CBD
8335010012	CSP DownTown Core	DT-SP MU-CBD
8335010907	CSP DownTown Core	DT-SP MU-CBD
8335010905	CSP DownTown Core	DT-SP MU-CBD
8335009017	CSP DownTown Core	DT-SP MU-CBD
8335011007	CSP DownTown Core	DT-SP MU-CBD
8335009002	CSP DownTown Core	DT-SP MU-CBD
8335010010	CSP DownTown Core	DT-SP MU-CBD
8335010004	CSP DownTown Core	DT-SP MU-CBD
8335009011	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
8335009008	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
8335011014	CSP DownTown Core	DT-SP MU-CBD
8335009014	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
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Exhibit A: Parcels in Downtown Pomona Specific Plan Update

8341005023 CSP - DownTown Core DT-SP - MU-CBD 8341004018 CSP - DownTown Core DT-SP - MU-CBD 8341004054 CSP - DownTown Core DT-SP - MU-CBD 8336026900 CSP - Height Overlay - DT-SP - MU-HDR 8336025012 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336032004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336032002 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025003 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336027024 DT-SP - Height Overlay - DownTown Gateway Segment DT-SP - MU-HDR 8336027028 DT-SP - MU-AR DT-SP - MU-HDR 8336027013 DT-SP - MU-AR DT-SP - MU-HDR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR 8336027006 DT-SP - MU-AR DT-SP - MU-HDR 8336027006 DT-SP - MU-AR DT-SP - MU-HDR
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8336032004 CSP - Height Overlay - DownTown Gateway Segment 8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336027017 CSP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR
8336032002 CSP - Height Overlay - DownTown Gateway Segment 8336025003 CSP - Height Overlay - DownTown Gateway Segment B336032001 CSP - Height Overlay - DownTown Gateway Segment B336025004 CSP - Height Overlay - DownTown Gateway Segment B336025004 CSP - Height Overlay - DownTown Gateway Segment B336026017 CSP - Height Overlay - DownTown Gateway Segment B336027024 DT-SP - MU-AR B336027024 DT-SP - MU-AR B336027028 DT-SP - MU-AR B336027013 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B336027005 DT-SP - MU-AR B7-SP - MU-HDR B7-SP - MU-HDR B7-SP - MU-HDR B7-SP - MU-HDR
8336025003 CSP - Height Overlay - DownTown Gateway Segment 8336032001 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336025004 CSP - Height Overlay - DownTown Gateway Segment 8336026017 CSP - Height Overlay - DownTown Gateway Segment 8336027024 DT-SP - MU-AR 8336027028 DT-SP - MU-AR 8336027013 DT-SP - MU-AR 8336027005 DT-SP - MU-AR DT-SP - MU-HDR 8736027005 DT-SP - MU-AR DT-SP - MU-HDR
8336032001 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336025004 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336026017 CSP - Height Overlay - DownTown Gateway Segment DT-SP MU-HDR 8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027024 DT-SP MU-AR DT-SP MU-HDR 8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027028 DT-SP MU-AR DT-SP MU-HDR 8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027013 DT-SP MU-AR DT-SP MU-HDR 8336027005 DT-SP MU-AR DT-SP MU-HDR
8336027005 DT-SP MU-AR DT-SP MU-HDR
\$336027006 DT_SD MII_AR
8336027021 DT-SP MU-AR DT-SP MU-HDR
8336027027 DT-SP MU-AR DT-SP MU-HDR
8336027008 DT-SP MU-AR DT-SP MU-HDR
8336027029 DT-SP MU-AR DT-SP MU-HDR
8336027012 DT-SP MU-AR DT-SP MU-HDR
8336027019 DT-SP MU-AR DT-SP MU-HDR
8336027014 DT-SP MU-AR DT-SP MU-HDR
8336027002 DT-SP MU-AR DT-SP MU-HDR
8336027018 DT-SP MU-AR 8336027026 DT-SP MU-AR DT-SP MU-HDR DT-SP MU-HDR
8336027010 DT-SP MU-AR DT-SP MU-HDR
8336027010 DT-SP MU-AR DT-SP MU-HDR
8336027007 DT-SP MU-AR DT-SP MU-HDR
8337027008 DT-SP MU-AR DT-SP R-MF
8337027008 DT-SF MU-AR DT-SP R-MF
8337027023 DT-SP MU-AR DT-SP R-MF
8337027012 DT-SP MU-AR DT-SP R-MF
8337027020 DT-SP MU-AR DT-SP R-MF
8336027003 DT-SP MU-AR DT-SP MU-HDR
8336027011 DT-SP MU-AR DT-SP MU-HDR
8337027007 DT-SP MU-AR DT-SP R-MF
8337027022 DT-SP MU-AR DT-SP R-MF
8337027017 DT-SP MU-AR DT-SP R-MF
8337027002 DT-SP MU-AR DT-SP R-MF
8336027020 DT-SP MU-AR DT-SP MU-HDR
8336027004 DT-SP MU-AR DT-SP MU-HDR
8336027001 DT-SP MU-AR DT-SP MU-HDR
8337027001 DT-SP MU-AR DT-SP R-MF
8337027003 DT-SP MU-AR DT-SP R-MF
8337027010 DT-SP MU-AR DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
	9 DT-SP MU-AR	DT-SP R-MF
833702700	4 DT-SP MU-AR	DT-SP R-MF
833702701	3 DT-SP MU-AR	DT-SP R-MF
833702701	6 DT-SP MU-AR	DT-SP R-MF
833702701	1 DT-SP MU-AR	DT-SP R-MF
833702700	6 DT-SP MU-AR	DT-SP R-MF
	9 DT-SP MU-AR	DT-SP R-MF
	5 DT-SP MU-AR	DT-SP R-MF
833702701	8 DT-SP MU-AR	DT-SP R-MF
	1 DT-SP MU-AR	DT-SP R-MF
	5 DT-SP MU-AR	DT-SP R-MF
	6 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	3 DT-SP MU-CBD	DT-SP MU-CBD
	0 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
	2 DT-SP MU-CBD	DT-SP MU-CBD
	6 DT-SP MU-CBD	DT-SP MU-CBD
	9 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	9 DT-SP MU-CBD	DT-SP MU-CBD
	0 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
	9 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
	7 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
834100280	0 DT-SP MU-CBD	DT-SP MU-CBD
	3 DT-SP MU-CBD	DT-SP MU-CBD
	2 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	7 DT-SP MU-CBD	DT-SP MU-CBD
	1 DT-SP MU-CBD	DT-SP MU-CBD
	2 DT-SP MU-CBD	DT-SP MU-CBD
	9 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	4 DT-SP MU-CBD	DT-SP MU-CBD
	6 DT-SP MU-CBD	DT-SP MU-CBD
	1 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	5 DT-SP MU-CBD	DT-SP MU-CBD
	8 DT-SP MU-CBD	DT-SP MU-CBD
	1 DT-SP MU-CBD	DT-SP MU-CBD
	7 DT-SP MU-CBD	DT-SP MU-CBD
00.100100	, 5. 5	5 1 51 1110 055

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8341005015 DT-SP MU-CBD	DT-SP MU-CBD
8341006025 DT-SP MU-CBD	DT-SP MU-CBD
8341003010 DT-SP MU-CBD	DT-SP MU-CBD
8341002021 DT-SP MU-CBD	DT-SP MU-CBD
8341006005 DT-SP MU-CBD	DT-SP MU-CBD
8341001036 DT-SP MU-CBD	DT-SP MU-CBD
8341006022 DT-SP MU-CBD	DT-SP MU-CBD
8341001901 DT-SP MU-CBD	DT-SP MU-CBD
8341001902 DT-SP MU-CBD	DT-SP MU-CBD
8341002006 DT-SP MU-CBD	DT-SP MU-CBD
8341003006 DT-SP MU-CBD	DT-SP MU-CBD
8341002024 DT-SP MU-CBD	DT-SP MU-CBD
8341002007 DT-SP MU-CBD	DT-SP MU-CBD
8341003001 DT-SP MU-CBD	DT-SP MU-CBD
8341003008 DT-SP MU-CBD	DT-SP MU-CBD
8341006003 DT-SP MU-CBD	DT-SP MU-CBD
8341005003 DT-SP MU-CBD	DT-SP MU-CBD
8341005021 DT-SP MU-CBD	DT-SP MU-CBD
8341005013 DT-SP MU-CBD	DT-SP MU-CBD
8341004902 DT-SP MU-CBD	DT-SP MU-CBD
8341003901 DT-SP MU-CBD	DT-SP MU-CBD
8341005014 DT-SP MU-CBD	DT-SP MU-CBD
8341003003 DT-SP MU-CBD	DT-SP MU-CBD
8341002016 DT-SP MU-CBD	DT-SP MU-CBD
8341001900 DT-SP MU-CBD	DT-SP MU-CBD
8341002013 DT-SP MU-CBD 8341001039 DT-SP MU-CBD	DT-SP MU-CBD DT-SP MU-CBD
8341002022 DT-SP MU-CBD	DT-SP MU-CBD
8341004046 DT-SP MU-CBD	DT-SP MU-CBD
8341003004 DT-SP MU-CBD	DT-SP MU-CBD
8341004044 DT-SP MU-CBD	DT-SP MU-CBD
8341003011 DT-SP MU-CBD	DT-SP MU-CBD
8341005022 DT-SP MU-CBD	DT-SP MU-CBD
8341006024 DT-SP MU-CBD	DT-SP MU-CBD
8341005017 DT-SP MU-CBD	DT-SP MU-CBD
8341003904 DT-SP MU-CBD	DT-SP MU-CBD
8341003902 DT-SP MU-CBD	DT-SP MU-CBD
8341003903 DT-SP MU-CBD	DT-SP MU-CBD
8341003908 DT-SP MU-CBD	DT-SP MU-CBD
8341003909 DT-SP MU-CBD	DT-SP MU-CBD
8341003905 DT-SP MU-CBD	DT-SP MU-CBD
8341003907 DT-SP MU-CBD	DT-SP MU-CBD
8341003906 DT-SP MU-CBD	DT-SP MU-CBD
8341005904 DT-SP MU-CBD	DT-SP MU-CBD
8341005018 DT-SP MU-CBD	DT-SP MU-CBD
8336033012 DT-SP MU-HDR	DT-SP MU-HDR

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
833603480	2 DT-SP MU-HDR	DT-SP MU-HDR
833602401	2 DT-SP MU-HDR	DT-SP MU-HDR
833603201	4 DT-SP MU-HDR	DT-SP MU-HDR
833603201	0 DT-SP MU-HDR	DT-SP MU-HDR
833603301	3 DT-SP MU-HDR	DT-SP MU-HDR
833602400	7 DT-SP MU-HDR	DT-SP MU-HDR
833602401	9 DT-SP MU-HDR	DT-SP MU-HDR
833603301	8 DT-SP MU-HDR	DT-SP MU-HDR
833602400	5 DT-SP MU-HDR	DT-SP MU-HDR
833602402	3 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
833602400	4 DT-SP MU-HDR	DT-SP MU-HDR
833602401	7 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	9 DT-SP MU-HDR	DT-SP - MU-HDR
	9 DT-SP MU-HDR 8 DT-SP MU-HDR	DT-SP MU-HDR DT-SP MU-HDR
	8 DT-SP MU-HDR 1 DT-SP MU-HDR	DT-SP MU-HDR
	1 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	0 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	4 DT-SP MU-HDR	DT-SP MU-HDR
00000	9 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP MU-HDR
	3 DT-SP MU-HDR	DT-SP MU-HDR
	9 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	8 DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	6 DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	7 DT-SP MU-HDR	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8340029004	DT-SP MU-HDR	DT-SP R-MF
8340029001	DT-SP MU-HDR	DT-SP R-MF
8340029002	2 DT-SP MU-HDR	DT-SP R-MF
8340029024	I DT-SP MU-HDR	DT-SP R-MF
8340029023	B DT-SP MU-HDR	DT-SP R-MF
8340029017	DT-SP MU-HDR	DT-SP R-MF
8340036008	B DT-SP MU-HDR	DT-SP R-MF
8340036005	5 DT-SP MU-HDR	DT-SP R-MF
8340029006	5 DT-SP MU-HDR	DT-SP R-MF
8340036801	DT-SP MU-HDR	DT-SP R-MF
8340035003	B DT-SP MU-HDR	DT-SP R-MF
8340036002	PDT-SP MU-HDR	DT-SP R-MF
8340036802	2 DT-SP MU-HDR	DT-SP R-MF
8340036006	5 DT-SP MU-HDR	DT-SP R-MF
8340029008	B DT-SP MU-HDR	DT-SP R-MF
) DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	7 DT-SP MU-HDR	DT-SP R-MF
	I DT-SP MU-HDR	DT-SP R-MF
	2 DT-SP MU-HDR	DT-SP R-MF
	DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR	DT-SP R-MF
	DT-SP MU-HDR	DT-SP R-MF
	B DT-SP MU-HDR	DT-SP R-MF
	5 DT-SP MU-HDR L DT-SP MU-HDR	DT-SP R-MF DT-SP MU-HDR
	B DT-SP MU-HDR	DT-SP MU-HDR
	2 DT-SP MU-HDR	DT-SP MU-HDR
	5 DT-SP MU-HDR	DT-SP MU-HDR
	7 DT-SP MU-HDR	DT-SP MU-HDR
	DT-SP MU-HDR	DT-SP MU-HDR
8336033028	B DT-SP MU-HDR	DT-SP MU-HDR
8336033029	DT-SP MU-HDR	DT-SP MU-HDR
8335013903	B DT-SP MU-I	DT-SP MU-I
8335013907	DT-SP MU-I	DT-SP MU-I
8335013800	DT-SP MU-I	DT-SP MU-I
8335013801	DT-SP MU-I	DT-SP MU-I
8335014918	B DT-SP MU-I	DT-SP MU-I
8335013904	I DT-SP MU-I	DT-SP MU-I
8335011800	DT-SP MU-I	DT-SP MU-I
	DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
	2 DT-SP MU-I	DT-SP MU-I
	5 DT-SP MU-I	DT-SP MU-I
8335013900) DT-SP MU-I	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8335013908 DT-SP MU-I	DT-SP MU-I
8341002803 DT-SP MU-I	DT-SP R-MF
8341002019 DT-SP MU-I	DT-SP MU-CBD
8342006010 DT-SP MU-I	DT-SP R-MF
8341002025 DT-SP MU-I	DT-SP MU-CBD
8342006005 DT-SP MU-I	DT-SP R-MF
8342006021 DT-SP MU-I	DT-SP R-MF
8342006011 DT-SP MU-I	DT-SP R-MF
8342006004 DT-SP MU-I	DT-SP R-MF
8342001025 DT-SP MU-I	DT-SP R-MF
8342006002 DT-SP MU-I	DT-SP R-MF
8342001024 DT-SP MU-I	DT-SP R-MF
8342001023 DT-SP MU-I	DT-SP R-MF
8342001804 DT-SP MU-I	DT-SP R-MF
8342001914 DT-SP MU-I	DT-SP R-MF
8342006001 DT-SP MU-I	DT-SP R-MF
8342001805 DT-SP MU-I	DT-SP R-MF
8342006014 DT-SP MU-I	DT-SP R-MF
8341002020 DT-SP MU-I	DT-SP MU-CBD
8342006003 DT-SP MU-I	DT-SP R-MF
8335007904 DT-SP MU-I	DT-SP MU-I
8335008018 DT-SP MU-I	DT-SP MU-I
8335007906 DT-SP MU-I	DT-SP MU-I
8335008015 DT-SP MU-I	DT-SP MU-I
8335007011 DT-SP MU-I	DT-SP MU-I
8335008017 DT-SP MU-I	DT-SP MU-I
8335007018 DT-SP MU-I 8335008019 DT-SP MU-I	DT-SP MU-I DT-SP MU-I
8335008019 DT-SP MU-I	DT-SP MU-I
8336026903 DT-SP MU-I	DT-SP MU-HDR
8336026905 DT-SP MU-I	DT-SP MU-HDR
8336026011 DT-SP MU-I	DT-SP MU-HDR
8336026901 DT-SP MU-I	DT-SP MU-HDR
8336026902 DT-SP MU-I	DT-SP MU-HDR
8336026904 DT-SP MU-I	DT-SP MU-HDR
8336026015 DT-SP MU-I	DT-SP MU-HDR
8336026016 DT-SP MU-I	DT-SP MU-HDR
8335007001 DT-SP MU-I	DT-SP MU-I
8335007017 DT-SP MU-I	DT-SP MU-I
8335014043 DT-SP MU-I	DT-SP MU-I
8335007014 DT-SP MU-I	DT-SP MU-I
8335004054 DT-SP MU-I	DT-SP MU-I
8335004067 DT-SP MU-I	DT-SP MU-I
8335004066 DT-SP MU-I	DT-SP MU-I
8335005044 DT-SP MU-I	DT-SP MU-I
8335005050 DT-SP MU-I	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN Existing Zoning	Proposed Zoning
8335005049 DT-SP MU-I	DT-SP MU-I
8335005048 DT-SP MU-I	DT-SP MU-I
8335005038 DT-SP MU-I	DT-SP MU-I
8335005046 DT-SP MU-I	DT-SP MU-I
8335005047 DT-SP MU-I	DT-SP MU-I
8335005903 DT-SP MU-I	DT-SP MU-I
8335014044 DT-SP MU-I	DT-SP MU-I
8335014045 DT-SP MU-I	DT-SP MU-I
8335005045 DT-SP MU-I	DT-SP MU-I
8337024009 DT-SP MU-L	DT-SP R-MF
8337024006 DT-SP MU-L	DT-SP R-MF
8337024007 DT-SP MU-L	DT-SP R-MF
8337024016 DT-SP MU-L	DT-SP R-MF
8337029002 DT-SP MU-L	DT-SP R-MF
8337024010 DT-SP MU-L	DT-SP R-MF
8337028012 DT-SP MU-L	DT-SP R-MF
8337028013 DT-SP MU-L	DT-SP R-MF
8337028009 DT-SP MU-L	DT-SP R-MF
8337028007 DT-SP MU-L	DT-SP R-MF
8337023030 DT-SP MU-L	DT-SP R-MF
8337024020 DT-SP MU-L	DT-SP R-MF
8337024003 DT-SP MU-L	DT-SP R-MF
8337024012 DT-SP MU-L	DT-SP R-MF
8337023012 DT-SP MU-L	DT-SP R-MF
8337028010 DT-SP MU-L	DT-SP R-MF
8337028015 DT-SP MU-L	DT-SP R-MF
8337023013 DT-SP MU-L	DT-SP R-MF
8337024011 DT-SP MU-L	DT-SP R-MF
8337028011 DT-SP MU-L	DT-SP R-MF
8337028017 DT-SP MU-L	DT-SP R-MF
8337023019 DT-SP MU-L	DT-SP R-MF
8337029003 DT-SP MU-L	DT-SP R-MF
8337024015 DT-SP MU-L	DT-SP R-MF
8337028003 DT-SP MU-L	DT-SP R-MF
8337023010 DT-SP MU-L	DT-SP R-MF
8337028008 DT-SP MU-L	DT-SP R-MF
8337029005 DT-SP MU-L	DT-SP R-MF
8337029004 DT-SP MU-L	DT-SP R-MF
8337023031 DT-SP MU-L	DT-SP R-MF
8337028006 DT-SP MU-L	DT-SP R-MF
8337024013 DT-SP MU-L	DT-SP R-MF
8337024005 DT-SP MU-L	DT-SP R-MF
8337024014 DT-SP MU-L	DT-SP R-MF
8337028001 DT-SP MU-L	DT-SP R-MF
8337028002 DT-SP MU-L	DT-SP R-MF
8337024008 DT-SP MU-L	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

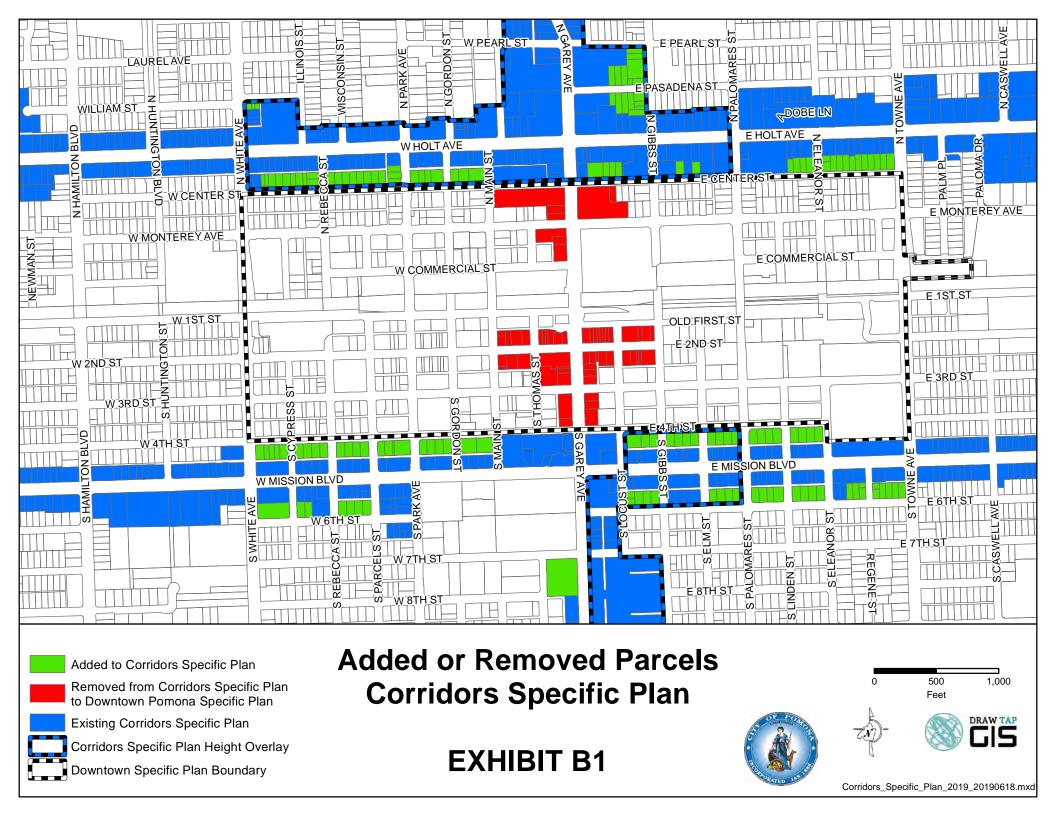
AIN	Existing Zoning	Proposed Zoning
833702301	4 DT-SP MU-L	DT-SP R-MF
833702400	2 DT-SP MU-L	DT-SP R-MF
833702302	8 DT-SP MU-L	DT-SP R-MF
833702800	4 DT-SP MU-L	DT-SP R-MF
833702801	6 DT-SP MU-L	DT-SP R-MF
833702900	1 DT-SP MU-L	DT-SP R-MF
833702400	4 DT-SP MU-L	DT-SP R-MF
833702301	1 DT-SP MU-L	DT-SP R-MF
833702800	5 DT-SP MU-L	DT-SP R-MF
833702303	2 DT-SP MU-L	DT-SP R-MF
833702302	0 DT-SP MU-L	DT-SP R-MF
833702200	5 DT-SP MU-L	DT-SP R-MF
833702200	4 DT-SP MU-L	DT-SP R-MF
833500180	1 DT-SP MU-L	DT-SP R-MF
833500190	3 DT-SP MU-L	DT-SP MU-HDR
833500180	0 DT-SP MU-L	DT-SP MU-HDR
833702301	5 DT-SP MU-L	DT-SP R-MF
833702200	2 DT-SP MU-L	DT-SP R-MF
833702302	9 DT-SP MU-L	DT-SP R-MF
833500180	9 DT-SP MU-L	DT-SP MU-HDR
833500180	2 DT-SP MU-L	DT-SP R-MF
833500190	4 DT-SP MU-L	DT-SP R-MF
833500180	7 DT-SP MU-L	DT-SP R-MF
833702200	3 DT-SP MU-L	DT-SP R-MF
833500180	8 DT-SP MU-L	DT-SP R-MF
	4 DT-SP MU-L	DT-SP R-MF
	7 DT-SP MU-L	DT-SP R-MF
	5 DT-SP MU-L	DT-SP R-MF
	7 DT-SP MU-L	DT-SP R-MF
	0 DT-SP MU-L	DT-SP R-MF
	9 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
00.000.00	0 DT-SP MU-L	DT-SP R-MF
	2 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	5 DT-SP MU-L	DT-SP R-MF
	1 DT-SP MU-L	DT-SP R-MF
	3 DT-SP MU-L	DT-SP R-MF
	6 DT-SP MU-L	DT-SP R-MF
	9 DT-SP MU-L	DT-SP R-MF
	8 DT-SP MU-L	DT-SP R-MF
	5 DT-SP MU-L	DT-SP MU-HDR
	4 DT-SP MU-L	DT-SP MU-HDR
833501505	5 DT-SP R-MF	DT-SP MU-I

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

8335004052 DT-SP - R-RMF DT-SP - R-MF 8341006016 DT-SP - R-SF DT-SP - R-MF 8341006012 DT-SP - R-SF DT-SP - R-MF 8341006019 DT-SP - R-SF DT-SP - R-MF 8341006019 DT-SP - R-SF DT-SP - R-MF 8341006010 DT-SP - R-SF DT-SP - R-MF 8341006011 DT-SP - R-SF DT-SP - R-MF 8342006006 DT-SP - R-SF DT-SP - R-MF 8341006013 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF <	AIN	Existing Zoning	Proposed Zoning
8341006012 DT-SP - R-SF DT-SP - R-MF 8341006018 DT-SP - R-SF DT-SP - R-MF 8341006017 DT-SP - R-SF DT-SP - R-MF 8341006017 DT-SP - R-SF DT-SP - R-MF 8341006014 DT-SP - R-SF DT-SP - R-MF 8342006006 DT-SP - R-SF DT-SP - R-MF 8341006013 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006000 DT-SP - R-SF DT-SP - R-MF 8342006000 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006000 DT-SP - R-SF DT-SP - R-MF 8342006000 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 834200700 DT-SP - R-SF DT-SP - R-MF 834200700 DT-SP - R-SF DT-SP - R-MF 834200700 DT-SP - R-SF D	833500405	2 DT-SP R-MF	DT-SP MU-I
8341006018 DT-SP - R-SF 8341006017 DT-SP - R-SF 8341006017 DT-SP - R-SF 8341006017 DT-SP - R-SF 8341006014 DT-SP - R-SF 8342006006 DT-SP - R-SF 8342006006 DT-SP - R-SF 8342006005 DT-SP - R-SF 8342006013 DT-SP - R-SF 8342006015 DT-SP - R-SF 8342006017 DT-SP - R-SF 8342007014 DT-SP - R-SF 8342007010 DT-SP - R-SF 834200	834100601	6 DT-SP R-SF	DT-SP R-MF
8341006019 DT-SP - R-SF 8341006017 DT-SP - R-SF 8341006014 DT-SP - R-SF 8341006014 DT-SP - R-SF 8341006016 DT-SP - R-SF 8342006006 DT-SP - R-SF 8342006006 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006007 DT-SP - R-SF 8342007014 DT-SP - R-SF 8342007015 DT-SP - R-SF 8342007015 DT-SP - R-SF 8342007010 DT-SP - R-	834100601	2 DT-SP R-SF	DT-SP R-MF
8341006017 DT-SP R-SF DT-SP R-MF 8341006014 DT-SP R-SF DT-SP R-MF 8342006006 DT-SP - R-SF DT-SP - R-MF 8341006013 DT-SP - R-SF DT-SP - R-MF 8341006015 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342007014 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007001 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF	834100601	8 DT-SP R-SF	DT-SP R-MF
8341006014 DT-SP - R-SF 8342006006 DT-SP - R-SF 8341006013 DT-SP - R-SF 8341006013 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006007 DT-SP - R-SF 8342006007 DT-SP - R-SF 8342006007 DT-SP - R-SF 8342007010 DT-SP - R-SF 8342006019 DT-SP - R-SF 8342006015 DT-SP - R-SF 8342006015 DT-SP - R-SF 834200704 DT-SP - R-SF 834200704 DT-SP - R-SF 834200700 DT-SP - R-SF 834200700 DT-SP - R-SF 834200700 DT-SP - R-SF 8342007010 DT-SP - R-SF 834200700 DT-SP - R-SF 8342007010 DT-SP - R-SF 83420070	834100601	9 DT-SP R-SF	DT-SP R-MF
8342006006 DT-SP - R-SF DT-SP - R-MF 8341006013 DT-SP - R-SF DT-SP - R-MF 8341006015 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342006007 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007001 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007001 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 83420007010 DT-SP - R-SF <	834100601	7 DT-SP R-SF	DT-SP R-MF
8341006013 DT-SP - R-SF 8341006015 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342006008 DT-SP - R-SF 8342007014 DT-SP - R-SF 8342007015 DT-SP - R-SF 8342007010 DT-SP - R-SF 8342006015 DT-SP - R-SF 8342006015 DT-SP - R-SF 8342007002 DT-SP - R-SF 8342007002 DT-SP - R-SF 8342007002 DT-SP - R-SF 8342007002 DT-SP - R-SF 8342007003 DT-SP - R-SF 8342007009 DT-SP - R-SF 8342007009 DT-SP - R-SF 8342007010 DT-SP - R-	834100601	4 DT-SP R-SF	DT-SP R-MF
8341006015 DT-SP - R-SF DT-SP - R-MF 8342006008 DT-SP - R-SF DT-SP - R-MF 8342007014 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 8342006010 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 83420007012 DT-SP - R-SF <	834200600	6 DT-SP R-SF	DT-SP R-MF
8342006008 DT-SP - R-SF DT-SP - R-MF 8342007014 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF <t< td=""><td>834100601</td><td>3 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834100601	3 DT-SP R-SF	DT-SP R-MF
8342007014 DT-SP - R-SF DT-SP - R-MF 8342006007 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF <t< td=""><td>834100601</td><td>5 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834100601	5 DT-SP R-SF	DT-SP R-MF
8342006007 DT-SP - R-SF DT-SP - R-MF 8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF <t< td=""><td>834200600</td><td>8 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200600	8 DT-SP R-SF	DT-SP R-MF
8342007015 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF <t< td=""><td>834200701</td><td>4 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200701	4 DT-SP R-SF	DT-SP R-MF
8342007001 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007016 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8341006020 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF <t< td=""><td>834200600</td><td>7 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200600	7 DT-SP R-SF	DT-SP R-MF
8342007010 DT-SP - R-SF DT-SP - R-MF 8342007016 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8341006020 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF <t< td=""><td>834200701</td><td>5 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200701	5 DT-SP R-SF	DT-SP R-MF
8342007016 DT-SP - R-SF DT-SP - R-MF 8342006009 DT-SP - R-SF DT-SP - R-MF 8341006020 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF <t< td=""><td>834200700</td><td>1 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200700	1 DT-SP R-SF	DT-SP R-MF
8342006009 DT-SP - R-SF DT-SP - R-MF 8341006020 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF <t< td=""><td>834200701</td><td>0 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200701	0 DT-SP R-SF	DT-SP R-MF
8341006020 DT-SP - R-SF DT-SP - R-MF 8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF <t< td=""><td>834200701</td><td>6 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200701	6 DT-SP R-SF	DT-SP R-MF
8342006015 DT-SP - R-SF DT-SP - R-MF 8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF <t< td=""><td>834200600</td><td>9 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834200600	9 DT-SP R-SF	DT-SP R-MF
8342006017 DT-SP - R-SF DT-SP - R-MF 8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF <t< td=""><td>834100602</td><td>0 DT-SP R-SF</td><td>DT-SP R-MF</td></t<>	834100602	0 DT-SP R-SF	DT-SP R-MF
8342007004 DT-SP - R-SF DT-SP - R-MF 8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 834003003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200601	5 DT-SP R-SF	DT-SP R-MF
8342007002 DT-SP - R-SF DT-SP - R-MF 8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342007010 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 83400300015 DT-SP - R-SF DT-SP - R-MF 83400300004 DT-SP - R-SF	834200601	7 DT-SP R-SF	DT-SP R-MF
8342007017 DT-SP - R-SF DT-SP - R-MF 8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 834003003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	4 DT-SP R-SF	DT-SP R-MF
8342007003 DT-SP - R-SF DT-SP - R-MF 8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	2 DT-SP R-SF	DT-SP R-MF
8342007009 DT-SP - R-SF DT-SP - R-MF 8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030005 DT-SP - R-SF DT-SP - R-MF 8340030005 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200701	7 DT-SP R-SF	DT-SP R-MF
8342006019 DT-SP - R-SF DT-SP - R-MF 8342007013 DT-SP - R-SF DT-SP - R-MF 8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007000 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029013 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	3 DT-SP R-SF	DT-SP R-MF
8342007013 DT-SP - R-SF DT-SP - R-MF 8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030005 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF DT-SP - R-MF 8340030001 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	9 DT-SP R-SF	DT-SP R-MF
8342006018 DT-SP - R-SF DT-SP - R-MF 8342007012 DT-SP - R-SF DT-SP - R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342006016 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 834003003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200601	9 DT-SP R-SF	DT-SP R-MF
8342007012 DT-SP R-SF DT-SP R-MF 8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342006016 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029013 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200701	3 DT-SP R-SF	DT-SP R-MF
8342007019 DT-SP - R-SF DT-SP - R-MF 8342007007 DT-SP - R-SF DT-SP - R-MF 8342006016 DT-SP - R-SF DT-SP - R-MF 8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342006020 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030005 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200601	8 DT-SP R-SF	DT-SP R-MF
8342007007 DT-SP R-SF DT-SP R-MF 8342006016 DT-SP R-SF DT-SP R-MF 8342007005 DT-SP R-SF DT-SP R-MF 8342007018 DT-SP R-SF DT-SP R-MF 8342006020 DT-SP R-SF DT-SP R-MF 8342007006 DT-SP R-SF DT-SP R-MF 8342007008 DT-SP - R-SF DT-SP R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200701	2 DT-SP R-SF	DT-SP R-MF
8342006016 DT-SP R-SF DT-SP R-MF 8342007005 DT-SP R-SF DT-SP R-MF 8342007018 DT-SP R-SF DT-SP R-MF 8342006020 DT-SP R-SF DT-SP R-MF 8342007006 DT-SP R-SF DT-SP R-MF 8342007008 DT-SP R-SF DT-SP R-MF 8342007020 DT-SP R-SF DT-SP R-MF 8340029010 DT-SP R-SF DT-SP R-MF 8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834200701	9 DT-SP R-SF	DT-SP R-MF
8342007005 DT-SP - R-SF DT-SP - R-MF 8342007018 DT-SP - R-SF DT-SP - R-MF 8342006020 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 834003003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	7 DT-SP R-SF	DT-SP R-MF
8342007018 DT-SP R-SF DT-SP R-MF 8342006020 DT-SP R-SF DT-SP R-MF 8342007006 DT-SP - R-SF DT-SP R-MF 8342007008 DT-SP - R-SF DT-SP R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029013 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200601	6 DT-SP R-SF	DT-SP R-MF
8342006020 DT-SP - R-SF DT-SP - R-MF 8342007006 DT-SP - R-SF DT-SP - R-MF 8342007008 DT-SP - R-SF DT-SP - R-MF 8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029013 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	5 DT-SP R-SF	DT-SP R-MF
8342007006 DT-SP R-SF DT-SP R-MF 8342007008 DT-SP R-SF DT-SP R-MF 8342007020 DT-SP R-SF DT-SP R-MF 8340029010 DT-SP R-SF DT-SP R-MF 8340029013 DT-SP R-SF DT-SP R-MF 8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834200701	8 DT-SP R-SF	DT-SP R-MF
8342007008 DT-SP R-SF DT-SP R-MF 8342007020 DT-SP R-SF DT-SP R-MF 8340029010 DT-SP R-SF DT-SP R-MF 8340029013 DT-SP R-SF DT-SP R-MF 8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834200602	0 DT-SP R-SF	DT-SP R-MF
8342007020 DT-SP - R-SF DT-SP - R-MF 8340029010 DT-SP - R-SF DT-SP - R-MF 8340029013 DT-SP - R-SF DT-SP - R-MF 8340030003 DT-SP - R-SF DT-SP - R-MF 8340030015 DT-SP - R-SF DT-SP - R-MF 8340030004 DT-SP - R-SF DT-SP - R-MF	834200700	6 DT-SP R-SF	DT-SP R-MF
8340029010 DT-SP R-SF DT-SP R-MF 8340029013 DT-SP R-SF DT-SP R-MF 8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834200700	8 DT-SP R-SF	DT-SP R-MF
8340029013 DT-SP R-SF DT-SP R-MF 8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834200702	0 DT-SP R-SF	DT-SP R-MF
8340030003 DT-SP R-SF DT-SP R-MF 8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834002901	0 DT-SP R-SF	DT-SP R-MF
8340030015 DT-SP R-SF DT-SP R-MF 8340030004 DT-SP R-SF DT-SP R-MF	834002901	3 DT-SP R-SF	DT-SP R-MF
8340030004 DT-SP R-SF DT-SP R-MF	834003000	3 DT-SP R-SF	DT-SP R-MF
	834003001	5 DT-SP R-SF	DT-SP R-MF
8340030009 DT-SP R-SF DT-SP R-MF	834003000	4 DT-SP R-SF	DT-SP R-MF
	834003000	9 DT-SP R-SF	DT-SP R-MF
8340030011 DT-SP R-SF DT-SP R-MF	834003001	1 DT-SP R-SF	DT-SP R-MF

Exhibit A: Parcels in Downtown Pomona Specific Plan Update

AIN	Existing Zoning	Proposed Zoning
8340029009	DT-SP R-SF	DT-SP R-MF
8340030010	DT-SP R-SF	DT-SP R-MF
8340029020	DT-SP R-SF	DT-SP R-MF
8340030006	DT-SP R-SF	DT-SP R-MF
8340030005	DT-SP R-SF	DT-SP R-MF
8340030007	DT-SP R-SF	DT-SP R-MF
8340030024	DT-SP R-SF	DT-SP R-MF
8340029025	DT-SP R-SF	DT-SP R-MF
8340030012	DT-SP R-SF	DT-SP R-MF
8340029021	DT-SP R-SF	DT-SP R-MF
8340030023	DT-SP R-SF	DT-SP R-MF
8340030002	DT-SP R-SF	DT-SP R-MF
8340029019	DT-SP R-SF	DT-SP R-MF
8340029014	DT-SP R-SF	DT-SP R-MF
8340029012	DT-SP R-SF	DT-SP R-MF
8340030021	DT-SP R-SF	DT-SP R-MF
8340030013	DT-SP R-SF	DT-SP R-MF
8340030008	DT-SP R-SF	DT-SP R-MF
8340030014	DT-SP R-SF	DT-SP R-MF
8340030020	DT-SP R-SF	DT-SP R-MF
8340030019	DT-SP R-SF	DT-SP R-MF
8340029011	DT-SP R-SF	DT-SP R-MF
8340029018	DT-SP R-SF	DT-SP R-MF
8340030022	DT-SP R-SF	DT-SP R-MF
8340030018	DT-SP R-SF	DT-SP R-MF
8337019004	M-1	DT-SP MU-I
8337021051	M-1	DT-SP MU-I



AIN	Existing Zoning	Proposed Zoning
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP - Height Overlay - DownTown Gateway Segment	DT-SP MU-HDR
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
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	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core	DT-SP MU-CBD
	CSP DownTown Core CSP DownTown Core	DT-SP MU-CBD
		DT-SP MU-CBD
	CSP DownTown Core	
o341UU4U42	CSP DownTown Core	DT-SP MU-CBD

Exhibit B1: Parcels **Removed** from Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8341001031	CSP DownTown Core	DT-SP MU-CBD
8341001904	CSP DownTown Core	DT-SP MU-CBD
8341001012	CSP DownTown Core	DT-SP MU-CBD
8341001035	CSP DownTown Core	DT-SP MU-CBD
8341004022	CSP DownTown Core	DT-SP MU-CBD
8341001034	CSP DownTown Core	DT-SP MU-CBD
8341004041	CSP DownTown Core	DT-SP MU-CBD
8336026900	CSP - Height Overlay -	DT-SP MU-HDR
8341005023	CSP DownTown Core	DT-SP MU-CBD
8341004018	CSP DownTown Core	DT-SP MU-CBD
8341004054	CSP DownTown Core	DT-SP MU-CBD

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN Existing Zoning	Proposed Zoning
8337026010 DT-SP MU-AR	CSP City Gateway Segment
8337025016 DT-SP MU-AR	CSP City Gateway Segment
8337025020 DT-SP MU-AR	CSP City Gateway Segment
8337025019 DT-SP MU-AR	CSP City Gateway Segment
8337025018 DT-SP MU-AR	CSP City Gateway Segment
8337025011 DT-SP MU-AR	CSP City Gateway Segment
8337025010 DT-SP MU-AR	CSP City Gateway Segment
8337025013 DT-SP MU-AR	CSP City Gateway Segment
8337025017 DT-SP MU-AR	CSP City Gateway Segment
8337025014 DT-SP MU-AR	CSP City Gateway Segment
8337025012 DT-SP MU-AR	CSP City Gateway Segment
8337025021 DT-SP MU-AR	CSP City Gateway Segment
8337026009 DT-SP MU-AR	CSP City Gateway Segment
8337026008 DT-SP MU-AR	CSP City Gateway Segment
8337025022 DT-SP MU-AR	CSP City Gateway Segment
8337026011 DT-SP MU-AR	CSP City Gateway Segment
8335022008 DT-SP MU-AR	CSP MidTown Segment
8335021017 DT-SP MU-AR	CSP MidTown Segment
8335022016 DT-SP MU-AR	CSP MidTown Segment
8335021020 DT-SP MU-AR	CSP MidTown Segment
8335022007 DT-SP MU-AR	CSP MidTown Segment
8335021015 DT-SP MU-AR	CSP MidTown Segment
8335021055 DT-SP MU-AR	CSP MidTown Segment
8335022009 DT-SP MU-AR	CSP MidTown Segment
8335021054 DT-SP MU-AR	CSP MidTown Segment
8335021008 DT-SP MU-AR	CSP MidTown Segment
8335022017 DT-SP MU-AR	CSP MidTown Segment
8335021019 DT-SP MU-AR	CSP MidTown Segment
8335022014 DT-SP MU-AR	CSP MidTown Segment
8335021018 DT-SP MU-AR	CSP MidTown Segment
8335021053 DT-SP MU-AR	CSP MidTown Segment
8335021016 DT-SP MU-AR 8335022006 DT-SP MU-AR	CSP MidTown Segment CSP MidTown Segment
8341010041 DT-SP MU-AR	CSP MidTown Segment
8341010041 DT-SF MU-AR	CSP MidTown Segment
8341010024 DT-SP MU-AR	CSP MidTown Segment
8335022015 DT-SP MU-AR	CSP MidTown Segment
8342013015 DT-SP MU-AR	CSP MidTown Segment
8342013021 DT-SP MU-AR	CSP MidTown Segment
8341010040 DT-SP MU-AR	CSP MidTown Segment
8342013016 DT-SP MU-AR	CSP MidTown Segment
8341008016 DT-SP MU-CBD	CSP MidTown Segment
8341007020 DT-SP MU-CBD	CSP MidTown Segment
8341007900 DT-SP MU-CBD	CSP MidTown Segment
	CSI WIIGTOWII SCRIIICIIL
8341007027 DT-SP MU-CBD	CSP MidTown Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN Existing Zoning	Proposed Zoning
8341007030 DT-SP MU-CBD	CSP MidTown Segment
8341007022 DT-SP MU-CBD	CSP MidTown Segment
8341008904 DT-SP MU-CBD	CSP MidTown Segment
8341007033 DT-SP MU-CBD	CSP MidTown Segment
8341007026 DT-SP MU-CBD	CSP MidTown Segment
8335014912 DT-SP MU-I	CSP MidTown Segment
8335014910 DT-SP MU-I	CSP MidTown Segment
8335014907 DT-SP MU-I	CSP MidTown Segment
8335014913 DT-SP MU-I	CSP MidTown Segment
8335014909 DT-SP MU-I	CSP MidTown Segment
8335014915 DT-SP MU-I	CSP MidTown Segment
8335014908 DT-SP MU-I	CSP MidTown Segment
8341013030 DT-SP MU-I	CSP MidTown Segment
8335014916 DT-SP MU-I	CSP MidTown Segment
8341007002 DT-SP R-SF	CSP MidTown Segment
8341007004 DT-SP R-SF	CSP MidTown Segment
8341007003 DT-SP R-SF	CSP MidTown Segment
8342012010 DT-SP R-SF	CSP MidTown Segment
8342012012 DT-SP R-SF	CSP MidTown Segment
8342012009 DT-SP R-SF	CSP MidTown Segment
8342012003 DT-SP R-SF	CSP MidTown Segment
8342012011 DT-SP R-SF	CSP MidTown Segment
8342012004 DT-SP R-SF	CSP MidTown Segment
8341007001 DT-SP R-SF	CSP MidTown Segment
8342012013 DT-SP R-SF	CSP MidTown Segment
8342012001 DT-SP R-SF	CSP MidTown Segment
8342012002 DT-SP R-SF	CSP MidTown Segment
8336020008 DT-SP MU-AR	CSP - Height Overlay - City Gateway Segment
8336020017 DT-SP MU-AR	CSP - Height Overlay - City Gateway Segment
8340024008 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8340024007 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8340024014 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8340028002 CSP - Height Overlay -	, ,
8340027009 CSP - Height Overlay -	
8336023011 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336021017 DT-SP MU-AR 8336021018 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336023018 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	CSP - Height Overlay - DownTown Gateway Segment
8336023021 DT-SP MU-AR 8336023022 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment CSP - Height Overlay - DownTown Gateway Segment
8336023016 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336023017 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336021016 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336023026 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336021901 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8340028003 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
	4 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
834002800	5 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	5 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
	7 DT-SP MU-AR	CSP - Height Overlay - DownTown Gateway Segment
8336017019	9 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
	4 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017013	1 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016029	9 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017010	DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016024	4 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017018	B DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
833601601	5 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017009	9 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016016	5 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017003	3 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
833601601	7 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017004	4 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016013	3 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336017008	B DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8336016025	5 DT-SP MU-I	CSP - Height Overlay - DownTown Gateway Segment
8340028009	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8340028008	B DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8340028012	2 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8340028013	1 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
8340028010	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	1 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	2 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	1 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	3 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	2 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	4 DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	DT-SP R-SF	CSP - Height Overlay - DownTown Gateway Segment
	B DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	7 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	7 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	4 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	5 DT-SP MU-AR 5 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
) DT-SP MU-AR	CSP - Height Overlay - MidTown Segment CSP - Height Overlay - MidTown Segment
	9 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	2 DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	DT-SP MU-AR	CSP - Height Overlay - MidTown Segment
	7 DT-SP MU-I	CSP - Height Overlay - MidTown Segment
	9 DT-SP MU-I	CSP - Height Overlay - MidTown Segment
022201200;	л Б 1-3F IVIU-I	Cor - Height Overlay - Mintrown Segment

Exhibit B1: Parcels **Added** to Pomona Corridors Specific Plan

AIN	Existing Zoning	Proposed Zoning
8335013019	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335012015	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013909	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013807	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013808	B DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013806	DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335012901	. DT-SP MU-I	CSP - Height Overlay - MidTown Segment
8335013030	DT-SP MU-I	CSP - Height Overlay - MidTown Segment

EXHIBIT B2

The following text changes shall be made to the Pomona Corridors Specific Plan as a related action to the comprehensive update to the Downtown Pomona Specific Plan:

1. Change the entirety of 2.2.1 Use Types in the Downtown Core Segment (2.1.2) to read as follows:

2.2.1 Use Types

A. Retail

- 1. Specialty Goods Anchors (--)
- 2. Community Oriented Anchors (NC)
 - 3. Entertainment Anchors (--)
- 4. Eating & Drinking Establishments (NC)
 - 5. Specialty Goods & Foods (NC)
- 6. Entertainment & Recreation (permitted)
 - 7. Convenience Uses (NC, CS)
 - 8. Business Services (permitted)
 - 9. Personal Services (permitted)
- 10. Service Commercial & Repair (conditional)
- 11. Large Scale Commercial Goods (conditional)
 - 12. Vehicle Sales (--)
 - B. Civic & Cultural (conditional)
 - C. Workplace
 - 1. Professional Services (permitted)
 - 2. Medical Services (permitted)
 - 3. Light Industrial (permitted)
 - D. Lodging (permitted)
 - E. Live Work (permitted)
 - F. Residential
 - 1. Multi-Family w/ Common Entry (permitted)
- 2. Multi-Family w/ Individual Entries (permitted)
 - 3. Attached Single Family (permitted)
 - 4. Detached Single Family (--)

2. Change "Community Oriented Anchors" in 2.2.1 Use Types in the Midtown Segment (2.1.11) to read as follows:

2. Community Oriented Anchors (permitted) (L2)

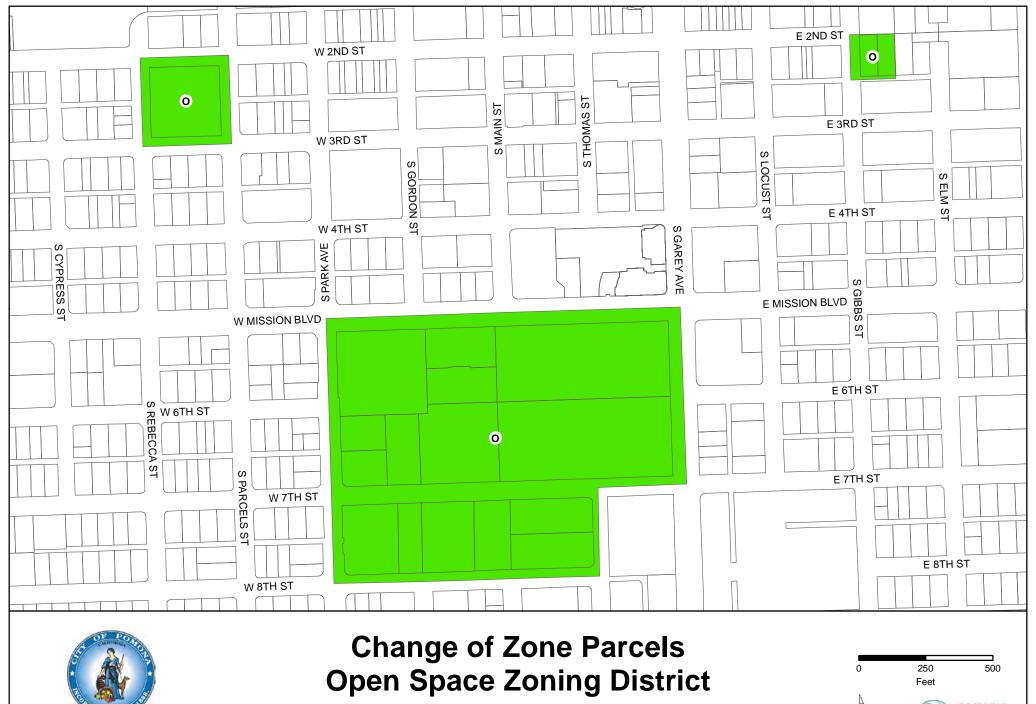




EXHIBIT C



Zoning_Open_Space_2019_20190613.mxd

Exhibit C: Parcels Added to Open Space Zoning District in Pomona Zoning Ordinance

AIN	Existing Zoning	Proposed Zoning
8341009908	DT-SP MU-I	OS
8341011906	DT-SP MU-I	OS
8341013935	DT-SP MU-I	OS
8341014908	DT-SP MU-I	OS
8341012933	DT-SP MU-I	OS
8341011912	DT-SP MU-I	OS
8341003900	DT-SP MU-I	OS
8341013931	DT-SP MU-I	OS
8341012932	DT-SP MU-I	OS
8341014909	DT-SP MU-I	OS
8341009910	DT-SP MU-I	OS
8341013933	DT-SP MU-I	OS
8341013934	DT-SP MU-I	OS
8335007910	DT-SP MU-I	OS
8335007909	DT-SP MU-I	OS
8341011901	DT-SP MU-I	OS
8341009911	DT-SP MU-I	OS
8341010922	DT-SP MU-I	OS



ERRATA SHEET

The following table identifies changes made to the Draft of the Downtown Pomona Specific Plan between its release on May 20, 2019 and posting of agenda on June 20, 2019, in addition to recommend changes made since June 20, 2019, for Planning Commission consideration at the public hearing scheduled for June 26, 2019.

ERRATA SHEET PAGE INDEX

CH. 1. INTRODUCTION: PAGE 2

CH. 2. PRIVATE REALM: PAGES 3 TO 12

CH.3. PUBLIC REALM: PAGES 13 TO 15

APPENDIX: PAGES 16 TO 17

ADDITIONAL RECOMMENDED CHANGES: PAGE 18

CHAPTER 1: INTRODUCTION

PAGE	CHANGE
1	Content edit for "Introduction" and captions:
	Change from "The Downtown Pomona Specific Plan (Specific Plan) is a comprehensive document that implements the vision for the Specific Plan area established by the City of Pomona General Plan and the Specific Plan planning process" to "The Downtown Pomona Specific Plan is a comprehensive document that implements the vision for Downtown Pomona established in the City of Pomona General Plan." Caption: "Second Street"
5-7	Content edit for "Transit Center," "Historic Context," and "Arts and Culture" and captions:
	Change "connects to the Central Business District." to "connects to Downtown Pomona."
	Change "34 acres" to <u>"3.42 acres"</u>
	Remove "Within the north and south western Specific Plan boundaries there are a number of Craftsman bungalows and Victorian cottages. Although many of these houses are not in good condition, there are enough houses of architectural interest that this area could be renovated to be a very desirable neighborhood."
	Change from "Moving forward, it is envisioned that the Arts Colony will move beyond Second Street and into the greater Downtown area, as these venues and works of art help contribute to the City's overall presence and identity, especially within Downtown Pomona," to "The Arts Colony has already moved beyond Second Street, and is envisioned to include the entirety of the Downtown Pomona Specific Plan Boundary, as the arts are fundamental to Downtown's identity."
	Caption: "Antiques Row"
8	Content edit for "Community Engagement"
	Update photos
	Change from "Address undesirable uses and vagrants" to "Address undesirable uses"
9	Remove "Opportunities and Constraints" section
11	Content edit for "Planning Principles"
	Change "Celebrate Arts and Culture—continue and expand arts, culture and appropriate entertainment opportunities" to "Celebrate Arts and Culture—continue and expand arts, culture and entertainment opportunities."
	Change "Inviting and Safe—foster a safe and clean downtown for residents, visitors and workers and discourage unwelcome uses" to "Inviting and Safe—foster a safe and clean downtown for residents, visitors and work workers."

CHAPTER 2: PRIVATE REALM

PAGE	CHANGE				
21	Content edit for "How to Use This Chapter"				
	Change "The following steps are recommended to ensure project consistency with the contents provided herein:" with "These sections are further categorized as follows:"				
	Change "Area Wide Development Standards" to "Area Wide Urban Form Standards"				
	Change "Design Guidelines" to "Urban Form Guidelines"				
23	Change "District Specific Development Standards" to "District Specific Urban Form Standards" Content edit to "Applicability"				
	Change "All nonconforming buildings and structures are subject to the regulations pertaining to nonconformity contained in the Pomona Zoning Code" to "All nonconforming buildings and structures are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions. Applicability thresholds shall not apply to legally non-conforming single-unit residential, which shall be subject to R-1-10,000 standards."				
24	Content edit to "Review and Approval Process"				
	Change "The action taken by the Development Services Director on a Development Plan Review at a public hearing is final unless appealed to the Planning Commission by either the applicant or a member of the Pomona City Council within 20 calendar days of the action" to "The action taken by the Development Services Director on a Development Plan Review at a public hearing is final unless appealed to the Planning Commission by either the applicant or a member of the Planning Commission within 20 calendar days of the action." Change "If the applicant has not consistently pursued necessary permits for construction"				
	to "If the applicant has not obtained grading or building permits or consistently pursued necessary permits for construction"				
	Add Landscaping. Landscaping is subject to the requirements of Section .503-J-Landscaping of the Pomona Zoning Ordinance, except for percentage minimums for parcels on residential and nonresidential landscape standards.				
25	Content Edit to "Areawide Urban Form Standards"				
	Change "Areawide Form Standards" to "Areawide Urban Form Standards"				
	Remove "Interior Pedestrian Walkway Design"				
	Change "To Circulation Network. Regular connections between on-site walkways and the public sidewalk shall be provided. An on-site walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage. Such walkway shall be the shortest practical distance between the main entry and sidewalk, generally no more than 125 percent of the straight-line distance" to				
	To Circulation Network. Regular connections between onsite walkways and the public sidewalk shall be provided.				

DOWNTOWN POMONA SPECIFIC PLAN / JUNE 26 2019

- A. An on-site walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage.
- B. Walkways shall be the shortest practical distance between the main entry and sidewalk, generally no more than 125 percent of the straight-line distance.
- <u>C. Where a required walkway crosses a driveway, parking area, or loading area, it must be clearly identifiable through the use of a raised crosswalk, a different paving material, or similar method.</u>
- D. Where a required walkway is parallel and adjacent to an auto travel lane, it must be raised or separated from the auto travel lane by a raised curb at least four inches (4") high, bollards or other physical barrier.
- 27 Remove "Good Neighbor" section

28-29 | Content edits to Parking

Change "Parking for Downtown is either self-contained or located within parking facilities that serve groups of buildings. The majority of the Downtown area south of First Street, including Western University, has shared parking provided in public parking facilities. Parking ratios are provided in Chapter 2 of this plan. Standards pertaining to dimensions, such as drive aisle width, etc., are provided in Section .503-H of the Pomona Zoning Ordinance. The zoning code shall apply except when parking is provided by the Vehicle Parking District and except when alternative parking standards are established by a registered engineer under an approved parking study adopted as part of a conditional use permit." to "The following provisions apply to parking within the Downtown Pomona Specific Plan."

Change "Parking shall be located on site or in Vehicle Parking District, subject to parking sufficiency approval by the Board of the Vehicle Parking District" to "Parking shall be located on site or provided within 1,000 feet of proposed development. However, at least 50% of total required parking shall be provided on-site. Moreover, the use of off-site parking as part of a proposed development shall require conditions of approval to ensure the ability to use such parking for an extended period of time."

Add "Parking requirements do not apply to the first two floors of any proposed development within the MU-CBD and MU-I districts. Moreover, the first two floors of any existing development within said boundaries are also not subject to parking requirements."

Add <u>"Parking structures adjacent to residential uses shall incorporate design features to minimize light or sound impacts."</u>

Add "Light fixtures within parking structures shall be designed to minimize off-site spill lighting."

Change from

- C. Parking Ratios
- 1. Residential Uses (per unit)
- a. Live/Work: 1 min / 1 max
- b. Studio: 1 min / 1 max
- c. 1br: 1.5 min / 1.5 max
- d. 2br: 2 min / 2 max
- e. Additional bedrooms: .5/room
- f. Guest: 1 per 4 units max
- 2. Public/Semi-Public Uses
- a. 1 per 35 square feet of assembly area
- 3. Commercial (per 1,000 total square feet)
- a. Retail: 3 min / 4 max
- b. Eating & Drinking: 4 min / 10 max
- c. Entertainment & Recreation: 6 min / 10 max

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- d. Office: 3 min / 4 max
- e. Medical Office: 4.5 min / 5.5 max
- 4. Lodging (per guest room)
- a. Hotel: 1 min / 1.2 max

to

- E. Parking Ratios
- 1. Residential Uses (per unit)
- a. Live/Work: .5 min / .5max
- b. Studio: .5 min / .5 max
- c. One bedroom: 1 min / 1 max
- d. Two bedroom: 1.5 min / 1.5 max
- e. Additional bedrooms: .5/room
- 2. Public/Semi-Public Uses
- a. One per 35 square feet of assembly area
- 3. Commercial (per 1,000 total square feet)
- a. Retail: 2.5 min / 3.5 max
- b. Eating & Drinking: 3 min / 8 max
- c. Entertainment & Recreation: 5 min / 8 max
- d. Office: 2.5 min / 3.5 max
- e. Medical Office: 4 min / 5 max
- 4. Lodging (per guest room)
- a. Hotel: 1 min / 1.2 max

Change from

D. Parking Reductions

Minimum parking requirements may be reduced by the following amount where applicable:

- 1. 10% reduction for all development located within a 3,000 foot walk from a light rail line or bus rapid transit stop.
- 2. 5% reduction for all development that is 4 floors or taller.
- 3. Provide a parking demand study to substantiate a reduction in total parking requirements; studies are subject to review and approval by the Development Services Director.

To

C. Parking Relief

1. Provide a parking demand study, conducted by a registered traffic engineer, to substantiate a reduction in total parking requirements; studies are subject to review and approval by the Development Services Director.

Remove graphic

32-39 Content edit to "Areawide Urban Form Guidelines"

Change "Areawide Design Guidelines" to "Areawide Urban Form Guidelines"

Change "General Design Elements" to "General Guidelines"

Change "The following general design elements..." to <u>"The following general urban form guidelines.."</u>

Change "Significant, sustainable landscape design elements..." to <u>"Significant, sustainable, drought-tolerant landscape design..."</u>

Change from

- "The following design elements are not desirable and should be avoided:
- 1. Large, blank, unarticulated wall surfaces.
- 2. Unpainted, plain color concrete block walls.
- 3. Highly reflective surfaces.
- 4. Metal or plastic siding on prominent buildings.
- 5. Visible outdoor storage, loading and equipment areas.
- 6. Exterior security bars, gates, and grills are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
- 7. Chain link fencing is prohibited.

to

- <u>"The following design elements are not desirable and should be avoided:</u>
- 1. Large, blank, unarticulated wall surfaces.
- 2. Visible outdoor storage, loading and equipment areas.
- 3. Exterior security bars, gates, and grills are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
- 4. Chain link fencing is prohibited.

The following design elements are encouraged only if incorporated into an architecturally interesting or artistic, unique development:

- 1. Unpainted, plain color concrete block walls.
- 2. Highly reflective surfaces.
- 3. Metal Siding."

Change "Site Planning and Design" to "Site Planning Guidelines"

Remove "New development should provide a minimum landscaped area along any shared property line with single-family detached residential zones, utilizing plant materials that visually combine with residential character."

Change from "Parking" to "Parking Guidelines"

Move to Parking Standards: "Parking structures adjacent to residential uses should incorporate design features to mitigate light or sound impacts. Light fixtures within parking structures should be designed to minimize off-site spill lighting."

Change "Building Form and Articulation" to "Architectural Guidelines"

Remove "At the intersection of single-family residential zones, non-residential buildings should step down building heights and utilize varying roof elements, such as hip or gable roofs, to reduce building mass."

Change from "All rooftop equipment shall be screened..." to <u>"All rooftop and mechanical equipment shall be screened..."</u>

Change from "Landscaping" to "Landscaping Guidelines"

	1					
	Change "Landscape treatments should incorporate colorful plant material" to "Landscape treatments should incorporate colorful native plant material"					
	Remove "Entry nodes should include low walls with signage, identification signage or monument signage, where adequate space allows for their construction."					
40	Change boundary of Mixed-Use Central Business District (MU-CBD) by expanding westw Parcels Street, including three parcels at the northwest corner of Parcels & Second (location former Armory)					
	Correct the location of Centennial Park					
43	Content edits to MU-CBD					
	Change "Density 50 to over 100 units/acre" to <u>East of Park Street: 50 to over 100</u> <u>Maximum West of Park Street: 40 to 70</u>					
	Content edits to MU-HDR					
	Change "allowing shifts in residential densities and housing types" to "allowing shifts in residential densities and housing types, including housing for artists, educators, and work-live units"					
	Content edits to R-MF					
	Remove "Maximum Height (ft.) 75"					
50	Content edits to "Incentives"					
	Add "At least 25% of all residential units must meet the standards of the Work/Live ordinance of the City of Pomona, including unit size and operations."					
	Change "A 50% reduction in total common open space requirements for proposed development" to "A 75% reduction in total common open space requirements for proposed development"					
51	Content edits to "Land Use"					
	Change "All nonconforming uses are subject to the regulations pertaining to nonconformity contained in the Pomona Zoning Code" to "All nonconforming uses are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions."					
55	Content edits to "Building Use Regulations"					
	Change from					
	"P2: The use is permitted only in upper story levels, lower levels/basement or in the rear of a building subject to the following provisions: • Maximum Area of Rear Use - Rear uses shall not exceed 50% of the ground story area of the unit, unless it can be determined by the Planning Division that designing the partition between the rear and front use would be more logical if it incorporated permanent structural or mechanical components of the building. In no event shall the rear use occupy more than 65% of the					
	ground story area. • Separation of Related Rear Use – When related P (Permitted Use) and P2 (Permitted Use in Upper Levels, Lower Levels or Rear) are located adjacent to one another within a structure, no separation will be required. Related uses shall include uses that have a common product or					

Dance Studio P P P -

customer base. For example, the retail and manufacturing portions of the El Merendero bakery are considered related uses. • Separation of Unrelated Rear Uses - Although there will be flexibility in the nature and design of the physical separation between unrelated uses, the use of walls or other physical separation will be encouraged. • Access to Unrelated Rear Uses – A separate rear or side entrance shall be provided for all unrelated uses. Entrances should be discouraged on service-oriented alleys.' toP2: The use is permitted only in upper story levels, lower levels/basement or in the rear of a ground-floor building subject to the following provisions: • In no event shall the rear use occupy more than 65% of the ground story area. 53 Content edit to "Minor Conditional Use Permit" Add "To approve a use, the Planning Division shall make the same findings as for a Conditional Use Permit as contained in the Pomona Zoning Ordinance." 57 Content edit to "Land Use Matrix" Change Single-Unit Dwelling from "P/P/P/P" in all districts to "-/-/-" Remove Single Room Occupancy classification Change Colleges and Trade Schools from "P2" in MU-CBD to "C" Change Cultural Institutions from "P/P/P/M" to "M/M/M/M" Change Clinics from "P" in MU-CBD to "-" Change Instructional Services from "P/P/P/-" to "M/M/M/M" Change Performing Arts, Fine Arts, Dinner Theater from "P" in MU-CBD to "M" Change Schools from "M/P/M/C" to "C/C/C/C" Add Alcohol Sales (on-sale only) as "M/M/M/-" Add Alcohol Sales (off-sale only) as "C/C/C/-" Change Veterinary Services from "P" in MU-CBD to "-" Change from Art Sales and Services - - - -Art Boutique P P P -Acting Studio P P P -Art Co-Op (ARI's) P P P -Art Gallery P P P -Art Museum PPP-Art Supply Store PPP -Arts and Crafts Store PPP -Carpentry Studio P P P -Culinary School P2 P2 P2 -

Decorative Ironworks P P P -

Design Studio P P P -

Fashion Design Studio PPP -

Film/Animation Studio P P P -

Furniture Restoration M M M -

Glassworks PPP-

Jewelry Design Studio P P P -

Painting Studio P P P -

Performing Arts Center P P P -

Photographic Arts Center P P P -

Pottery Studio P P P -

Printer M M M -

Recording Studio P P P -

Sculpture Studio P P P -

To

Art Sales and Services

Art Gallery P P P P

Artist Studio P P P P

Artisanal Manufacturing M M M M

Artist Cooperative (Co-op) P P P P

Remove Automobile Rentals

Change "Hotels and Motels" to "Hotels"

Change Maintenance and Repair Services from "P2/-/C/-" to "-/-/-"

Change Walk-in Clientele in MU-CBD from "P" to "-"

Remove Custom Manufacturing. Reclassify as Artisanal Manufacturing

60 Content edit to "Temporary Uses"

Change "at least ten days prior" to "at least 30 days prior"

Remove "...and Pomona Zoning Code" under Approval

Change "Pop-up restaurant uses may not also serve alcohol" to <u>"The sale of alcohol may be permitted subject to approval by Alcoholic Beverage Control (ABC)"</u>

Remove "Special events on private property. This temporary use applies to events on a private property if one of the following conditions applies: an event that will exceed the occupancy limit of the existing building or tenant space; an event that takes place on a vacant lot; or an event that takes place outside of an existing building. The temporary use permit shall be subject to the following conditions:

i. Limited to six days in a calendar month, with no more than three consecutive days.

C. Other temporary uses which meet the required"

Add Add

2.3.4 Non-Conforming Provisions

1. Uses that legally exist and are permitted prior to the adoption of this plan, shall be classified as legal, nonconforming uses and are allowed to continue operation provided they do not cause any adverse health, safety, or aesthetic related impact(s) on the community. These uses are not allowed to expand to additional sites, locations or addresses in a zone where the use is not specified as a permitted use except as allowed herein and as approved by the Planning

- Commission under a Conditional Use Permit. A legal non-conforming business may be sold or have a change of management, including a new building occupant or tenant with the same use as previously existed without having to revert to a use that is permitted by the applicable zone. A legal non-conforming use may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.
- 2. Commercial or residential use. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone unless a continuation or reestablishment of that commercial or residential nonconforming use is approved by the Planning Commission under a Conditional Use Permit within three years of its closure as noted herein and said use is shown to be a use that will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole. A revocation hearing will be held for a legal non-conforming use that displays a pattern of code violations or which create a public nuisance and if the use is revoked, itis deemed "closed" and must revert to a use that is permitted by the applicable zone. The continuation or reestablishment of a revoked non-conforming use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management, including a new building occupant and/or tenant, and that such continued or re-established use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation.
- 3. Residential structures. When a legal nonconforming, residential structure(s) is/are demolished, deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, the land use at that location must revert to a use permitted by the applicable zone. Where a legal nonconforming, residential structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of a Building Permit. A revocation hearing will be held for a legal non-conforming residential use that displays a pattern of code violations or creates a public nuisance and if the use is revoked, it is deemed "closed" and must revert to a use that is permitted by the applicable zone. The re-establishment of a revoked legal nonconforming residential use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management and that such re-established residential use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation. Applicability thresholds shall not apply to legally nonconforming single-unit residential, which shall be subject to R-1-10,000 standards.
- 4. Commercial or residential use within a nonconforming structure. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business or residence to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone, except that the same or substantially the same use may be reestablished or established within said non-conforming structure within three years of its closure as noted herein when that structure was specifically designed and built for, and historically occupied by, the same or substantially the same type of use.

5. Non-conforming structures. When a structure that was built to all applicable standards and codes at the time of construction and under an appropriately issued Building Permit becomes nonconforming due to a change(s) to the development standards of the Downtown Pomona Specific Plan (including setbacks, height limits, landscaping, parking, architecture, equipment screening, storage, fencing, etc.), said structure may continue to be occupied or may be reoccupied by a use permitted within the underlying applicable zone which required the same standards consistent with those for the structure as it was originally constructed. Said occupation or reoccupation may occur at any time provided that the structure has been maintained in a safe and sanitary condition, has not become a public or attractive nuisance, has not been physically altered in a manner that would reduce conformance to the original development standards at the time of original construction and upon approval of a Conditional Use Permit by the Planning Commission. Legal non-conforming structures may be sold or have a change of management, including a new building occupant or tenant with the same or substantially similar use as historically existed without having to physically alter or modify the structure or site to conform to the current development standards of the underlying applicable zone. When a legal nonconforming structure is deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, any future structure(s) at that location must conform to the development standards of the underlying applicable zone. Where a legal non-conforming structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate and re-occupy (with the same or substantially similar use) said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of an appropriate Building Permit.

A legal non-conforming structure may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.

64 Content edit to Signs

Change from "All new primary, secondary, and portable signs; historic signs; and alterations to existing signsbeyond like-for-like replacementor repair." To "All new primary, secondary, and portable signs; repurposed signs; and alterations to existing signs beyond like-for-like replacement or repair."

Change "All new signs on a registered historic landmark..." to "All new signs on a registered historic landmark or district..."

Remove "a maximum of two tenants per sign face are permitted" under Low-Profile Monument Signs

Remove "Restaurant menu signs shall be provided for all restaurants with table service" under Restaurant Menu Signs

Remove "Such signs are only permitted to identify a building or non-residential tenant that occupies over 50 percent of the building"

Change "maximum of 180 days" to <u>"maximum of 180 calendar days"</u> under Temporary Banner Signs

Change "Maximum size of a temporary banner is no more than 45 square feet in area, with a maximum length of 15 feet, and a maximum width of three feet." To "Maximum size of a temporary banner is no more than 45 square feet in area."

Remove "The tenant directory sign shall be mounted directly on a solid wall or incorporated into

DOWNTOWN POMONA SPECIFIC PLAN / JUNE 26 2019

a freestanding sign located on the property on which the tenants are located" under Tenant Directory Signs

Remove "(See zoning ordinance section .503-K-G.9 for additional wall sign regulations)"

Remove "Addresses larger than 3" in height" under Prohibited Signs

Remove "Sign Copy" provisions under General Criteria

Change from

Legal non conforming signs for a business, owner, product, service or activity that is no longer on the premise shall be brought into conformance prior to any new establishment. All signs that advertise or identify a business, owner, product, service or activity that is no longer on the site for a period exceeding 60 days without being replaced by a new establishment, shall be considered abandoned and shall be removed. Minor repairs, maintenance and upkeep of legal nonconforming signs are permitted but in no case shall the sign be modified or altered that would result in an increase of non-conformity. Any legal non-conforming sign that is destroyed by natural causes may not be repaired, reconstructed, or replaced except in conformity with all provisions of the sign section of the DPSP. All remnants of the former sign shall be cleared and removed. A non-conforming sign shall be considered "destroyed" if the cost of such reconstruction, repair or rebuilding exceeds 50% of the sign's value prior to the damage.

To

"Minor repairs, maintenance, and upkeep of legal nonconforming signs are permitted but in no case shall the sign be modified or altered that would result in an increase of non-conformity. All signs that advertise a business that is no longer on site shall be considered abandoned and shall be removed within 60 days of the opening of a new establishment, unless the sign is approved under a repurposed sign permit. Any legal nonconforming sign destroyed by natural causes may be restored to its original state."

CHAPTER 3: PUBLIC REALM

PAGE	CHANGE					
89	Update Figure 3-3 Bicycle Network for clarity of legend					
92	Update Figure 3-4 Bicycle parking locations for accuracy					
102- 106	Remove					
	Parking Implementation					
	Parking as a Revitalization Incentive					
	A coordinated parking approach enables reduction of required parking ratios for the mixed-use land use districts as compared to viewing parking on a parcel- by-parcel basis. Thus, property owners and developers are not forced to buy land or pay fees to purchase unneeded parking. This may be accomplished with any number of potential parking reduction programs that promote alternative modes of transportation and reduce parking demands. These strategies may include parking space reductions for projects that provide free or discounted transit passes, on-site bike sharing, car share memberships, childcare facilities, or other accommodations such as Wi-Fi to support telecommuters.					
	Need for a Coordinated Parking Approach To create a cost-effective, user friendly Downtown parking system, there needs to be an overall approach overseen by an appropriate entity, such as the Vehicle Parking District (VPD), which deals with the construction, maintenance, security, financing and promotional issues associated with parking. Customers should enjoy a consistent easy-to- use, reasonably priced parking system that adds to the convenience and enjoyment of shopping, dining, entertainment or work.					
	Metrolink Parking's Strategic Role in Downtown Revitalization Metrolink riders present several important opportunities to support Downtown revitalization. First, daily commuters represent potential, regular customers for convenience goods and services. Through their exposure to the charm of the Downtown, the Metrolink riders can become potential return visitors for shopping, dining and entertainment. They can also share their experiences with their friends, providing effective, word-of-mouth promotion. Second, the commuter rail can carry visitors to the historic areas of Downtown featuring unique dining, shopping and entertainment on weekends and in the evening. In order to fulfill this strategic role, the Metrolink parking needs to have locations and connective pathways that bring riders through a portion of the Downtown. Attractive, informational and marketing kiosks, directories, catalogues, etc., should be readily available to Metrolink riders.					
	Maintenance of Parking Expansion Potential As shown in Figure 3-8 , parking in the Downtown area provides a combination of on-street and off-street parking totaling approximately 7,132 spaces. Most of these spaces are off-street surface lots accounting for approximately 4,858 spaces.					
	Replacement of Vehicle Parking District (VPD) Lots The Specific Plan considers replacement of several VPD lots to either create open space or a development opportunity site. According to the Vehicle Parking District Act of 1943, the City is required to initiate substitution proceedings pursuant to Section 31910 et seq. of the Act. These proceedings require that the City Council submit a resolution of declaring their proposed actions to the VPD Commission, and then receive and consider a report and recommendations from the VPD Commission regarding the proposed actions.					

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The Council may then adopt the proposed resolution in its original form or modify it in response to suggestions from the VPD Commission. The Council may then initiate substitutions proceedings. The contents of the substitution resolution must contain:

- The number of the Vehicle Parking District and a reference to the ordinance establishing its boundaries.
- A general description of the proposed substitution.
- Estimates of the fair market property value of the existing VPD property, and the fair market value and improvements cost for the replacement property to be acquired.

This resolution is followed by public hearings if not more than one-half of the owners of the assessable land object. If a hearing is held, then the Council makes findings related to the public interest and adopts a resolution of implementation.

Funding of Parking Lots and Structures

The funding sources are applicable to new or replacement parking lots or structures. Potential funding sources for parking lots and structures include:

- User fees such as in-and-out, hourly charges, monthly parking pass charges, valet parking charges, etc.
- Lease revenues from the "Retail Edges" recommended for a number of the parking structure edges to maintain pedestrian continuity.
- Development Fees in the form of Zoning Credits or In-Lieu Parking Fees, which give developers the right to construct by paying towards spaces in the Parking District as opposed to buying their own land for parking.
- Public support through local tax increments, or sales tax revenues and any state or federal programs which are or may become available.

Parking Security

Parking security should be coordinated with the overall promotion, maintenance and security district described in the Specific Plan. Parking security for the lots and structures requires:

- Proper lighting.
- Design of structures for maximum openness (clear span structures are recommended to minimize columns and add to the feeling of openness.)
- Electronic surveillance and regular patrols by parking facility operators.
- Encouragement of activity in the vicinity of parking facilities so the public also serves as a security force.
- Coordination between security staff of the parking facilities and the public, as well as, foot and bicycle patrols comprised of Pomona police officers who are authorized to make arrests.

Remove

- "3.2.21 Follow VPD procedures for potential conversion of existing public lots to open space amenities or redevelopment.
- 3.2.22 Coordinate with Metrolink to ensure adequate parking and supporting amenities such as information kiosks and directories."

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	Add 3.2.23 Pursue the expansion of on-street parking throughout Downtown Pomona.
111	Update Figure 3-9 Historic Areas and Landmark Structures for accuracy

APPENDIX

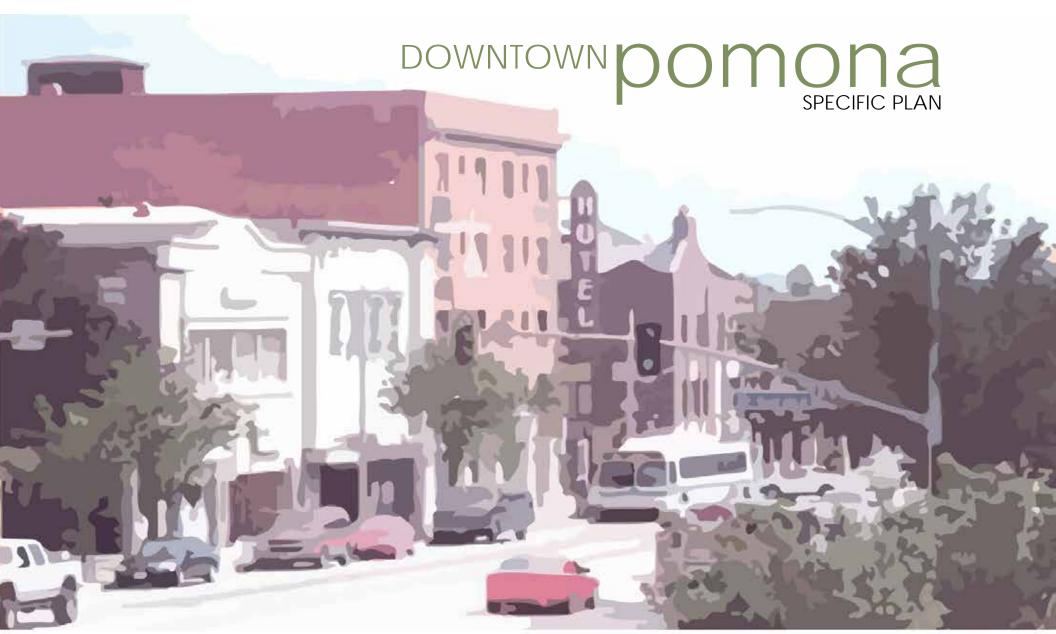
PAGE	CHANGE
157	Add the following definitions:
	Art Sales and Services.
	Art Gallery. Retail sales and display of pieces of art such as
	paintings, sculptures, ceramics, crafts and other works of
	visual art. Art pieces may be made on site.
	Artist Studio. Space primarily used as a work room for at least
	one artist that may be open to the public for demonstrations,
	classes, and retail sales and may involve the use of hand tools
	or small mechanical equipment. Examples include acting,
	dance, design, furniture restoration, film and animation,
	jewelry, screen printing, painting, photography, recording,
	sculpture. This does not include artist-related activities that
	involve heavy mechanical equipment, which are defined
	under "Artisanal Manufacturing".
	Artisanal Manufacturing. Any establishment primarily
	engaged in on-site production of goods which involves the
	use of heavy mechanical equipment and the incidental direct
	sale to consumers of only those goods produced on site.
	Includes large format ceramic studios, carpentry, decorative
	ironworks, glassworks, printing, and woodworking.
	Artist Cooperative (Co-Op). An organization or association
	jointly owned or controlled by its membership that provides
	professional facilities and services for artists, including
	studios, workshops, equipment, exhibition galleries, and
	educational resources.
	Food and Beverage Manufacturing (Small Scale). A small scale food
	and beverage products manufacturing that must include a retail
	sales component. It is characterized by local or regional products,
	specialty or artisanal foods. Examples include small coffee roasters,
	micro-breweries, micro-distilleries, wine manufacturing, meat or
	fish processing, and wholesale bakeries.
	Hotels. An establishment providing overnight lodging to transient
	patrons. These establishments may provide additional services,
	such as conference and meeting rooms, restaurants, bars, or
	recreation facilities available to guests or to the general public.
	Post anging Station A multip on mainstenanting and the state of the st
	Recharging Station. A public or private parking space that is served
	by battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive
	means) to a battery or other energy storage device in an electric
	vehicle.

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Solar Energy System. Roof- or building-mounted solar energy
systems attached to the top of a building or structure, generally
secured using racking systems that minimize impacts and is
mounted either level with the roof or tilted toward the sun. Does
not include ground-mounted or freestanding solar energy systems.

ADDITIONAL RECOMMENDED CHANGES

PAGE	CHANGE						
151	Add to Table 4-3 Implementation Action Plan Matrix:						
	"Linear Park: Develop a linear park along First Street frontage, between White and Towne						
	Avenues." Timing: 2 Lead: Public Works, Support/Coordination: Development Services, Potential Funding Sources: Measure A. FFID						
56	Potential Funding Sources: Measure A, EFID Add to Minor Conditional Use Permit:						
	Add to Millor Conditional OSE I Citilit.						
	"The Development Services Director may escalate the review of a Minor Conditional Use Permit						
	to the Planning Commission at their discretion."						
	Add to Development Plan Review:						
	"The Davidonment Compies Director may accelete the review of a Davidonment Plan Paviou to						
	"The Development Services Director may escalate the review of a Development Plan Review to the Planning Commission at their discretion."						
55-56	Change from						
	Offices						
	Business Professional & Technology (P2/P2/P/-)						
	Medical and Dental (P2/P2/P/-)						
	to						
	<u>Offices</u>						
	Business Professional & Technology (P2*/P2/P/-) Madical and Dantal (P2*/P2/P/-)						
	Medical and Dental (P2*/P2/P/-)						
	*"P" west of Park Ave.						



JUNE 2019 DRAFT

ACKNOWLEDGMENTS

Mayor and City Council

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DOWNTOWN POMONA SPECIFIC PLAN



PREPARED BY:



WITH SUPPORT FROM:

The Natelson Dale Group
Urban Crossroads

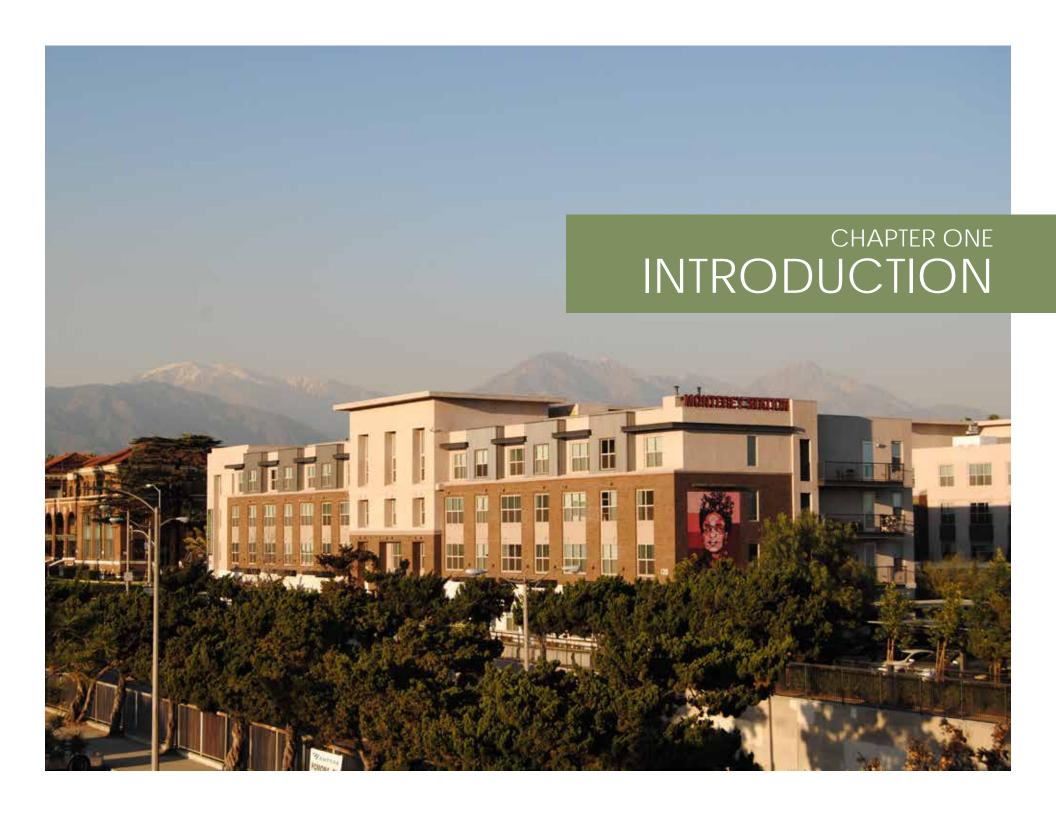


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	pg 10	1.3 Document Organization
	pg 11	1.4 Relationship to Other Planning Documents
	pg 12	1.5 Administration
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CHAPTER TWO	pg 19	2.1 Introduction
PRIVATE REALM	pg 21	2.2 Urban Form
	pg 51	2.3 Land Use
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CHAPTER THREE	pg 75	3.1 Introduction
PUBLIC REALM	pg 75	3.2 Mobility
	pg 103	3.3 Placemaking
	pg 115	3.4 Infrastructure and Utilities
CHAPTER FOUR	pg 125	4.1 Introduction
IMPLEMENTATION	pg 126	4.2 Attracting Private Investment and Providing Public Benefits
	pg 127	4.3 Economic Conditions Influencing Development Potentials in Downtown Pomons
	pg 137	4.4 Overview of Community Revitalization and Investment Authorities
	pg 141	4.5 Leveraging the City of Pomona's Opportunity Zones
	pg 144	4.6 Implementation Action Plan
appendix DEFINITIONS	pg 153	A.1 Definitions

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1.1 Context

1.1.1 Purpose

The Downtown Pomona Specific Plan is a comprehensive document that implements the vision for Downtown Pomona established by the City of Pomona General Plan. The Specific Plan was originally adopted in 1994 and has had several targeted updates. In 2017, the City of Pomona began a comprehensive update to the Specific Plan, funded by a grant from the Los Angeles County Metropolitan Transportation Authority (Metro) Round 4 Transit Oriented Development (TOD) Planning Grant Funds. The purpose of the grant is to create a regulatory environment supportive of TOD around station areas and adjacent transit corridors. TOD increases accessibility and utilization of public transportation. An additional purpose of the specific plan update is to reconcile overlapping development standards, land uses, and boundary conditions with the Pomona Corridors Specific Plan and implement the Pomona General Plan update, both adopted in March 2014.



Second Street

1.1.2 Boundary

Downtown Pomona is centrally located near several freeways, including Interstate 10, State Route 60, and State Route 71. Major universities are located nearby including California Polytechnic State University, Mount San Antonio Community College, University of La Verne, the Claremont Colleges and Western University of Health Sciences. Several major regional parks and the Ontario International Airport are also located in the vicinity. The Downtown Pomona Specific Plan area is located in the heart of Pomona, as shown in **Figure 1-1, Regional Context**.

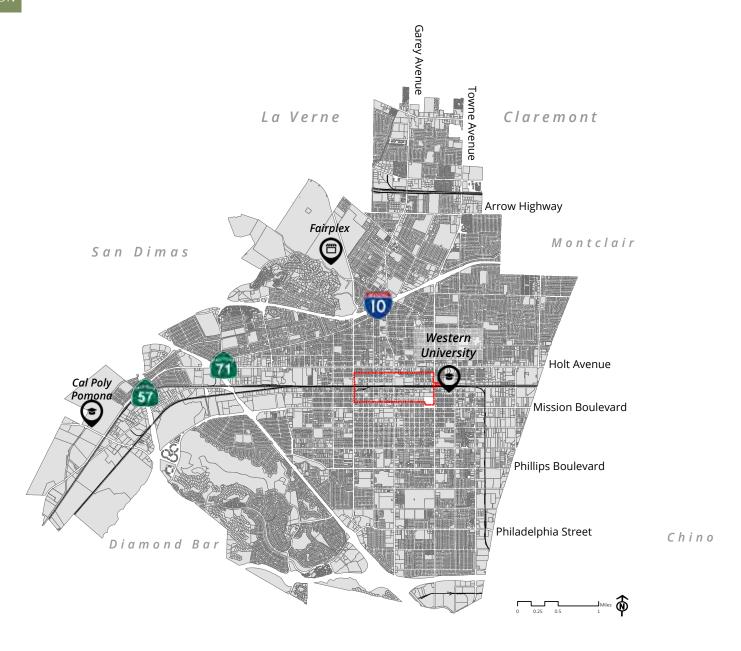


Figure 1-1, Regional Context

The Specific Plan pertains to all private and public properties that come under the purview of the Specific Plan as indicated in **Figure 1-2**, **Specific Plan Area**. The total acreage of the area is approximately 232 acres with approximate boundaries at Center Street to the north, Towne Avenue to the east, Fourth Street to the south, and White Avenue to the west.

1.1.3 General Plan Vision

As envisioned and described in the Pomona General Plan, Downtown is the community's center of business, culture and living. The Specific Plan area houses several assets and destinations such as the Pomona Transit Center and pedestrian bridge, the Arts Colony, Thomas Street, the Fox Theater and Western University of Health Sciences. Taking advantage of this diversity of uses, the General Plan reinforces Downtown as the mixed-use "heart of the City," placing emphasis upon its many activities and exceptional transit service. Building scale, location, and orientation foster the traditional pedestrian focus of Downtown. Likewise, streetscape and infrastructure improvements enhance walkability and the visual character of the City, helping to re-establish Pomona's identity and vitality.

Downtown will continue its renaissance as a mixed-use center enjoyed by locals and visitors alike. The greatest building heights and development intensities will be focused Downtown, paired with the Pomona Transit Center. Transit access — combined with Downtown's orientation to pedestrian use — makes it a place where people can live and work without relying on automobiles. As a result, particular emphasis will be placed on parking requirements appropriate to transit districts, strategically located shared parking facilities, and active parking management. As more people make Downtown their home, new parks and outdoor gathering spaces will make the district more livable and community-oriented.

Surrounding the activity and intensity of the Downtown core, the Downtown neighborhood will be a neighborhood with a difference. It will feature the City's widest range of contemporary housing types, and a wide mixture of uses that are compatible with the district's housing, all concentrated within walking distance of the Downtown Core's theater, shops, restaurants, cafes, nightlife, and amenities. As infill proceeds and the region continues to invest in its transit infrastructure, the neighborhood will benefit from an increasing level of service and activity generated by the Pomona Transit Center while also promoting transit ridership.

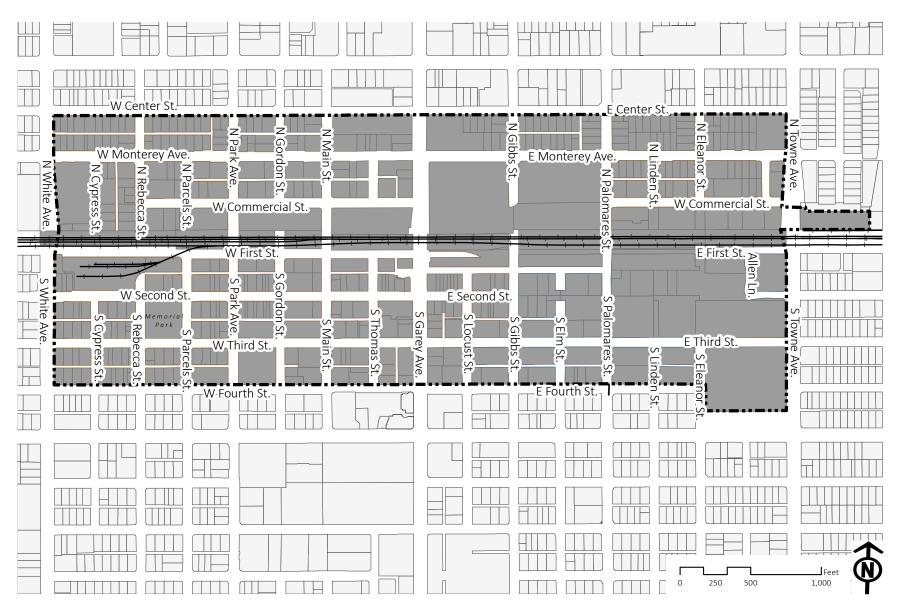


Figure 1-2, Specific Plan Area

5

Transit Center

The Pomona Transit Center, a facility providing both train and bus services, is located at the corner of Garey Avenue, between Commercial Street and the railroad tracks. The existing transit center has two distinct landmarks, the historic Pomona train station and the pedestrian bridge that services the station and connects to Downtown Pomona. In the immediate vicinity of the transit center, there are several sites potentially well-suited for future mixed-use development.



Pomona Transit Center

Historic Context

An important part of the Downtown Pomona Specific Plan is the preservation and enhancement of historic structures and areas. The Seventh Day Adventist Church and Fox Theater have already been designated as historic landmarks by City Council upon recommendation of the Historic Preservation Commission. There are four additional key structures identified in the original Specific Plan which were recommended to be designated as City of Pomona Historic Landmark Structures - Southern Pacific Rail Station, YMCA, Armory, and Masonic Temple. In 1986, the Edison Historic District was designated significant in the National Register of Historic Places for its contribution to architecture and engineering from 1875 through 1924. The District consists of 3.42 acres and includes eight contributing buildings located on the 500 block of West Second Street and two buildings on the 600 block of West Second Street. Edison Historic District is not listed on Pomona's Historic Register.

The Arts Colony and Antique Row run along Second Street, between Gordon and Gibbs Streets. This area is characterized by one-, two-, and three-story buildings presenting a fairly continuous scale. More than any other area in Downtown, this area maintains a feeling of a small rural California downtown Main Street with a few remaining buildings constructed of brick. Although individually none of the buildings are of significant architectural merit, the overall ambiance of the area has its own charm and should be maintained. Ideally, new buildings located within the area would complement the existing scale and character. Buildings in this area should be remodeled within the original style of each individual

building. These styles run from turn- of-the-century commercial buildings to streamline modern and 1950's styles. The small-town quality should be maintained and enhanced with additional street furniture and embellishments, such as colorful banners and flowers.

Thomas Street also contains a wide-variety of buildings including the Masonic Temple, a fine neo-classical building in brick, as well as the small, adjacent commercial structure, an example of a turn-of-the century commercial building. Thomas Street also contains excellent examples of Art Deco and Stream-Line Modern architectural styles.

Where new developments are proposed adjacent to existing buildings with unique architectural character and style(s), new development should take cues from the existing built environmental context including the definition of a clear architectural style, detailing appropriate to the chosen style, appropriate scale in relation to the existing structures, and pedestrian-orientation along Second and Thomas Streets.



Mayfair Hotel



Masonic Temple

Arts and Culture

The General Plan acknowledges the important role public art plays in relaying the story and identity of a city, district or neighborhood. Pomona has a rich and diverse culture and a variety of historic and cultural districts. Public art venues and forums create the opportunity for residents and visitors to participate in developing and sharing the City's culture and identity. The Arts Colony, originally established along Second Street, reflects the City's support and focus upon the arts and cultural expression, with its many galleries, performance venues and public murals. The Arts Colony has already moved beyond Second Street, and is envisioned to include the entirety of the Downtown Pomona Specific Plan boundary, as the arts are fundamental to Downtown's identity.

The General Plan and Specific Plan seek to further the growth of cultural and artistic awareness in the City by emphasizing public art along major transportation corridors and entryways into the City as well as within Downtown and neighborhood centers.

The enhancement of City streets, gateways and parks with public art is coupled with support for public murals and art installations throughout the City.

The Specific Plan supports integration of public art in new development and art and culture retail and service uses. It promotes the Downtown Pomona Arts Colony mission "...to make the arts inclusive and accessible, grow arts venues and organizations, nurture arts-friendly businesses, and celebrate emergent and established artists...bring communities together and revitalize the downtown with public art and festivals, exhibitions and performances, cuisine and entertainment..."



Antique Row

1.2 Community Engagement

Community engagement and analysis of opportunities and constraints informed development of key planning principles and provided the foundation for the Specific Plan.

1.2.1 Meetings and Workshops

The Specific Plan document is based on community input resulting from two community meetings, interviews with key stakeholders, Historical Preservation Commission and Vehicle Parking District Ad-Hoc Committee meetings, and the public hearing process with the Planning Commission and City Council. A brief summary of community engagement events and feedback themes is provided below.

- Key Stakeholder Meetings January 2017
- Community Workshop March 2017
- Community Open House May 2019

Key themes of public comments received include:

- Economic revitalization and business friendly approaches
- · Access, mobility and connectivity
- · Cleaner, safer and more welcoming environment
- Address undesirable uses and homelessness
- Community character and beautification
- · Mix of commercial, residential and institutional uses

- · Importance of arts and education
- Family-orientation and need to accommodate all ages
- Protect historic resources
- Streamlined processes and clear policies





Community Open House

1.2.2 Planning Principles

Crafted from community input, the following planning principles shape the guidelines and standards contained in the Specific Plan.

- 1. **Heart of Pomona** Reinforce Downtown as the historic heart of the community and enhance its unique identity.
- 2. Celebrate Arts and Culture Continue and expand arts, culture and entertainment opportunities.
- 3. Historic Heritage Recognize, conserve and leverage Pomona's historic heritage and resources.
- 4. Inviting and Safe Foster a safe and clean downtown for residents, visitors and workers.
- 5. Expanded Mobility Improve and expand the mobility network that includes public transit, bicycle facilities, pedestrian corridors and links, as well as vehicle circulation and parking strategies.
- **6. Economic Revitalization** Support business owners and attract key employers to invest and thrive through education, resources, and programs.
- 7. Refresh and Maintain Encourage and facilitate resources for the upkeep and enhancement of properties.
- **8. Streetscape Beautification** Catalyze the beautification of streets and public spaces with trees, landscaping and furnishings.
- **9.** Residential Opportunities Create more places to live that offer a variety of options and inviting neighborhoods.
- **10.** *Implementation* Provide easy-to-use policies that are compatible with other documents and programs.





Downtown Pomona Streetscape

1.3 Document Organization

The Downtown Pomona Specific Plan is intended to be used by property owners, design professionals, developers, City staff, and review bodies to provide a clear set of policies for uses and development in the area over time. The Specific Plan consists of four chapters, briefly summarized below.

- *Chapter 1 Introduction* Provides an overview and context, describes the area affected, summarizes community engagement and vision, describes the relationship to other planning documents, and provides information on the administration of the specific plan.
- Chapter 2 Private Realm Provides the standards for form and property use, included permitted and prohibited land uses, areawide and specific standards, and design guidelines describing how the area's downtown, walkable character will be maintained and improved through architectural features and building and site standards.
- *Chapter 3 Public Realm* Describes plans for improvements to the area's network of streets, bicycles routes, sidewalks, parking and mobility, describes placemaking efforts, and provides information on infrastructure and utilities within the specific plan area.
- Chapter 4 Implementation Provides strategies and an action plan with which to implement the concepts described within the Specific Plan, as well as a guide to funding and financing sources.
- *Appendix* Provides background information such as a definition of terms.

1.4 Relationship to Other Planning Documents

The Specific Plan carries out and is influenced by a number of additional existing planning documents. This section briefly discusses key documents and policies considered in the formulation of the Specific Plan.

General Plan

Updated in 2014, the Pomona General Plan is the City's governance document for guiding decision making and outlines the City's visions and policies. The Downtown Pomona Specific Plan is consistent with the General Plan and provides for more precise implementation of the goals, objectives, and policies outlined within the General Plan.

Zoning Code

The City of Pomona Zoning Code is the primary regulatory document that implements the General Plan. The Zoning Code provides specific development regulations that are applicable to individual neighborhoods, districts and corridors that are consistent with the General Plan. In a case where the Specific Plan development standards and the City's Zoning Code should conflict, then the provisions and standards set forth in the Specific Plan shall prevail unless otherwise stated.

Corridors Specific Plan

Adopted concurrently with the Pomona General Plan update in March 2014, the Pomona Corridors Specific Plan provides regulations for public and private development and activities along key corridors including: Garey Avenue, Holt Avenue, and Foothill Boulevard. The Pomona Corridors Specific Plan boundary and regulations overlap with the Downtown Pomona Specific Plan, necessitating reconciliation of the boundaries and development of complementary development standards as part of the Downtown Pomona Specific Plan update.

Active Transportation Plan

Adopted in 2012, the Pomona Active Transportation Plan (ATP) provides recommendations for improving a system of bikeways connecting neighborhoods to key activity centers throughout the City, developing support facilities, such as bike parking, and education programs, and improving bicyclist safety. This document satisfies the requirements of the California Bicycle Transportation Act, granting the City eligibility for Bicycle Transportation Account (BTA) funding through the California Department of Transportation (Caltrans) improvements identified in this plan. The ATP also contains detailed recommendations for improving pedestrian safety and comfort at intersections and for prioritizing the installation of sidewalks where they do not currently exist.

Western University Master Plan

Situated in the southeastern portion of the Specific Plan, Western University of Health Sciences has developed a master plan. Western University is a private graduate school for health professionals and the Pomona campus consists of a renovated outdoor shopping center and nearby buildings. The Master Plan will address physical solutions to the future needs of the students, faculty and practitioners on campus. While not a City of Pomona document, the Western University Master Plan was considered in developing a collective vision for Downtown Pomona.



Western University

1.5 Administration

Authority and Procedure

The Specific Plan is established by the Pomona City Council in accordance with Section .584 of the Pomona Zoning Code, which establishes Specific Plans as an authorized mechanism for regulating land use and development in the City, as enabled by the State of California Government Code Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457. All property within the Specific Plan area is subject to the development regulations of this Specific Plan. Where the Specific Plan is silent on a topic, the Zoning Ordinance will remain in force.

A specific plan may be adopted either by ordinance or by resolution (Government Code Section 65507). Should the legislative body wish to change a proposed specific plan recommended by the Planning Commission, the change must first be referred back to the Commission for consideration (Government Code Section 65504).

Specific Plan Amendments

Specific Plan amendments or modifications shall be made as described in Section .584 of the Pomona Zoning Code.

Interpretation

This section provides the rules for resolving questions about the meaning or applicability of any part of this Specific Plan and the General Plan. The following rules are established for interpretation.

- **A. Authority** The Development Services Director shall have the responsibility and authority to interpret the meaning and applicability of all provisions and requirements of this Specific Plan.
- **B. State Law Requirements** Where the Specific Plan references applicable provisions of State law (such as the California Government Code, Subdivision Map Act, Public Resources Code), the reference shall be construed to be to the applicable State law provision as they may be amended from time to time.
- **C. Conflicting Requirements** Any conflicts between the different requirements of this Specific Plan, or between this Specific Plan and other regulations, shall be resolved as follows:
 - **1. Specific Plan Provisions** In the event of any conflict among the provisions of this Specific Plan, the most restrictive requirement shall control.
 - **2. Specific Plans** In the event of any conflict between the requirements of the Zoning Code and standards adopted as part of this Specific Plan, the requirements of this Specific Plan shall control.

- **3.** City Code Provisions In the event of a conflict between requirements of this Specific Plan and other regulations of the City, the most restrictive shall control.
- 4. Private Agreements It is not intended that the requirements of this Specific Plan shall interfere with, repeal, abrogate or annul any easement, covenant, or other agreement that existed when this Specific Plan became effective. This Specific Plan applies to all land use and development regardless of whether it imposes a greater or lesser restriction on the development or use of structures or land than an applicable private agreement or restriction, without affecting the applicability or any agreement or restriction. The City shall not enforce any private covenant or agreement unless it is a party to the covenant or agreement.
- **D. Land Use District Boundaries** If there is uncertainty about the location of any land use district boundary shown on Figure 2-1, Specific Plan Districts, the following rules are to be used in resolving the uncertainty:
 - 1. Where the proposed zoning map shows any lot or area within a particular land use district, the zoning district shall extend to the centerline of any adjacent road, street, parkway or highway.
 - 2. If a district boundary divides a parcel and the boundary line location is not specified by distance printed on the zoning map, the Development Services Director shall determine the location of the boundary during project review by using the scale appearing on the zoning map.

3. Where a public street, alley, railroad, or other public right-of-way is officially vacated or abandoned, property that was within the former right-of-way shall be deemed to be included within the zoning district applicable to the property to which the abandoned right-of-way is being attached.

Severability

If any section, subsection, sentence, clause, phrase, or portion of this document, or any future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan document, or any future amendments or additions hereto. The City hereby declares that it would have adopted this document and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.

1.6 Split Parcel

A development site that spans multiple districts shall be subject to the Substantial Conformance process as defined in Section 1.7, which shall establish the development standards for the site in a manner most consistent with the underlying districts' standards. Any use that is permitted or conditionally-permitted over a portion of such a site shall be permitted or conditionally permitted anywhere on the site.

1.7 Substantial Conformance

Purpose

The purpose of the Substantial Conformance process is to determine whether the proposed development or use substantially complies with the standards, regulations, and guidelines of the Downtown Pomona Specific Plan and to maintain a degree of flexibility with respect to the details of the development approved within the Specific Plan area. Use of the Substantial Conformance process includes, but is not limited to the following purposes:

- 1. Determinations regarding issues, conditions, or situations that arise that are not addressed by the Downtown Pomona Specific Plan.
- 2. Determinations as to whether a use not listed in the Downtown Specific Plan is similar to the uses, including temporary uses, listed in Private Realm—Land Use.

- 3. Approvals of modifications to the Urban Form standards and guidelines in Private Realm—Urban Form.
- 4. Approval of signs in Substantial Conformance with the sign provisions described in Private Realm—Signage.
- 5. Additions, deletions, and changes to the Specific Plan exhibits or text.
- 6. Determinations on applicable standards for split parcels.

Authority

The Development Services Director shall review a request for a determination of Substantial Conformance and have the discretion to approve, with or without conditions; deny or refer the request to the Planning Commission and/or City Council for action in a noticed public hearing. Additional environmental review and/or analysis will be conducted to determine the potential impact of the request, if necessary under the California Environmental Quality Act (CEQA). Determinations of Substantial Conformance shall be provided to the applicant in writing and be based on findings that the request:

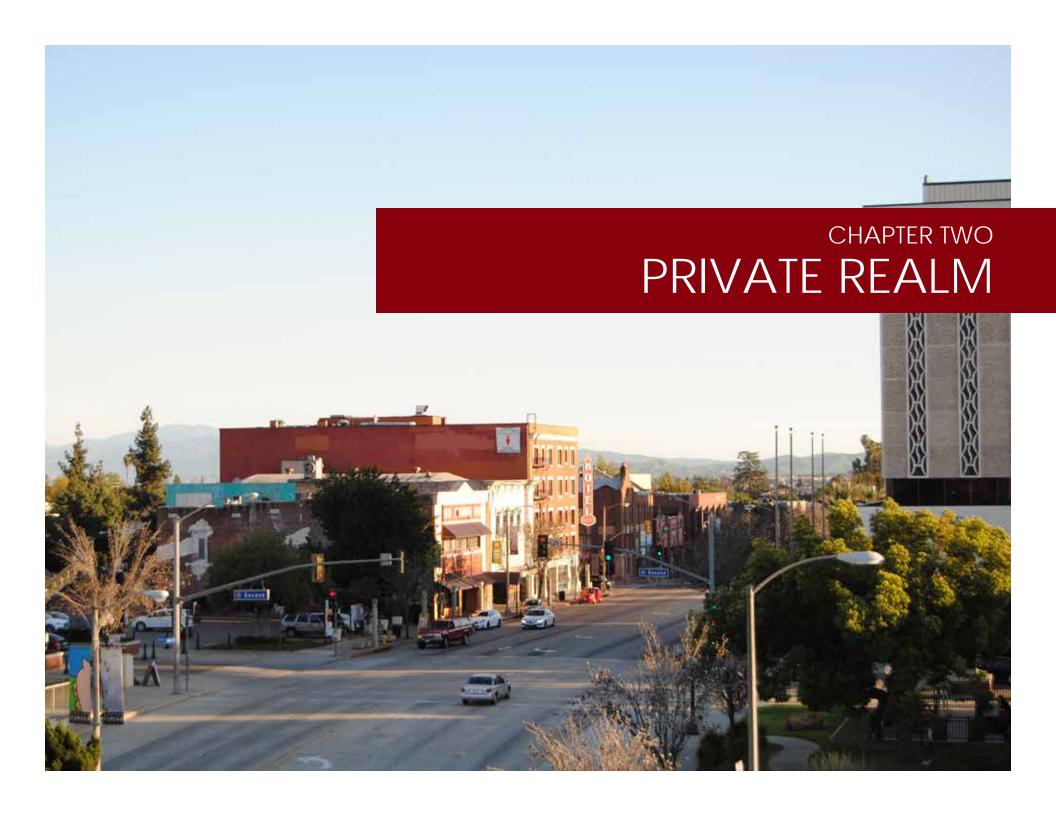
- 1. Substantially complies with the Downtown Pomona Specific Plan.
- 2. Will not adversely affect public health and Safety.
- 3. Will not adversely affect adjacent properties.

Application

Requests for a determination of Substantial Conformance shall be submitted on forms and with information as required by the Development Services Director.

The applicant or members of the City Council may appeal decisions of the Development Services Director, or his or her designee, regarding Substantial Conformance to the Planning Commission within 15 working days of receipt of the Development Services Director, or his or her designee's written decision. Decisions of the Planning Commission may be appealed within 15 working days of receipt of the Planning Commission's written decision to the City Council and payment of a fee consistent with the current fee resolution of the City Council.

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2.1 Introduction

The Private Realm chapter provides direction for development of privately-owned areas within the Specific Plan to shape the design character and built environment. It sets forth general provisions for urban form, development standards, design guidelines and details permitted land uses. The provisions contained within this chapter apply to new construction, building additions, exterior remodels, relocations, and other modifications requiring a building permit. As stated in Chapter 1, Introduction, this Specific Plan supersedes the Zoning Code within the Specific Plan boundary. Where the Specific Plan is silent on a topic, the Zoning Code will remain in force.

2.1.1 How to Use This Chapter

This chapter sets the regulatory and design framework for developers, designers, city staff, and review bodies to develop, critique, and implement projects as they proceed through the entitlement process. It is divided into three sections; Urban Form, Land Use, and Private Signage. These sections are further categorized as follows:

Area Wide Urban Form Standards - Section 2.2.3 includes standards/regulations that apply to the entire Specific Plan area. Applicants must review and incorporate all applicable requirements. Reviewers shall ensure consistency between the project and the regulations/standards as applicable.



Pomona Holy Church



Urban Form Design Guidelines - Designers should comply with the intent of the guidelines that have been provided in Section 2.2.4 of this document. Reviewers should ensure compliance with the design guidelines.

District-Specific Urban Form Standards - Section 2.2.5 include tailored standards/regulations that serve as the zoning regulations for that sub-area. Applicants must comply with all applicable regulations. Reviewers shall ensure consistency between the project and the regulations/standards as applicable.

Land Uses - Applicants should review the list of permitted uses within Section 2.3 to ensure the proposed use is allowed.

Private Signage - Section 2.4 includes tailored standards/ regulations regulating private signage within the Specific Plan area. Applicants must comply with all applicable regulations. Reviewers shall ensure consistency between the project and the regulations/ standards as applicable.

2.2 Urban Form

The future urban form of Downtown Pomona will be established by providing opportunities for development that adheres to the following customized context-sensitive development standards and placemaking design guidelines. Projects shall comply with the urban form standards contained within this section and the intent of the guidelines.

Standards. Standards are specifications that the community considers essential to the creation and preservation of a high quality, sustainable and coherent city. Conformance with Standards is mandatory. Such provisions are indicated by the use of the words "shall," "must," "is required," and "is/is not permitted.

Guidelines. Guidelines provide additional information to assist the designers with fulfilling the intent of the Specific Plan. Guidelines pertain to issues of visual character and aesthetics. Conformance with Guidelines is recommended, especially to ensure the swiftest possible approval. Although conformance with Guidelines is recommended, developers are permitted to propose alternative design solutions if they are able to show that such design solutions meet the overall objectives of the Specific Plan. Guidelines are indicated by the use of the words "should," "may," or "is/are encouraged."

2.2.1 Applicability

Compliance with all Urban Form standards and guidelines may require review and approval of a Development Plan Review. The following thresholds require review and approval of a Development Plan Review:

- 1. New construction of a primary structure on a vacant or developed lot.
- 2. New construction of accessory structures that are greater than 25 percent of the gross floor area of the primary structure.
- 3. Additions to a primary structure that are greater than 25 percent of the gross floor area of the primary structure.
- 4. Reconstruction of a primary structure that will either a) change the façade of the existing primary structure, or b) exceed 25 percent of the gross floor area of the existing primary structure.

If a proposed project does not meet any of these four thresholds, it shall be subject to a Planning ministerial review as part of a plan check submittal to the Building and Safety Division. All nonconforming buildings and structures are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions. Applicability thresholds shall not apply to legally non-conforming single-unit residential, which shall be subject to R-1-10,000 standards.

2.2.2 Review and Approval Process

- 1. The Director of Development Services has the authority to approve, conditionally approve, or deny a Development Plan Review. To approve a Development Plan Review, the Director of Development Services shall make the following four findings:
 - a. The project is consistent with the City of Pomona General Plan and all applicable requirements of the City of Pomona Municipal Code; and
 - The project will not be detrimental to the general welfare of persons working or residing in the vicinity nor detrimental to the value of the property and improvements in the neighborhood; and
 - c. The project will not adversely affect the policies contained in the Public Realm chapter of the Downtown Pomona Specific Plan; and
 - d. The project complies with the applicable regulations contained in the Private Realm chapter of the Downtown Pomona Specific Plan and all other applicable Federal, State, and local regulations.
- 2. All Development Plan Reviews require a public hearing before the Director of Development Services. Noticing shall be given to the general public by the applicant at least 14 calendar days prior to the day of the scheduled hearing, to be sent by physical mail to owners and occupants of

- property located within a 400-foot radius of the project site boundaries. The City of Pomona may also voluntarily notice the public hearing through any other manner necessary.
- 3. The Director of Development Services has the authority to approve or deny a CEQA exemption, Negative Declaration, or Mitigated Negative Declaration, and to certify an Environmental Impact Report, in association with the Development Plan Review.
- 4. The action taken by the Development Services Director on a Development Plan Review at a public hearing is final unless appealed to the Planning Commission by either the applicant or a member of the Planning Commission within 20 calendar days of the action.
- 5. If a Development Plan Review is submitted in conjunction with another discretionary application, it shall be subject to review and approval by the highest applicable approving body as one item.

An approved Development Plan Review shall be valid for a period of 12 months. A maximum of two 12-month time extensions may be requested from the Director of Development Services. Such requests shall be submitted in writing by the applicant, property owner(s), and/or authorized designee at a minimum of 30 days prior to the expiration of the Development Plan Review. If the applicant has not obtained grading or building permits or consistently pursued necessary permits for construction approved under the Development Review within the approved or extended time period, the Development Plan Review shall be terminated.

2.2.3 Areawide Urban Form Standards

The following form standards apply to all districts within the Specific Plan area.

1. Site Planning

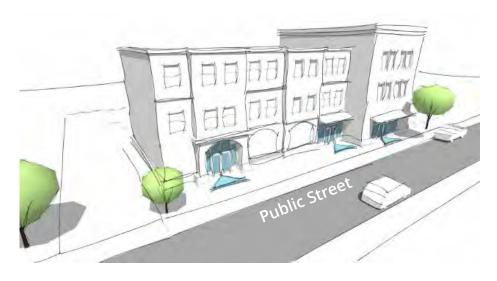
Site planning is key to quality development. The following standards promote orienting buildings toward the street, providing access for both vehicles and pedestrians, and facilitating internal pedestrian circulation.

A. Building Articulation

1. Any building over 125 feet wide shall be broken down to read as a series of buildings no wider than 75 feet each.

B. Building Orientation

- 1. Buildings shall be oriented and contain an entrance parallel to the primary street frontage.
- 2. Buildings shall be designed and oriented to minimize the visual intrusion into adjoining residential properties. Window, balcony and deck locations shall be directed away from window and private areas of adjoining residences (onsite or off-site).



Building Orientation

C. Pedestrian Access

On-site pedestrian circulation and access shall be provided according to the following standards:

- 1. Internal Connections. A system of pedestrian walkways shall connect all buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas or pedestrian amenities.
- 2. To Circulation Network. Regular connections between onsite walkways and the public sidewalk shall be provided.
 - A. An on-site walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage.
 - B. Walkways shall be the shortest practical distance between the main entry and sidewalk, generally no more than 125 percent of the straight-line distance.
 - C. Where a required walkway crosses a driveway, parking area, or loading area, it must be clearly identifiable through the use of a raised crosswalk, a different paving material, or similar method.
 - D. Where a required walkway is parallel and adjacent to an auto travel lane, it must be raised or separated from the auto travel lane by a raised curb at least four inches (4") high, bollards or other physical barrier.

- To Neighbors. Direct and convenient access shall be provided from commercial and mixed-use projects to adjoining residential and commercial areas to the maximum extent feasible while still providing for safety and security.
- 4. To Transit. Safe and convenient pedestrian connections shall be provided from transit stops to building entrances. Sidewalk "bulb-outs" or bus "pull-outs" may be required at potential bus stops serving commercial centers (building floor area over 25,000 square feet) to provide adequate waiting areas for transit users and safety for passing motorists.

E. Limitations on Curb Cuts

Curb cuts shall be minimized and located in the location least likely to impede pedestrian circulation. Curb cuts shall be located at least 10 feet from an intersection curb return or pedestrian crosswalk.

F. Landscaping

Landscaping is subject to the requirements of Section .503-J-Landscaping of the Pomona Zoning Ordinance, except for percentage minimums for parcels on residential and non-residential landscape standards.

3. Parking

The following provisions apply to parking within the Downtown Pomona Specific Plan:

A. Limitations on Location of Parking.

- 1. Off-street parking spaces shall be located to the rear of principal buildings whenever possible. Above ground parking may not be located within 20 feet of a street-facing property line. Exceptions may be granted where the review authority makes the following findings:
 - a. The design incorporates habitable space built close to the public sidewalk to the maximum extent feasible; and
 - b. The site is small and constrained such that underground parking or surface parking located more than 40 feet from the street frontage is not feasible.

B. Parking Regulations.

- 1. Parking requirements shall be calculated on the total square footage for each primary use on a site. Total square footage includes any square footage where a primary use occurs, excluding incidental spaces such as storage rooms, restrooms, kitchens, and other secondary spaces that support a primary use. Determinations on which spaces can be considered secondary shall be made by the Development Services Director.
- 2. Parking requirements do not apply to the first two floors of any proposed development within the MU-CBD or MU-I districts.

 Moreover, the first two floors of any existing development within said boundaries are also not subject to parking requirements.
- 3. Parking shall be located on site or provided within 1,000 feet of proposed development. However, at least 50% of total required parking shall be provided on-site. Moreover, the use of off-site parking as part of a proposed development shall require conditions of approval to ensure the ability to use such parking for an extended period of time.
- 4. The maximum number of parking spaces permitted does not apply to parking structures.

- Parking structures adjacent to residential uses shall incorporate design features to minimize light or sound impacts.
- 6. Light fixtures within parking strucutres shall be designed to minimize off-site spill lighting.

C. Parking Relief

 Provide a parking demand study, conducted by a registered traffic engineer, to substantiate a reduction in total parking requirements; studies are subject to review and approval by the Development Services Director.

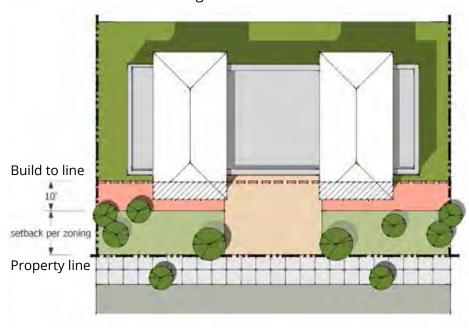
E. Parking Ratios

- 1. Residential Uses (per unit)
 - a. Live/Work: .5 min / .5max
 - b. Studio: .5 min / .5 max
 - c. One bedroom: 1 min / 1 max
 - d. Two bedroom: 1.5 min / 1.5 max
 - e. Additional bedrooms: .5/room
- 2. Public/Semi-Public Uses
 - a. One per 35 square feet of assembly area
- 3. Commercial (per 1,000 total square feet)
 - a. Retail: 2.5 min / 3.5 max
 - b. Eating & Drinking: 3 min / 8 max
 - c. Entertainment & Recreation: 5 min / 8 max
 - d. Office: 2.5 min / 3.5 max
 - e. Medical Office: 4 min / 5 max
- 4. Lodging (per guest room)
 - a. Hotel: 1 min / 1.2 max

4. Required Building Location

The following building location requirements apply in MU-CBD, HU-HDR, and MU-I districts:

A. Build-to Line. Buildings shall be located within 10 feet of the required setback line for at least 40 percent of the linear street frontage.



Build-to Line

- **B. Corner Build Area.** Buildings shall be located within 10 feet of the required setback line within 30 feet of the street corner.
- **C. Frontage Improvements.** The area between buildings and the property line shall be improved as part of a wider sidewalk, outdoor dining/seating area, or with landscaping. Paving shall be minimized.
- **D. Exceptions.** These requirements may be modified or waived by the review authority upon finding that:
 - 1. Entry courtyards, plazas, entries, or outdoor eating areas are located adjacent to the property line and buildings are built to the edge of the courtyard, plaza, or dining area; or
 - 2. The building incorporates an alternative entrance design that creates a welcoming entry feature facing the street.



5. Building Transparency/Required Openings.

The following building transparency and opening requirements apply in MU-CBD, MU-I, and MU-HDR districts:

Exterior walls facing and within 20 feet of a front or street side lot line or pedestrian walkway shall include windows, doors, or other openings for at least 50 percent of the building wall area located between 2.5 and 7 feet above the level of the sidewalk. Such walls may run in a continuous plane for no more than 30 feet without an opening.

A. Design of Required Openings. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.

- **B. Exceptions for Parking Garages.** Multi-level garages are not required to meet the ground-story transparency requirement.
- **C. Reductions.** The building transparency requirement may be reduced or waived by the review authority upon finding that:
 - 1. The proposed use has unique operational characteristics with which providing the required windows and openings is incompatible, such as in the case of a cinema or theater; and
 - 2. Street-facing building walls will exhibit architectural relief and detail and will be enhanced with landscaping in such a way as to create visual interest at the pedestrian level.



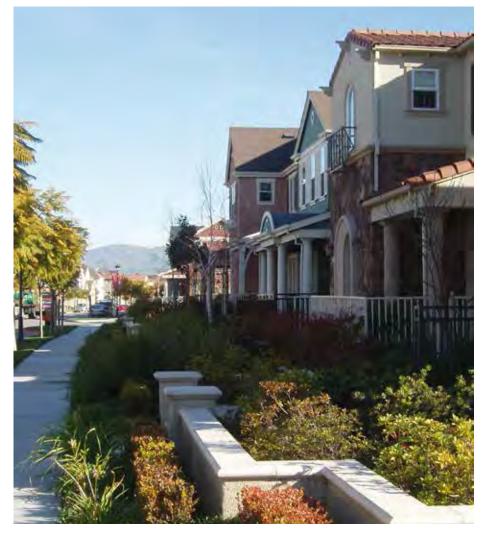
Building transparency/required openings

2.2.4 Areawide Urban Form Guidelines

The following urban form design guidelines apply to all districts within the Specific Plan area. The City should consider implementing Architectural Design Incentives for the Downtown Pomona Specific Plan to promote and preserve the historical integrity of existing visible streetscape.

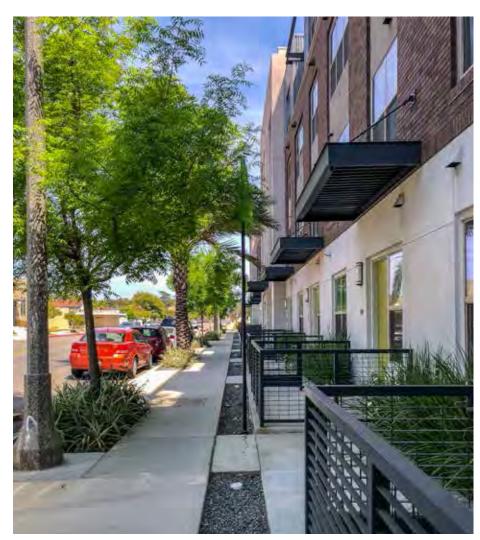
1. General Guidelines

- A. The following general urban form guidelines are desirable, and should be incorporated into all project designs where most applicable:
 - 1. Significant wall articulation.
 - 2. Variation in building height on larger buildings to give appearance of distinct massing elements.
 - 3. Multi-pitched roofs.
 - 4. Durable building materials and finishes.
 - 5. Richness of surface and texture.
 - 6. Awnings and roof overhangs.
 - 7. Regular or traditional window rhythm.
 - 8. Easily identifiable entries.



Distinct massing elements



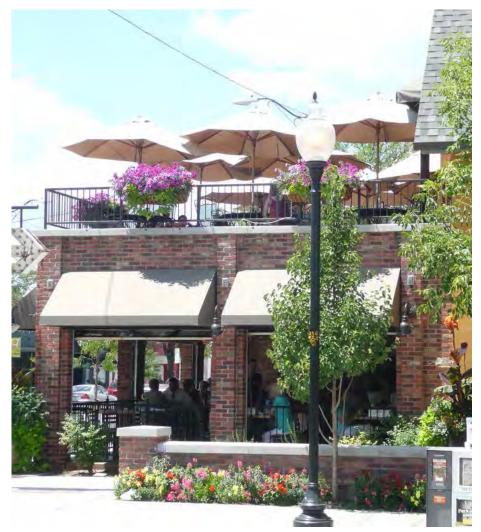


Buildings oriented towards street

- 9. Significant, sustainable, drought-tolerant landscape design elements arranged in a thoughtful, comprehensive manner.
- 10. Enhanced paving and other elements giving attention to pedestrian scale.
- 11. Adequate landscape and screening for parking areas.
- B. The following design elements are not desirable and should be avoided:
 - 1. Large, blank, unarticulated wall surfaces.
 - 2. Visible outdoor storage, loading and equipment areas.
 - 3. Exterior security bars, gates, and grills are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
 - 4. Chain link fencing is prohibited.
- C. The following design elements are encouraged only if incorporated into an architecturally interesting or artistic, unique development:
 - 1. Unpainted, plain color concrete block walls.
 - Highly reflective surfaces.
 - 3. Metal Siding.

2. Site Planning Guidelines

- A. Buildings should be oriented toward streets, plazas and open spaces to embrace intersections and surrounding neighborhoods.
- B. Buildings should be placed at the setback line(s) to define and enliven the street edge and maximize access from the public sidewalk.
- C. Buffer residential uses from incompatible commercial and office development by providing intensified landscaping, increased setbacks, appropriate building orientation, or combination thereof, where mixed use development abuts a residential use.
- D. Orient commercial and office buildings, wherever feasible, to minimize shading of adjacent residences and impairing privacy.
- E. Outdoor dining and cafes are encouraged, particularly along Second and Third Streets.
- F. Units and buildings should be sited with view opportunities to open space as much as possible.
- G. Buildings should be sited in a manner that will be compatible with other buildings in the vicinity. Long rows of garages that create a monotonous streetscape should be avoided.



Outdoor dining





Streeet parking along Garey Avenue

- H. Private open space should be provided adjacent to each unit, at least partially screened from adjacent common areas, to the extent feasible.
- I. Create public spaces and plazas for social gathering and activities.
- J. Provide open space and recreation areas.
- K. Buildings with ground level restaurants should have spaces for outdoor patios and sidewalk dining.
- L. Entry nodes should include low walls with signage, identification signage or monument signage, where adequate space allows for their construction.

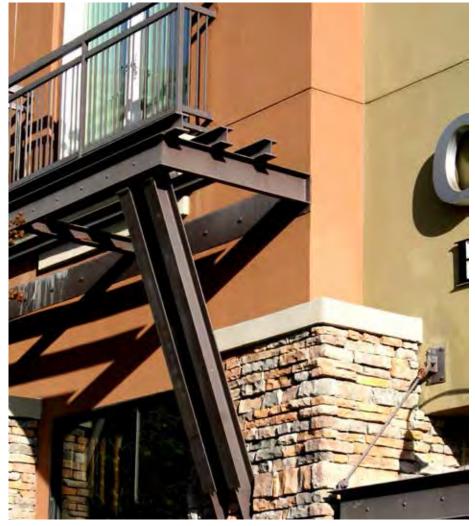
3. Parking Guidelines

- A. Parking should not dominate street frontages and should be provided within commercial, office, and residential structures.
- B. Parking structure facades should be articulated in similar character as surrounding uses.
- C. Parking for new development should either be underground or hidden in the interior of the block, with retail, or office space on the ground level facing the sidewalks.

- D. All parking areas visible from the public right-of-way should be screened by hedges, shrubs or other plant materials. Large expanses of parking should be broken up by planting trees, particularly at the ends of and along the length of aisles.
- E. Parking lot landscaping should accent driveways, frame the major circulation aisles, and highlight pedestrian pathways.

4. Architectural Guidelines

- A. Attached residential units should include design elements to add visual interest and to avoid "box-like" appearances. Elements such as balconies, porches, arcades, dormers, and cross gables should be considered.
- B. Wall and roof planes should be varied and articulated into smaller modules that reduce the overall massing and scale of the building and add richness and variety. Long, unarticulated building facades are discouraged.
- C. Renovated buildings should retain historic architectural features of existing structures. New construction should make a strong new architectural contribution.
- D. Balconies, porches and projecting windows are encouraged to help contribute to eyes on the street.
- E. Primary building materials should include brick, brick façade, stucco, stone, stone veneer or other similar materials.



Varying building textures



Art installation

- F. Varying building textures are encouraged.
- G. Signage should be integrated into architectural design.
- H. Awnings and canopies are encouraged.
- I. Orient windows in non-residential structures to avoid direct line of sight into adjacent residential uses.
- J. All rooftop and mechanical equipment should be screened from public and pedestrian view using parapets or other architecturally acceptable methods.
- K. Trellises, pergolas, gazebos, patios/courtyards and other outdoor structures are encouraged.
- L. To the extent feasible, avoid windows facing directly onto a neighboring window or private outdoor space.
- M. Exterior window bars, grills and gates are prohibited, unless required by the Police Department for safety purposes. If bars are required, they must be designed to be architecturally complementary to the building design.
- N. Street trees, landscaping, street furniture and architecture should contribute to the human scale.
- O. Provide art in public gathering areas for visual appeal and placemaking.
- P. Provide lighting to create safe environments.
- Q. Special consideration should be given to lighting of entry areas, courtyards, and other pedestrian gathering places.

5. Landscaping Guidelines

- A. Landscape treatments should incorporate native colorful plant material and evergreen trees.
- B. The use of trees to define and enclose exterior space, and to provide physical protection from the sun and wind, is encouraged.
- C. Provide for landscape open space areas that enhance the building design and public views, provide buffers/ screening and transitions between adjacent uses.
- D. Screen undeveloped, vacant land intended for future phases with plants that prevent wind and water erosion.
- E. Existing healthy mature trees should be preserved and incorporated into the overall landscaping design, where appropriate.
- F. Use of vines and climbing plants on buildings, trellises and privately-owned perimeter walls is encouraged.
- G. Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.
- H. Minimize the use of water through the selection of plants that are drought-tolerant when they are mature.



Use of vines and potted landscaping



Drought-tolerant planting



2.2.5 Districts

This Specific Plan establishes a district-based zoning approach with each district guided by a tailored set of form standards and allowable uses that support the desired future desired character and user experience. This hybrid approach combines form-based development standards with a selection of compatible uses that have been tailored for each Specific Plan District resulting in a mix of uses, whereby the desired building forms and uses dictate what is permitted, conditionally permitted, or not permitted. The form standards regulating each district constitute the constraints for a project's building envelope in which new construction or a remodel is permitted. In addition, design guidelines are provided to ensure high-quality design that reflects the area's envisioned character. Form standards and design guidelines are captured within this section, Urban Form, and allowed uses are located in the second part of this chapter, Land Use.

Individual district boundaries, **Figure 2-1 Districts Map**, within the Specific Plan have been organized by land use and zoning regulations and have been updated to reflect and reconcile the Specific Plan relationship to the Pomona Corridors Specific Plan boundaries.

The following districts have been established:

- Mixed Use Central Business District (MU-CBD)
- Mixed Use Institutional (MU-I)
- Mixed Use High Density Residential (MU-HDR)
- Residential Multifamily (R-MF)

For each district, the following sets of policies are provided:

- Vision and Character
- Purpose and Form Standards



Figure 2-1, Districts Map



Cafe in the Central Business District

1. Mixed Use – Central Business District (MU-CBD)

A. Vision and Character

The Mixed Use-Central Business District is envisioned to allow for either horizontal or vertical mixed-use development to support a vibrant, pedestrian-friendly urban environment providing a variety of employment, entertainment, cultural, and shopping opportunities for both residents and non-residents of Pomona. An influx of daily transit users and weekend visitors entering and exiting the adjacent Amtrak/Metrolink Station are envisioned to be greeted by inviting plazas and informative wayfinding signage that connect to the Central Business District through enhanced pedestrian linkages. Home to the expanding arts colony, the MU-CBD district is also intended to support arts-related uses.

New, mixed use developments reflecting the character of Pomona will cater to the pedestrian by incorporating active and articulated front facades. Cafes with outdoor dining, coffeehouses, pubs, and retail shops will be located at street-level, enlivening the district environment and providing commuters and residents access to early morning coffee and after work dining opportunities. Structured parking, located above ground and hidden from public view, will provide commuters a safe place to park their vehicles with convenient access to the nearby Amtrak/Metrolink Station.

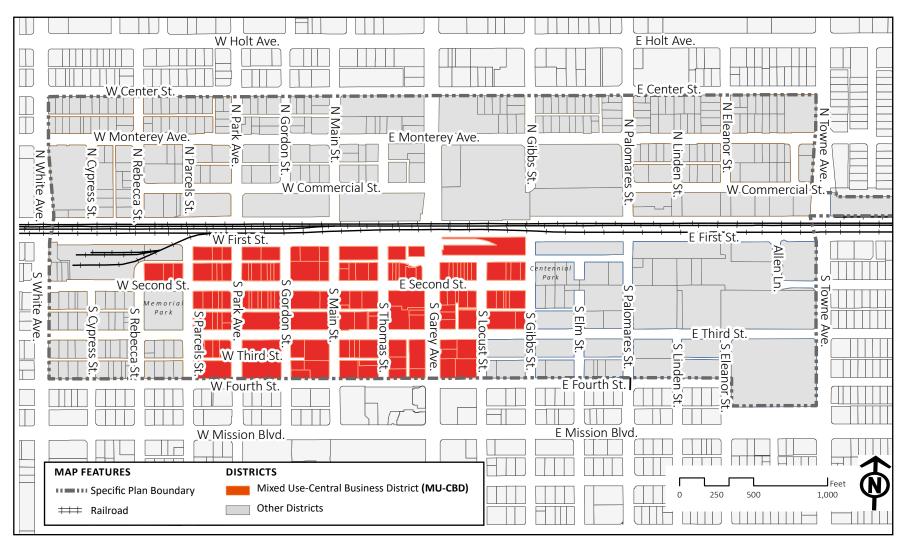


Figure 2-2, Mixed Use Central Business District (MU-CBD) Map

B. Purpose and Form Standards

1. Purpose

Projects located within the MU-CBD should strive to accomplish the following objectives:

- A. Provide for the orderly, well-planned, and balanced development of commercial and mixed-use districts;
- B. Designate adequate land for a full range of local- and regional-serving economic and cultural resources consistent with the General Plan to maintain and strengthen the city's economic resources;
- C. Provide appropriately located areas for a range of commercial uses that provide for a variety of good and services for residents, employees, and visitors;
- D. Provide opportunities for a mix of complementary uses that may combine residential and nonresidential uses or combine a variety of nonresidential uses on the same site; and
- E. Promote pedestrian-oriented, mixed-use commercial centers at appropriate locations.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-CBD district are contained within Table 2-1.

3. Street Frontage

A minimum six-foot unobstructed walkway shall be provided between a building facade and the street/curb along any street frontage within this district. Sidewalks located within the public right-of-way may be included.

Table 2-1: Mixed-Use Central Business District (MU-CBD) Form Standards			
Standard	Requirement		
Density (units/acre)	East of Park Street: 50 to over 100 maximum		
	West of Park Street: 40 to 70		
Number of Stories	3 minimum to 12 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks of the R-MF District		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential unit)	Total/unit: 150 sf		
	Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		

2. Mixed Use - Institutional (MU-I)

A. Vision and Character

The Mixed Use-Institutional District is intended to serve as the major governmental, educational and office spine for the Downtown while still incorporating mixed use developments with residential and retail uses. The MU-I zone is also intended to allow for higher education development and, in particular, for Western University campus expansion including classrooms and administrative offices, dormitories, other student/faculty housing, ancillary retail/service and office uses, such as a college bookstore, educational materials and supply store, tutorial services, etc. In general, single-user office buildings, and multitenant office buildings are encouraged in this zone with the intent of providing economic and employment base opportunities. It is projected that office and related service/support uses and ancillary development could locate in proximity to the MU-CBD zone and, thus, complement the entertainment commercial development within the "core" area as employees of MU-I zone patronize the entertainment and associated retail uses provided.



Western University





Figure 2-3, Mixed Use Institutional Map

B. Purpose and Form Standards

1. Purpose

The specific purposes of the Mixed Use-Institutional District are to:

- A. Provide areas for educational facilities, cultural and institutional uses, health services, parks and recreation, general government operations, utility and public service needs, and other similar and related supporting uses;
- B. Provide for the orderly, well-planned, and balanced development of institutional, commercial and mixed-use land uses compatible with the surrounding districts including the MU-CBD district;
- C. Provide opportunities for attached and multi-family residential development; and
- D. Promote pedestrian-oriented, mixed-use development at appropriate locations.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-I district are contained within Table 2-2.

Table 2-2, Mixed Use-Institutional District (MU-I) Form Standards			
Standard	Requirement		
Maximum Density (units/acre)	West of Palomares: 40 to 100 East of Palomares: 40 to 80		
Number of Stories	3 minimum to 6 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks of the R-MF District		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential unit)	Total/unit: 150 Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		



Surrounding the activity and intensity of the Downtown core, the Downtown neighborhood will be a neighborhood with a difference. It will feature the City's widest range of contemporary housing types, and a wide mixture of uses that are compatible with the district's housing, all concentrated within walking distance of the Downtown Core's theater, shops, restaurants, cafes, nightlife, and amenities.

-Pomona General Plan

3. Mixed Use-High Density Residential (MU-HDR)

A. Vision and Character

This Mixed Use-High Density Residential District includes residential, retail, service and office uses and is intended to function as a pedestrian precinct complementing the Downtown area. One of the Specific Plan objectives is to provide housing opportunities for households of varying economic means and social needs. To allow for variations in the marketplace, the land use plan has been designed with flexibility allowing shifts in residential densities and housing types, including housing for artists, educators, and work-live units, in addition to assuring a range of choices in price and lifestyle for future residents. This will result in a greater mixing of residents, incomes and lifestyles and a less insular neighborhood identity. With the location of the transit center within the MU-HDR district, this area is envisioned as a pedestrian friendly, high-density commuter residential neighborhood with ancillary retail, service and office uses. Residential products in this area will be a mix of lofts, apartments, town homes and condominiums at a variety of medium to high densities.



Figure 2-4, Mixed Use High Density Residential Map

B. Purpose and Form Standards

1. Purpose

The specific purposes of the Mixed Use - High Density Residential District are to:

- A. Provide for the orderly, well-planned, and balanced development of commercial, residential and mixed-use districts;
- B. Designate adequate land for a full range of localand regional-serving retail and commercial services consistent with the General Plan to maintain and strengthen the city's economic resources;
- Provide appropriately located areas for a range of commercial uses that provide for a variety of good and services for residents, employees, and visitors;
- D. Provide opportunities for a mix of complementary
 uses that may combine residential and
 nonresidential uses or combine a variety of nonresidential uses on the same site; and
- E. Promote pedestrian-oriented, mixed-use commercial centers at appropriate locations.

_	_	- .	
2.	Form	Stan	dards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the MU-HDR district are contained within Table 2-3.

Table 2-3, Mixed Use-High Density Residential Form Standards			
Standard	Requirement		
Density (units/acre)	40 to 100		
Number of Stories	3 minimum to 6 maximum		
Minimum Setbacks, Residential-only Development	Subject to setbacks from R-MF district		
Minimum Setbacks, Nonresidential and Mixed-Use Development (ft)	See subclassifications below		
Front	0		
Interior Side	0		
Street Side	0		
Rear	0		
Open Space (sf/residential unit)	Total/unit: 150 Private: minimum 50 sf for 50% of units Common: minimum 100 sf/unit		

4. Residential Multifamily (R-MF)

A. Vision and Character

The Residential Multifamily District allows multifamily dwellings at densities that range between 70-80 maximum dwelling units per acre. The R-MF District is intended to provide areas for a wide variety of medium to high-density residential development. Housing types emphasize multiple units and include townhouses, condominiums, and apartment buildings. This district also provides for uses such as schools, daycare centers, parks, and community facilities that may be appropriate in a higher-density residential environment.







Figure 2-5, Residential Multifamily Map

B. Purpose and Form Standards

1. Purpose

The purposes of the Residential Multifamily District are to:

- A. Provide for a full range of multi-unit housing types consistent with the General Plan:
- B. Preserve, protect, and enhance the character of the City's different residential neighborhoods and the quality of life of City residents.
- C. Ensure adequate light, air, privacy, and open space for each dwelling.
- D. Ensure that the scale and design of new development and alterations to existing structures are compatible with surrounding homes and appropriate to the physical characteristics of the site and the area where the project is proposed.
- E. Provide sites for public and semi-public land uses such as parks, schools, daycare, and other uses that will serve City residents and will complement surrounding residential development.

2. Form Standards

Form standards provide direction for the physical form of buildings, including specifying allowed density, height, setbacks and open space. Form standards for the R-MF district are contained within Table 2-4.

Table 2-4, Residential Multifamily Form Standards			
Standard	Requirement		
Maximum Density (units/acre)	South of Railroad Corridor: 40 to 70		
	North of Railroad Corridor 40 to 80		
Number of Stories	South of Railroad Corridor: 4 maximum		
	North of Railroad Corridor 6 maximum		
Minimum Setbacks (ft.)	For attached single-unit dwellings, required setbacks apply to the ends of rows of the dwellings		
Front	0		
Interior Side	10		
Street Side	10		
Rear	10		
Minimum Building Separation	See subclassifications below		
Between Residenital Buildings	15		
Between Residential Building and Accessory Building	6		
Between Accessory Building	6		
Open Space (sf/unit)	Total/unit: 150		
	Private: minimum 50 sf for 50% of units		
	Common: minimum 100 sf/unit		

3. Incentives

A. Affordable Artist Work/Live Units

Developers that voluntarily provide affordable artist work/ live units for new development within the Downtown Pomona Specific Plan may take advantage of development concessions that are intended to make such projects more financially viable.

- i. To qualify, the proposed work/live unit must:
 - a. At least 25% of all residential units must meet the standards of the Work/Live ordinance of the City of Pomona, including unit size and operations.
 - Work/Live units must be rented at fair market value, to be recorded against the property through a housing covenant.
- Upon qualifying as an affordable artist work/live unit, the following development concessions shall be granted:
 - a. A 50% reduction in total parking demand for proposed development.
 - b. A 25% reduction in required square footage of common open space for proposed development.

B. Privately Owned Public Open Space (POPOS)

Developers that voluntarily provide privately owned public open space (POPOS) as part of new development within the Downtown Pomona Specific Plan may take advantage of development concessions that will aid the City of Pomona in providing accessible public open space across Downtown Pomona.

To qualify, the proposed POPOS must:

- i. Be accessible to the general public.
 - a. For open plazas that do not require entry through a building, public access shall be 24 hours a day.
 - b. For public open spaces that require entry through a building or gate, public access shall be from sunrise to one hour after sunset.
 - c. For all proposed public open spaces, alternative hours requested by a private property owner are subject to review and approval by the Director of Development Services.
- ii. Follow the design guidelines for POPOS as indicated in 3.2.2—Downtown Placemaking Standards and Guidelines.

Upon qualifying as privately owned public open space, the following development concessions shall be granted:

- i. A 75% reduction in total common space requirements for proposed development.
- ii. POPOS will be considered common open space.

2.3 Land Use

An important component of this Specific Plan process is the provision of a mix of land uses that reflect the intended vision for each District. The uses shown in Table 2-5 as Permitted (P) are acceptable anywhere in the land use designation in which they are located. They will not require special conditions when developed in accordance with this Specific Plan. Several uses are shown as Conditionally Permitted (CUP) as a result of potentially incompatible characteristics. Conditional uses require special consideration so they may be properly located with respect to their effects on surrounding properties. This may be because of their hours of operation or their potential nuisance aspects such as light, odors, glare, or noise. Conditional Use Permits are subject to public review and require a public hearing before the Planning Commission.

2.3.1 Applicability and Thresholds

1. Applicability

Any use to be established on a property located within the Downtown Pomona Specific Plan is subject to the provisions of this section. All nonconforming uses are subject to Section 2.3.4 of this Plan, Non-Conforming Provisions

When a use is not specifically listed in the Land Use Matrix it shall be assumed that such uses are expressly prohibited. Proposed uses that are not explicitly listed in the Land Use Matrix may be permitted through the Substantial Conformance process, see Section 1.7.



2. Building Use Regulations

Table 2-5 specifies the various land uses that are allowed within each of the land use districts of the Downtown Pomona Specific Plan. The letter in the columns beneath the district designations describe the allowed uses category described as follows:

- *P:* The use is permitted by right within the specified district (i.e., no discretionary application required).
- P2: The use is permitted only in upper story levels, lower levels/basement or in the rear of a ground-floor building subject to the following provisions:
 - In no event shall the rear use occupy more than 65% of the groud story area.
- M: The use is only allowed within the specified district with the approval of a Minor Conditional Use Permit consistent with applicable provisions of the DPSP and Pomona Zoning Code.
- CUP: The use is only allowed within the specified district with the approval of a Conditional Use Permit consistent with applicable provisions of the DPSP and Pomona Zoning Code.
- N/A: Not Allowed in this zone district. A symbol of " " also indicates a not-allowed use.

3. Review and Approval Process

A. Minor Conditional Use Permit

- i. Uses subject to a Minor Conditional Use Permit shall be approved, approved with conditions, or disapproved by the Planning Division. To approve a use, the Planning Division shall make the same findings as for a Conditional Use Permit as contained in the Pomona Zoning Ordinance. The Planning Division may impose conditions deemed reasonable and necessary to ensure that these findings are met.
- ii. An application for a Minor Conditional Use Permit shall not require a public hearing.
- iii. The action taken by the Planning Division is final unless appealed to the Planning Commission by either the applicant or a member of the Planning Commission within 20 calendar days of the action.

B. Conditional Use Permit

i. Uses subject to a Conditional Use Permit shall be approved, approved with conditions, or disapproved by the Planning Commission, and are subject to the regulations and procedures pertaining to Conditional Use Permits contained in Section .580 of the Pomona Zoning Code.



Pomona Fish Market



2.3.2 Land Use Matrix

Table 2-5, Land Use Matrix					
	MU-CBD	MU-I	MU-HDR	R-MF	
Residential Uses	Residential Uses				
Single-Unit Dwelling, Detached	-	-	-	-	
Single-Unit Dwelling, Attached	-	-	-	Р	
Two-Unit Dwelling	-	-	-	-	
Multi-Unit Development	Р	Р	Р	Р	
Work/Live Units	Р	Р	Р	Р	
Family Day Care					
Small	Р	Р	Р	Р	
Large	-	-	-	М	
Group Residential	-	-	М	М	
Residential Care Facility					
Small	Р	Р	Р	Р	
Large	-	-	М	М	
Residential Facility, Assisted Living	-	-	М	М	
Supportive Housing	Transitional and supportive housing				
Transitional Housing	constitute a residential use and are subject only to those restrictions that apply to other residential uses of the same type in the same district.				

Table 2-5, Land Use Matrix					
	MU-CBD	MU-I	MU-HDR	R-MF	
Public/Semi-Public Uses	Public/Semi-Public Uses				
Colleges and Trade Schools	С	С	-	-	
Convention Center	-	С	-	-	
Cultural Institutions	М	М	М	М	
Day Care Centers	М	М	М	М	
Government Offices	P2	Р	-	-	
Hospitals and Clinics			<u> </u>		
Hospitals	-	С	-	-	
Clinics	-	Р	Р	-	
Instructional Services	М	М	М	М	
Park and Recreational Facilities	Р	Р	Р	Р	
Parking Lots and Structures	М	М	М	-	
Performing Arts, Fine Arts, Dinner Theater	М	С	С	-	
Public Assembly	С	С	С	С	
Public Safety Facilities	С	С	С	С	
Schools	С	С	С	С	
Social Service Facilities	М	М	М	М	
Tutoring Facilities	Р	Р	Р	-	

Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Commercial Uses				
Alcohol Sales (On-sale only)	M	М	М	-
Alcohol Sales (Off-sale only)	С	С	С	-
Animal Care, Sales, and Services				
Animal Sales and Grooming	Р	Р	Р	-
Veterinary Services	-	Р	Р	-
Art Sales and Services	Art Sales and Services			
Art Gallery	Р	Р	Р	Р
Artist Studio	Р	Р	Р	Р
Artisanal Manufacturing	М	М	М	М
Artist Cooperative (Co-op)	Р	Р	Р	Р
Banks and Financial Institutions	P2	Р	-	-
Commercial Entertainment and Recreation				
Cinema/Theaters	Р	Р	Р	-
Indoor Sports and Recreation	Р	Р	Р	-

Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Eating and Drinking Establishments				
Bars/Night Clubs/Lounges	М	М	М	-
Restaurant, Full Service	Р	Р	М	-
Restaurant, Limited Service	Р	Р	Р	-
Restaurant, Take-Out Only	Р	Р	М	-
Drive Through	-	-	-	-
Outdoor Dining & Eating	Р	Р	Р	-
Food and Beverage Manufacturing (Small Scale)	Р	Р	-	-
Hotels	С	С	С	-
Maintenance and Repair Services	-	-	-	-
Nurseries and Garden Center	-	-	-	-
Offices				
Business, Professional, and Technology	P2	P2	Р	-



Table 2-5, Land Use Matrix				
	MU-CBD	MU-I	MU-HDR	R-MF
Medical and Dental	P2	P2	Р	-
Walk-In Clientele	1	Р	Р	-
General Personal Services	Р	Р	Р	-
Private Garden	Р	Р	Р	Р
Recharging Station	Р	Р	Р	Р
Retail Sales				
Convenience Markets	Р	Р	Р	-
Food and Beverage Sales	Р	Р	Р	-
General Retail	Р	Р	-	-
Solar Energy Systems	Р	Р	Р	Р

2.3.3 Temporary Uses

1. Intent

The Downtown Pomona Specific Plan recognizes that a diversity of land uses and activity will contribute towards making Downtown Pomona a vibrant destination to live, work, and play. The purpose of this section is to encourage a variety of temporary uses on private property in Downtown Pomona while also establishing conditions to ensure that such temporary uses do not adversely affect the public health, safety, and welfare, are compatible with surrounding land uses, protect the rights of adjacent residents and land owners, and minimize any adverse effects on surrounding properties and the environment. This section will supersede any temporary use provisions in the Pomona Zoning Code for all parcels located within the Downtown Pomona Specific Plan. This section does not apply to any publicly-owned property, which is subject to a Special Event Permit.

2. Authority

The Development Services Director is authorized to approve, conditionally approve or to deny a Temporary Use Permit application. The Development Services Director may establish conditions and limitations to minimize detrimental effects on surrounding properties, including but not limited to hours of operation, provision of parking areas, signing, lighting, and traffic circulation access. A business license shall also be obtained.

3. Application

The property owner or the owner's authorized representative shall file an application for a Temporary Use Permit with the Planning division at least 30 days prior to the date the proposed use takes place. Non-profit organizations or other organizations with verifiable tax-exempt status are not subject to any fees associated with a Temporary Use Permit. For reoccurring temporary uses, the property owner or the owner's authorized representative shall file a single application for a Temporary Use Permit with the Planning Division with the predetermined dates at least ten days prior to the first predetermined date of the proposed use to take place. All reoccurring temporary uses may be approved for predetermined dates for up to three calendar months.

The Development Services Director may request additional documentation, such as traffic studies, operation plans, or other data necessary to adequately review a Temporary Use Permit application.

4. Approval

To approve a Temporary Use Permit application, the Development Services Director shall make the following findings:

- A. The proposed temporary use will be located, operated, and maintained in a manner consistent with the policies of the General Plan and the provisions of the Downtown Pomona Specific Plan.
- B. Approval of the application will not be detrimental to property or improvements in the surrounding area or to the public health, safety or general welfare.
- C. The proposed temporary use complies with the various provisions of this ordinance.
- D. All building, electric, plumbing, fire, encroachment or other permits required by city ordinances shall be obtained.

5. Permitted Uses

The following temporary uses are allowed on all private property located within the boundaries of the Downtown Pomona Specific Plan:

- A. Pop-up restaurant and retail. The temporary use permit shall be subject to the following conditions:
 - i. Limited to one restaurant or retail use for a 60-day day period.
 - ii. The sale of alcohol may be permitted subject to approval by Alcoholic Beverage Control (ABC).
 - iii. The proposed use shall obtain all necessary permits with the Planning and Building and Safety Divisions for interior tenant improvements proposed for the restaurant or retail use. Restaurant uses shall also obtain all necessary healthy and safety permits.
 - iv. The proposed signage shall abide by the Temporary Sign Permit regulations, with the exception to duration. The proposed signage shall be limited to the period approved for the temporary use permit.
- B. Other temporary uses which meet the required findings for Substantial Conformance.

2.3.4 Non-Conforming Provisions

1. Uses that legally exist and are permitted prior to the adoption of this plan, shall be classified as legal, nonconforming uses and are allowed to continue operation provided they do not cause any adverse health, safety, or aesthetic related impact(s) on the community. These uses are not allowed to expand to additional sites, locations or addresses in a zone where the use is not specified as a permitted use except as allowed herein and as approved by the Planning Commission under a Conditional Use Permit.

A legal non-conforming business may be sold or have a change of management, including a new building occupant or tenant with the same use as previously existed without having to revert to a use that is permitted by the applicable zone.

A legal non-conforming use may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.

2. Commercial or residential use. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone unless a continuation or reestablishment of that commercial or residential nonconforming use is approved by the Planning Commission under a Conditional Use Permit within three years of its closure as noted herein and said use is shown to be a use that will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole.

A revocation hearing will be held for a legal non-conforming use that displays a pattern of code violations or which create a public nuisance and if the use is revoked, it is deemed "closed" and must revert to a use that is permitted by the applicable zone. The continuation or reestablishment of a revoked non-conforming use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management, including a new building occupant and/or tenant, and that such continued or re-established use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation.

- 3. Residential structures. When a legal nonconforming, residential structure(s) is/are demolished, deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, the land use at that location must revert to a use permitted by the applicable zone. Where a legal nonconforming, residential structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of a Building Permit. A revocation hearing will be held for a legal non-conforming residential use that displays a pattern of code violations or creates a public nuisance and if the use is revoked, it is deemed "closed" and must revert to a use that is permitted by the applicable zone. The re-establishment of a revoked legal non-conforming residential use may be allowed if it is shown, to the satisfaction of the Planning Commission under review of a Conditional Use Permit within three years of its revocation hearing, to have been sold or have a change of management and that such re-established residential use will not create or produce adverse physical or aesthetic impacts to the site, adjoining properties or the surrounding area as a whole as had lead to the original revocation. Applicability thresholds shall not apply to legally nonconforming single-unit residential, which shall be subject to R-1-10,000 standards.
- 4. Commercial or residential use within a nonconforming structure. When a legal nonconforming use closes (a cessation of regular business activity or residential occupancy of more than 180 days (6 months), or changes to a conforming use, or the lost or expiration of a business license, or the relocation of the business or residence to another location or address, or the suspension of utility services to the site, or the revocation of a conditional use permit for the use by the City, or the closure of the use is ordered by a public agency) the land use must revert to a use that is permitted by the applicable zone, except that the same or substantially the same use may be reestablished or established within said non-conforming structure within three years of its closure as noted herein when that structure was specifically designed and built for, and historically occupied by, the same or substantially the same type of use.
- 5. Non-conforming structures. When a structure that was built to all applicable standards and codes at the time of construction and under an appropriately issued Building Permit becomes nonconforming due to a change(s) to the development standards of the Downtown Pomona Specific Plan (including setbacks, height limits, landscaping, parking, architecture, equipment screening, storage, fencing, etc.), said structure may continue to be occupied or may be re-occupied by a use permitted within the underlying applicable zone which required the same standards consistent with those for the structure as it was originally constructed. Said occupation or reoccupation may occur at any time provided that the structure has been maintained in a safe and sanitary condition, has not become a public

or attractive nuisance, has not been physically altered in a manner that would reduce conformance to the original development standards at the time of original construction and upon approval of a Conditional Use Permit by the Planning Commission. Legal non-conforming structures may be sold or have a change of management, including a new building occupant or tenant with the same or substantially similar use as historically existed without having to physically alter or modify the structure or site to conform to the current development standards of the underlying applicable zone. When a legal non-conforming structure is deemed unfit for human occupation by a public agency, or otherwise destroyed or removed, any future structure(s) at that location must conform to the development standards of the underlying applicable zone. Where a legal non-conforming structure is deemed to be unfit for human occupation by a public agency, the property owner may, within three years of the date deemed unfit and provided such structure has not become a public or attractive nuisance, rehabilitate and re-occupy (with the same or substantially similar use) said structure to a safe and sanitary condition as provided within the Uniform Codes, upon the Planning Commission's approval of a Conditional Use Permit and issuance of an appropriate Building Permit.

A legal non-conforming structure may close for renovations that are approved by the City through a building permit or other discretionary permit, provided the closure does not last more than 120 days or as provided for by the City under the conditions of approval of a discretionary permit.

2.4 Signage

2.4.1 Intent

The signage regulations contained in this section are intended to promote the orderly development of new signage within the Downtown Pomona Specific Plan and to encourage the community to recognize, enhance, protect and promote Downtown Pomona's unique character and identity. The purpose of this section is to promote public safety and welfare by regulating signs with the following stated objectives:

- 1. Prioritize the pedestrian environment within the Downtown Pomona Specific Plan.
- 2. Encourage high quality sign design.
- 3. Assure that signs are sized and arranged to minimize clutter and prevent unsafe conditions.
- 4. Assure that signs harmonize with the underlying urban design district where they are located.
- 5. Assure that signs and their message are of sufficient size and design to be legible and easily read by the intended viewer.
- 6. Encourage creative and innovative approaches to signs within the context of Downtown Pomona.



- 7. Ensure that commercial signs are designed for the purpose of identifying a business in an attractive and functional manner, rather than to serve primarily as general advertising for a business.
- 8. Ensure that signs on the façades of buildings reinforce the existing character and are integrated into the architectural scheme of the building.
- 9. Promote cohesiveness among signs on a single building.
- 10. Ensure that new signs on historical structures are sensitive to the historic character of the structure.

2.4.2 Applicability

The sign requirements in this section apply to all new signage within the boundaries of the Downtown Pomona Specific Plan. Where the provisions of this section conflict with Section .503-K or any other provisions of the Zoning Code, the Downtown Pomona Specific Plan shall prevail. Where this section is silent regarding signs, the Zoning Code of the City of Pomona shall be reviewed by the Planning staff to determine the applicability of the development standards in the Zoning Code.

2.4.3 Application Procedure

Except as exempted in Section .503-K of the Zoning Ordinance, all new signs, reconstructed signs, and sign alterations, except change of copy, require approval of a sign permit from the City of Pomona Planning Division, with permit types defined in Table 2-6. Building permits and/or electrical permits shall be obtained as required in accordance with the Uniform Building Code and/or National Electrical Code.



Pomona Fish Market Neon Sign

Table 2-6, Permit Types, Applicability, and Review					
Type of Sign Permit	Applicability	Review Body	Apellant Body		
Sign Permit	All new primary, secondary, and portable signs; repurposed signs; and alterations to existing signs beyond like-for-like replacement or repair.	Planning Division	Planning Commission		
Master Sign Plan	As defined in .503-K C. 4	Planning Division	Planning Commission		
Creative Sign Permit	Any new creative sign that cannot meet standards due to innovative features	Planning Commission	City Council		
Minor Certificate of Appropriateness	All new signs on a registered historic landmark or district, as defined in .5809-13 (Historic Preservation)	Planning Division	Historic Preservation Commission		
Temporary Sign Permit	All new temporary signs	Planning Division	Planning Commission		

2.4.4 Sign Categories and Types

Commercial signage generally falls into one of the following four categories:

- 1. **Primary sign.** A business with one building frontage shall have only one primary sign, typically a wall sign, unless another type of sign is approved under a Creative Sign Permit. In cases where a Creative Sign Permit has been approved for a sign, that sign shall be designated as the primary sign. A business with multiple building frontages may have one primary sign on each building frontage.
- 2. Secondary sign. Secondary signs are usually installed in addition to the primary building sign. Typically they include awning signs, projecting signs and under-canopy signs; and are generally intended to be viewed by pedestrians. A business may have one secondary sign per storefront for the business.
- 3. **Portable sign.** Intended to be viewed by the pedestrian, portable signs are intended to be placed on the sidewalk in front of a business. They include sandwich board signs and "A"-frame signs. A maximum of one per business is permitted during hours of business operation only and is not counted as either a primary or secondary sign.
- **4. Repurposed sign.** A repurposed sign may advertise a business no longer in existence and/or is a sign that has been preserved for use in a new location for the purpose

- of improving the aesthetic quality of the location. Historic signs can add value to the Downtown area by providing artifacts of visual interest along building frontages.
- 5. Historic sign. A historic sign is one that is proposed to be located on a building that is registered as a federal, state, or local historic landmark. Such signs require a Minor Certificate of Appropriateness.
- 6. Creative sign. A creative sign cannot meet standards contained in this section due to innovative or creative elements unique to the sign. A creative sign, when approved, will take the place of a Primary sign for that respective building frontage. Creative signs are subject to approval by the Planning Commission.
- **7. Temporary sign.** Usually meant for a special purpose and are used for short periods. Only one per tenant is permitted and is not counted as either a primary or secondary sign.

2.4.5 Permitted Signs

The following types of signs are permitted within the DPSP area provided they are consistent with the stated criteria and have an approved sign permit. When the word "shall" is used or specific standards are given, the criteria are considered standards. If the words "may" or "should" are used, the criteria is a guidelines and compliance is strongly recommended unless a suitable alternative can be substituted that achieves the same purpose.

1. A-frame Signs (Sandwich Board)

- A. A-frame signs shall not obstruct or interfere with pedestrian traffic on the sidewalk.
- B. A-frame signs shall be removed from the sidewalk when the business it is advertising is closed.
- C. Maximum area is 12 square feet per sign face.
- D. Maximum size is four feet high by three feet wide.
- E. A-frame signs shall not be permanently affixed to any object, structure or the ground.
- F. A-frame signs shall be professionally constructed using one of the following materials: plastic, wood or metal suspended from a wire frame, wooden A-frame signs with open bases, or shaped silhouette signs made of plywood, metal, or similar wood-like material that can withstand various weather conditions.
- G. Glass, breakable materials, paper, laminated paper, vinyl, plastic, PVC pipe frames, or illumination are not permitted materials for A-Frame Signs.

2. Awning and Canopy Signs

- A. Lettering shall be permitted only on the valance and shall not exceed six inches in height.
- B. Awnings with a solid color that is compatible with the building colors are preferred.
- C. Striped awnings may be appropriate for buildings without ornamental facades. Striped awnings with highly contrasting, bright colors may be visually blaring and are inappropriate.
- D. Only permanent signs that are an integral part of the awning shall be permitted.
- E. Awning signs shall be permitted only for first floor tenants.
- F. Awnings shall not be lighted from under the awning (backlit) so that the awning appears internally lit. Lighting directed downwards that does not illuminate the awning is allowed.
- G. Awning and canopy materials are limited to matte finish canvas, glass, or metal.
- H. Translucent fabric and plastic are not permitted as awning or canopy material.

- I. Awnings shall be installed in a location that does not obscure transom windows, grillwork, piers, pilasters and other ornamental features. In openings with transoms, awnings shall be installed below the transom on the horizontal frame separating the storefront window from the transom.
- J. Shed awnings without end panels are the preferred awning style. Convex-shaped awnings may be appropriate for rounded arched window and door openings.
- K. Awnings should have simple horizontal valances. Scalloped or decorative valences are discouraged.

3. Building Edge Signs

- A. Building edge signs are permitted on buildings located on street corners.
- B. Edge signs shall include a graphic image relating to the business being advertised.

4. Exposed Neon Tube Signs

- A. Exposed neon signs are signs where the neon is exposed with or without a backing.
- B. Neon lighting shall not exceed 30 milliamps.

5. Hanging (under canopy) Signs

- A. Hanging signs shall only be used at ground floor locations and placed in front of the business frontage which it is advertising.
- B. Maximum area for a single face of a hanging sign is three square feet.
- C. The sign shall be a minimum of seven feet above the sidewalk.
- D. A maximum of one tenant per sign is permitted.

6. Low-Profile Monument Signs

- A. A low-profile monument sign shall have a base and sign face area above it.
- B. Use of architectural details such as cornices and materials that complement or reflect those on the building is required.
- C. Lettering shall consist of individual letters in a style and material that is consistent with the business use and/or the architectural style of the on-site building.
- D. Maximum height of the sign, including the base and sign face, shall be limited to four feet.
- E. Illumination shall consist of either external fixtures designed to complement the sign's appearance or halo-lighting.

- F. A maximum of one low-profile monument sign is permitted per property.
- G. The area around the base of the sign shall be covered with a minimum width of two feet of landscaping, rock, or other aesthetic material to soften the appearance of the sign.
- H. Low-profile monument signs may be located within any land use district in the Downtown Pomona Specific Plan, but are limited to properties where the main building is set back a minimum of ten feet from the front property line.
- I. Low profile signs shall only be used when other types of signage would not provide adequate identification.

7. Over-Sized Projecting Signs

Over-sized projecting signs shall only be permitted on buildings over two stories in height with approval of a Creative Sign Permit. Over-sized projecting signs shall be consistent with the following

- A. Project no more than two feet from a wall to which it is attached.
- B. Minimum of 10 feet above grade.
- C. One over-sized projecting sign is permitted per ground floor business with building frontage on a public street.
- D. Over-sized projecting signs shall be located directly in front of the business which it is advertising.

E. Located no higher than the height of the finished floor level of the fourth floor of the building.

8. Projecting Signs

- A. The total area of a sign face of a projecting sign shall not exceed 3.5 square feet.
- B. The bottom of a projecting signs shall be seven feet above the surface over which it projects.
- C. Projecting signs shall not project more than four feet into a public right-of-way and shall not project closer than two feet from a curb.
- D. Projecting signs shall have a minimum thickness of 18 inches.
- E. Internally lit projecting signs shall have opaque face panels so that only the letters, logos, numbers or symbols appear to be lit.
- F. Projecting signs shall be a minimum of 15 feet from another projecting sign and 5 feet from an interior property line or line dividing two separate business frontages.
- G. Projecting signs shall hang from decorative metal brackets of a size and design appropriate to the sign and the architectural character of the building. The design of the sign bracket shall not distract or compete with the design of the sign.

9. Restaurant Menu Signs

- A. Must be a permanently mounted display box or freestanding display box on one or two poles where the menu is placed within a display box. The sign shall be located outside adjacent to the main entrance of the restaurant.
- B. Freestanding restaurant menu signs shall be removed from the exterior located when the restaurant closes.
- C. High quality materials and artistic designs shall be used in the construction of menu signs.
- D. Menu signs are not included in the total allowable square footage for a business.
- E. Maximum allowable sign area is six square feet.
- F. Menu signs shall be appropriate in size, location and design to the character and architectural detail of the building as well as the restaurant.
- G. Only one menu sign is permitted per restaurant.

10. Roof Signs

- A. Roof signs may be permitted under a Creative Sign Permit with the following restrictions:
 - i. Such signs shall only be located on buildings six floors or higher.
 - ii. Location. Permitted on non-local streets.

11. Temporary Banner Signs

- A. Temporary banners signs in the Downtown Pomona Specific Plan area shall be consistent with the following:
 - i. A Temporary Banner sign shall be permitted for a maximum of 180 days per calendar year for each business storefront.
 - ii. Maximum size of a temporary banner is no more than 45 square feet in area.
 - iii. Shall require approval of a Temporary Sign Permit.
 - iv. Temporary Banner Signs shall be continuously maintained. Banners shall be replaced if tattered or worn.

12. Tenant Directory Signs

- A. Tenant directory signs shall be constructed and oriented to the pedestrian.
- B. Maximum height is seven feet.
- C. Sign copy shall include only the building or project name, project logo, address, business tenant names, and suite numbers or letters.
- D. The letter size of the building name, project name, or logo shall not exceed four inches in height. All other copy shall not exceed two inches in height.
- E. Tenant Directory signs should be constructed out of materials that compliment both the building structure and its use.

13. Wall Signs

- A. Wall signs shall be located only on a primary or secondary building frontage and shall not extend above an eave or parapet.
- B. Wall signs shall have individual letters and may be either internally (except as cabinet signs) or externally lit.

- C. Maximum area of a wall sign is limited to 1.5 square feet per linear foot of building frontage.
- D. Wall signs are only permitted for businesses located on the first floor of a building and businesses located on the other floors when limited to one square foot per lineal foot of building frontage.
- E. Wall signs should be designed to create a clearly defined edge, provide shadow relief, and a substantial appearance. This effect is generally difficult to achieve by painting the sign directly on the building. For this reason, painted signs are discouraged.
- F. Wall signs shall be designed to be compatible with the storefront in scale, proportions, and color.
- G. Wall signs shall be mounted flush and fixed securely to a building wall, projecting no more than 12 inches from the face of a building wall, and not extending sideways beyond the building face or above the highest line of the building to which it is attached.

14. Window Signs

Window signs shall be consistent with the following criteria:

- A. Permitted for ground floor commercial throughout the mixed use districts within the specific plan area.
- B. Placement is limited to ground floor storefront windows.
- C. Window signs above the first floor are prohibited.
- D. Maximum of 20 percent of the window area of each window on front of building occupied by business displaying signs.
- E. Sign copy shall not exceed eight inches in height.
- F. Window signs should be created from high-quality materials such as paint, gold-leaf, or neon. Appropriate techniques for application of window signs include sandblasting or etching.
- G. Window signs shall be applied directly to the interior surface of the window or hung inside the window, such that all mounting hardware and equipment is concealed. Window signs shall not be more than 12 inches from the window surface.

15. Prohibited Signs

- A. Cabinet signs
- B. Pole signs
- C. Off-site signs, except where otherwise permitted
- D. Inflatable signs
- E. Internally lit awnings
- Statues, sculptures, large three dimensional props and parked vehicles or vehicle trailers used specifically for advertising
- G. Telephone numbers
- H. Captive balloon
- I. Roof signs except as approved under a Creative Sign Permit
- J. Super graphics, except as approved under a Creative Sign Permit

- K. Electronic center displays, except time and temperature signs
- L. Electronic Signs
- M. LED signs
- N. Freestanding banner signs
- O. Spinners

2.4.6 General Criteria

1. Sign Illumination

The following types of sign illumination are permitted within the DPSP:

- 1. A projecting light source is recommended for signs within the DPSP area. Use of small, unobtrusive fixtures for external (projection) lighting is encouraged. Avoid use of oversized fixtures that are out of scale with the sign and structure. Light sources shall be shielded and directed away from adjacent residential uses.
- 2. Back-lit signs, also referred to as halo-lit signs, consisting of opaque individually cut letters installed directly on a building.
- 3. Neon signs

2. Electrical Raceways and Conduits

Use of electrical raceways and conduits shall be consistent with the following:

- 1. Electrical transformer boxes and raceways are required to be concealed from public view.
- 2. If a raceway cannot be mounted internally behind the finished exterior wall, the exposed metal surfaces of the raceway shall be finished to match the background wall or integrated into the overall design of the sign.
- 3. If a raceway is necessary, it should be as thin and narrow as possible and should never extend in width or height beyond the area of the sign's lettering or graphics.
- 4. All exposed conduit and junction boxes shall also be concealed from public view.

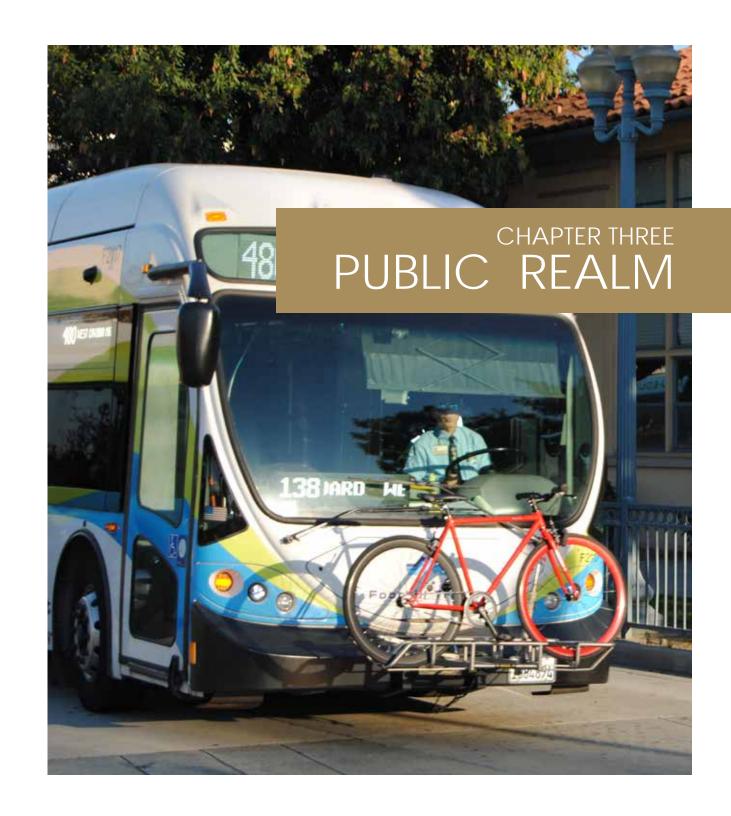
3. Sign Placement

Wall signs should be placed in the area of the façade historically used for this purpose such as the sign band between the storefront and the upper facade. Where no sign band exists, a wall sign shall be placed immediately above the storefront. For new construction, a sign panel should be included on the façade of the building directly above each storefront.



2.4.7 Nonconforming Signs

Minor repairs, maintenance, and upkeep of legal nonconforming signs are permitted but in no case shall the sign be modified or altered that would result in an increase of non-conformity. All signs that advertise a business that is no longer on site shall be considered abandoned and shall be removed within 60 days of the opening of a new establishment, unless the sign is approved under a repurposed sign permit. Any legal nonconforming sign destroyed by natural causes may be restored to its original state.



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3.1 Introduction

The Public Realm chapter provides direction for external spaces that are publicly accessible within the Specific Plan. The public realm should complement land uses and buildings (regulated by Chapter 2 – Private Realm) and consists of the 'everyday spaces' that are used by people to socialize, play, work, shop, traverse and use for range of activities and social opportunities. The provisions contained within this chapter apply where improvements are proposed in public rights of way as well as publicly accessible spaces. This chapter addresses a range of topics including mobility, placemaking, and infrastructure and utilities. The mobility section emphasizes providing a range of transportation options including pedestrian improvements, bicycle network enhancements, vehicular circulation considerations, and transit services as well as parking strategies. The placemaking section envisions an inviting and functional public realm and includes guidance relating



to historical landmarks, art in public places, parklets and plazas, pedestrian amenities, streetscapes, lighting and wayfinding. Lastly, the infrastructure and utilities section addresses public services and systems that provide backbone support for a successful downtown.

3.2 Mobility

3.2.1 Intent

One of the central directives of the Specific Plan is to create beautiful yet functional streets that include integrated transportation systems, provide pedestrian and bicycle links, and improve overall vehicular circulation. The beautification and mobility improvements for Downtown Pomona will strengthen the City's identity and move residents, visitors, goods, and services more efficiently. This chapter is organized into circulation, bicycle, pedestrian and transit networks followed by parking strategies.

3.2.2 Mobility Goals

Informed by the community engagement process, the mobility goals guide future multimodal transportation improvements.

- 1 Accommodate an effective public transportation system.
- 2 Improve accessibility for persons with disabilties.
- Provide connectivity and accessibility to the Commuter Rail Station from Downtown areas.
- Ensure an adequate supply of convenient parking for emerging development projects.
- Provide for parking and circulation of bicycles.
- Promote a pedestrian scale environment with enhanced pedestrian areas.







3.2.3 Circulation Network

Downtown Pomona is characterized by a traditional street system grid. The circulation network consists of streets (divided into arterial, collector and local classifications) that provides access to and from regional and local facilities and accommodates travel by various modes of transportation (automobiles, trucks, public transit, bicycles and walking). As shown in **Figure 3-1, Circulation Network**, streets are categorized using a hierarchical system of classification based on function, location, and design, and are assigned a classification based on the following descriptions.

Arterial

Arterial streets are generally high speed/high capacity roads that connect to major regional transportation facilities, such as the expressway system, and serve relatively long trips. Some designated arterials in Pomona are medium speed/medium capacity roads for intracommunity travel, usually providing direct connections from one side of the City to the other. Within Pomona high capacity roads are designated as "major" arterials and medium capacity roads are designated as "minor" arterials. Within the Specific Plan area, Holt Avenue is a prominent major arterial and Garey Avenue is considered a minor arterial.

Collector

Collectors are the 'bridge' between access and mobility in the functional classification system. These streets generally feature lower speeds and volumes than arterials and provide for circulation between neighborhoods (and not just within them). Their main function is to provide access for short trips and distribution to the arterial network.

Local

Local streets are low speed and low volume roadways that provide direct access to abutting land uses. Driveways to individual units, on-street parking, and pedestrian access are allowed.

The street grid system network supports convenient access to transit, with the Pomona Transit Center located centrally in the Specific Plan area. In addition, the downtown area benefits from lower speeds on arterial roadways. While the lower speeds reduce overall roadway capacity for vehicular traffic, lower speeds also provide an opportunity to increase the on-street parking supply and enhance the streetscape. Posted speed limits within the Specific Plan area range from 25 mph to 40 mph.

The Union Pacific Railway runs east-west through the Specific Plan area and can provide a barrier in the area. To address this challenge, an undercrossing is provided for Garey Avenue, White Avenue and Towne Avenue, while at-grade crossings are provided at Park Avenue, Main Street, and Palomares Street. A pedestrian bridge crosses the railway west of Garey Avenue near the Pomona Transit Center.

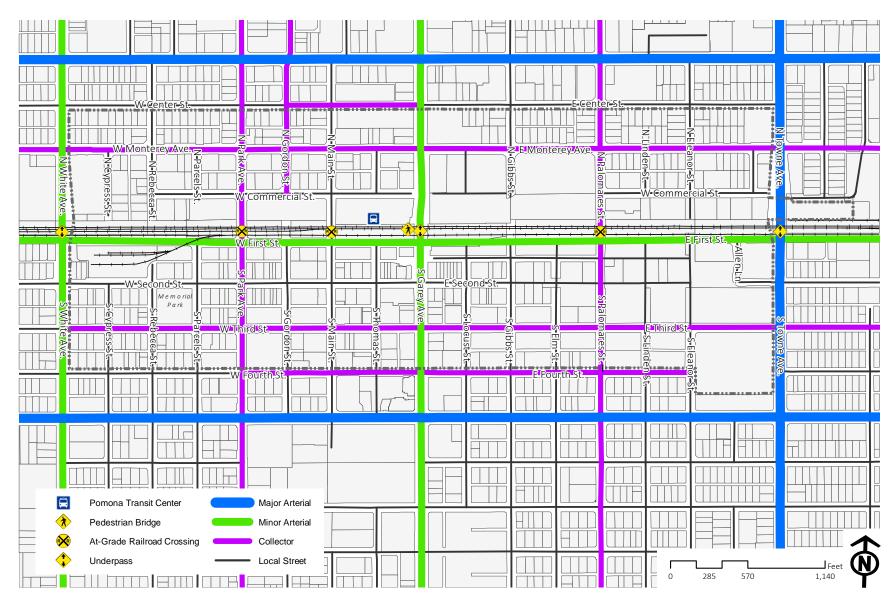


Figure 3-1, Circulation Network

A review of the Specific Plan area revealed a total of 409 driveway locations as shown in **Figure 3-2, Driveway Locations**. This includes one-way and two-way inbound and outbound driveway locations. Driveways create conflicts between vehicles entering or leaving a street and bicyclists and pedestrians riding or walking along the street. When possible, new driveways should be minimized and old driveways should be eliminated or consolidated. In addition, reducing the number of driveway locations will reduce conflicts and increase on-street parking opportunities. For this reason, the Specific Plan limits the maximum number of driveways/curb cuts with a single building along all streets to one two-lane curb cut or two one-lane curb cuts.

Public Realm Improvements

Streetscape Improvements

The City plans to implement phased streetscape improvements that will contribute significantly to the enhancement of the visual appeal and identity of key areas of the Downtown Pomona Specific Plan. Streetscape improvements have been designed to promote the type of change envisioned by the community by providing attractive and compatible environments for the desired types of new development, as well as for highly valued existing development.

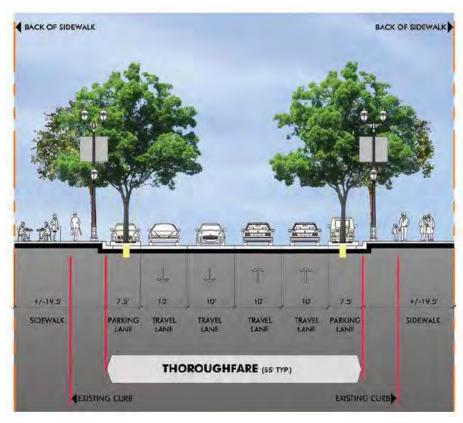
Implementation

Streetscape improvements will be installed and paid for by a combination of public and private investment. New private development in the Downtown Pomona Specific Plan boundaries will include the installation of sidewalk and landscaping improvements between property line and curb. Private investors will also provide payment for the costs of installing public improvements to the street centerline.

Public implementation of streetscape improvements will be staged over time, and as financial resources allow. Public implementation of streetscape improvements within the Downtown Pomona Specific Plan will provide improvements between curbs as well as improvements along public frontage areas of properties that have not yet been improved per the standards contained in this section.

Design

The design of specific streetscape improvements is integrated within the overall vision of Public Realm improvements within the Downtown Pomona Specific Plan. The integration of street design with building disposition and site improvements will result in the emergence of an increasingly cohesive and iconic Downtown Pomona.



Typical Downtown Boulevard Configuration

The following specific streetscape improvements shall be applicable to new development:

- **1. Garey Avenue**—improvements along Garey Avenue between Fourth Street and Center Street.
- **2. Second Street**—improvements to the portion of Second Street between South Main and Gibbs Streets.

Garey Avenue Public Realm Improvements

- **1. Streetscape.** Street improvements will feature an approximately 19.5 foot wide widened sidewalk with no planter strip. Infill street trees are planted in individual tree wells.
- 2. Street Lighting. Double-head/arm pendant boulevard-scale street lighting with double-head/arm post-top pedestrian-scale luminaires located at sidewalks, with a spacing of approximately 56 feet on-center, centered between street trees. The boulevard-scale light source should be located 25-30 feet above finished grade, with double-arms perpendicular to the street centerline; the pedestrian-height luminaires are mounted on double-arms parallel to the street centerline, located 12-14 feet above finished grade, with double-arms parallel to the street centerline.

Streetlight poles shall contain pairs of banner arms with wind relief mounting devices.

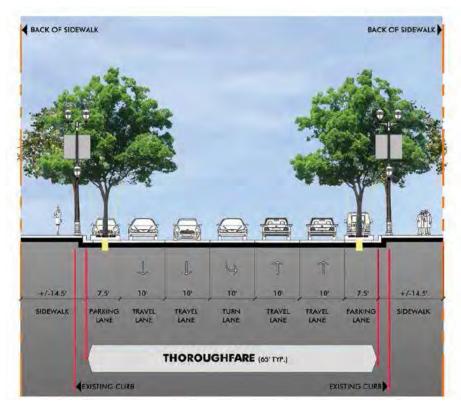
Where needed to supplement intersection lighting, singlearm pendant boulevard-scale street lighting located within the planter strip, at a sidewalk corner, or atop a traffic signal pole. Light source should be located 25-30 feet above finished grade.

Trees within parking lanes on curbed islands are each uplit from both front and rear with in-grade fixtures, on times to shut off after a late night hour designated by the City.

3. Street Trees. At sidewalk tree wells, trees are planted 28 feet on-center.

Parking lane trees are planted in curbed islands between every two parking stalls, within the parking lane at approximately 56 feet on center. Curbed islands are 6.5 feet wide by 8 feet long, offset from the existing face of curb by 12 inches to enable passage of existing drainage. Street tree trunks are asymmetrically positioned within the 8-foot length to allow for dissimilar front and rear parked vehicle overhangs, with the tree trunk centerline set back from face of curb by 3.5 feet at the vehicle front overhang and by 4.5 feet at the vehicle rear overhang.

4. Transit Stops. Special configuration of the streetscape at transit stops will require coordination with bus transit agencies for concept and final design.



Downtown Bouelvard Configuration with Turn Lane



Antique Row on Second Street

Second Street Public Realm Renewal (South Main to South Gibbs)

These four core blocks represent less than half the length of the original nine blocks of the historic 1962 pedestrian mall streetscape improvements. They represent only an initial phase associated with recommended improvements on Garey Avenue.

Improvements are intended only to renew the appearance and physical integrity of the current streetscape and are limited to: refurbishment of existing street lighting, tree uplighting, and ornamental street signs; addition of benches; infill of damages or missing street trees and associated irrigation and planting beds; and rehabilitation of worn, exposed paving or planter wall surfaces. The configuration of existing lanes, parking, paving and drainage will remain unchanged.

1. Street Lighting. Three-head, post-top pedestrian-scale decorative street lighting assemblies with luminaires, arms, and accessories to match boulevard fixtures, to replace existing 3-head decorative luminaires and arms at existing poles—or to also replace existing poles if required, at present locations. The luminaire shall be of a cutoff type with opaque cap, shall have a large prismatic globe lens to diffuse and mitigate glare, and feature capability for interchangeable convention or LED light sources. Paint color applied to metal finishes to match existing streetlight paint color.

Where needed to supplement intersection lighting, single-head/arm pendant boulevard-scale street lighting located at a sidewalk corner or atop a traffic signal pole. Light source should be located 25-30 feet above finished grade.

Existing street trees within raised concrete planters (historic) within parking lane are each uplit by uplight fixtures from within the planters at both front and rear, on timers to shut off after a late night hour designated by the City.

- **2. Furnishings.** Benches with arms may be mounted on the sidewalk-facing side of existing concrete planters. Refurbish and repaint existing decorative street name signs within existing raised concrete planters.
- **3. Pavement Surfaces and Planter Wall Surfaces.** At sidewalk, parking, and travel lane surfaces –replace existing surfaces in disrepair with same materials, or as closely matched as is feasible. Existing exposed vertical planter walls and fountain walls—repair and refurbish cracked, chipped or water materials and surfaces.
- **4. Street Trees.** Where existing street trees within raised planters are damaged or missing, infill with replacement trees of the original species at the largest practical size. Amend or repair planting pits, tree supports, soil media, and irrigation function as required.

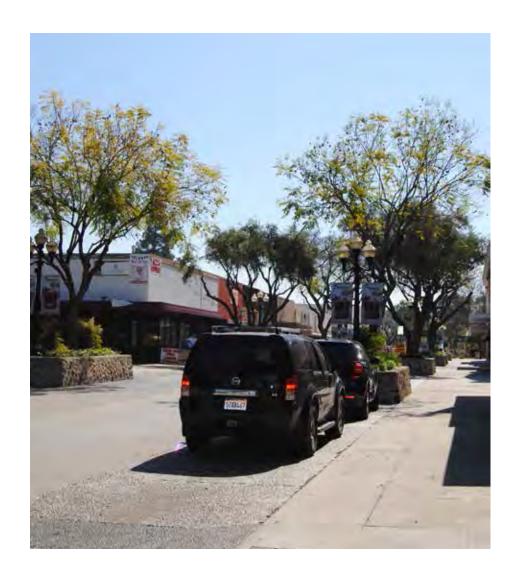


Existing street furnishings



Figure 3-2, Driveway Locations

- 3.2.1 Retain the grid circulation system.
- 3.2.2 Accommodate citywide/regional traffic on Holt Avenue, White Avenue and Towne Avenue.
- 3.2.3 Draw citywide/regional traffic into Downtown along Garey Avenue.
- 3.2.4 Provide appropriate pedestrian, bicyclist, and automobile circulation within and between Specific Plan Districts.
- 3.2.5 Accommodate the current pattern of bus stops on Holt Avenue, Garey Avenue, White Avenue and Towne Avenue.
- 3.2.6 Provide landscaped medians to reduce the scale of existing major arterials and other significant roads such as Towne Avenue and White Avenue, as appropriate.
- 3.2.7 Reduce the number of driveway locations. When possible, new driveways should be minimized and old driveways should be eliminated or consolidated.
- 3.2.8 Promote ridesharing opportunities by encouraging the use of drop-off/loading zones.



3.2.4 Bicycle Network

The size, topography and climate of Pomona make it an ideal setting for bicycling with a significant portion of the community within a 10-minute ride from Downtown, as demonstrated in the Mobility and Access Component of the Pomona General Plan where policies are provided for existing and planned bicycle facilities. In addition, the Pomona Active Transportation Plan (ATP) describes detailed direction on how to continue progress toward a better bicycling and walking environment. The ATP does this by proposing a system of bikeways to connect neighborhoods to key activity centers throughout the City, developing support facilities such as bike parking, and education programs, and by identifying recommendations for improving bicyclist safety.

Bicycle facilities are typically divided into three distinct types as described below:

Class I (Bicycle Paths): Class I bikeways are completely separated from motor vehicle traffic, as in the case of an off-street path along a river or railroad corridor. Class I bikeways have varying widths, generally between 8 and 12 feet. Pedestrians are allowed on Class I paths.

Class II (Bicycle Lanes): Class II bicycle lanes are located on streets and allow bicyclists to utilize a separate lane of travel, usually five to six feet wide, separated from motor vehicle traffic by a six-inch white stripe.

Class III (Bicycle Route): Class III bikeways are designated by signs only. Cyclists share the travel lane with motor vehicle traffic on these routes. Some Class III routes have a wide outer curb lane while others carry low volumes of motor vehicle traffic, making a separate bicycle lane or wide curb lane unnecessary.

Class IV (Separated Bikeway/Cycle Track): Class IV separated bikeways are often referred to as cycle tracks or protected bike lanes. Class IV facilities provide for the exclusive use of bicycles, and are separated from vehicular traffic by a physical barrier such as flexible posts, inflexible barriers, or parking.

Planned bicycle facilities are shown in **Figure 3-3**, **Bicycle Network**, and are proposed on the following streets:

North-south:

Palomares Street

East-west:

Second Street



CLASS I BIKE PATH



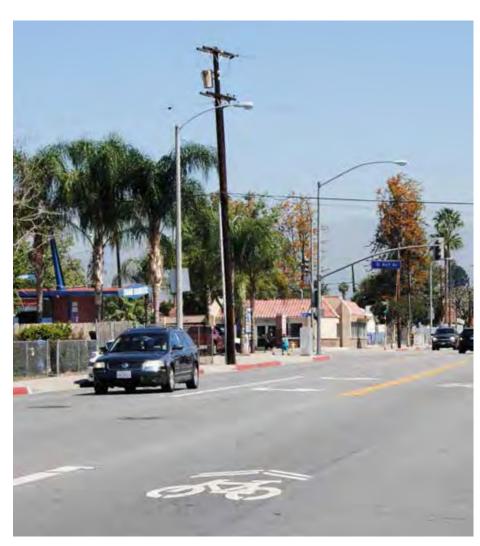
CLASS III BIKE ROUTE



CLASS II BIKE LANE



CLASS IV CYCLE TRACK



Bicycle facility

Recommended Bicycle Facility Treatments

The following bicycling-friendly treatments may be considered along bicycle routes. These treatments are described in detail in Chapter 8 (Design Guidelines) of the ATP.

- Five to six-foot bicycle lanes
- Physically separated bicycle lanes with buffer
- Colored bicycle lanes
- Sharrows
- Accommodation at large intersections and freeway interchanges
- Signage and wayfinding



Figure 3-3, Bicycle Network

To support bicyclists and the use of the bicycle network, establishing a bicycle rack program for the installation of bicycle parking facilities at key locations such as transit facilities, civic uses, and other bicycle generators. This strategy is recommended to complement private development bicycle parking requirements established in Chapter 2, Land Use and Urban Form. Identified locations for bicycle parking include:

- Pomona Transit Center
- Pomona Post Office (580 W. Monterey Avenue)
- The Glass House/Fox Theater
- Western University

It is recommended that the City enhance the accessibility of transit to bicyclists by installing secure short-term bicycle parking in the form of additional bicycle racks and long-term bicycle parking in the form of bicycle lockers. Bicycle lockers allow potential transit patrons to ride to a transit station and board transit service without needing to take their bicycle along. These should be acquired and established at the Downtown Pomona Metrolink station. In addition, support facilities such as restrooms, showers, and changing facilities should be provided at this location to further facilitate bike commuter comfort and willingness to bicycle to and from these facilities.

As plans become more fully developed for bicycle facilities improvements, coordination with programs (such as Safe Routes to School) and interested stakeholders (such as Pomona Valley Bicycle Coalition and Pomona Police Department) should be facilitated.

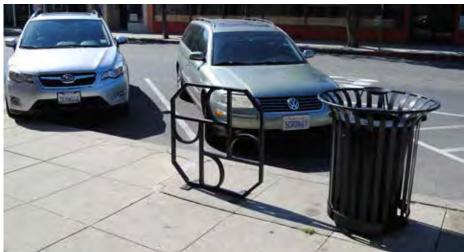


Figure 3-4, Bicycle parking locations

Bicycle Network Policies

- 3.2.9 Design and build recommended bicycle facilities:
 - Park Avenue Bike Route (north of 3rd Street), Bike Lane (south of 3rd Street)
 - Palomares Street Bike Lane
 - Monterey Avenue Bike Route
 - 2nd Street Bike Route (east of Gibbs Street), future facility type to be determined (between Gibbs and Towne Avenue)
- 3.2.10 Provide additional bicycle racks at the following locations:
 - Pomona Transit Center
 - Pomona Post Office (580 W. Monterey Avenue)
 - The Glass House/Fox Theater
 - Western University
- 3.2.11 Consider bicycle lockers and supporting restrooms, showers, and changing facilities at the Pomona Transit Center.





Bicycle parking

3.2.5 Pedestrian Network

Creating an inviting and safe environment for walking in Downtown Pomona is an important community goal. The pedestrian network is addressed in this section and is complemented by public realm recommendations under Section 3.3 Placemaking, that describe streetscape design and amenities to support pedestrian activity.

Sidewalks and crosswalks are the primary facilities for pedestrian travel in the Specific Plan area. An inventory of existing gaps in the sidewalk network is included in the Pomona Active Transportation Plan (ATP) and is graphically shown in **Figure 3-5**, **Pedestrian Network**. Existing improvements include current sidewalks and the pedestrian bridge over the railroad tracks by the Pomona Transit Center just west of N. Garey Avenue.

Crosswalks are typically located at intersection locations controlled by stop signs and traffic signals, additionally shown in **Figure 3-5**, **Pedestrian Network**. Existing crosswalks are present by law at all intersections, whether marked or unmarked, unless the pedestrian crossing is specifically prohibited. Application of marked crosswalks is inconsistent and includes a combination of unmarked, marked, textured and ladder style crosswalk pavement markings. Consistent with the recommendations in the Pomona ATP, improvements such as zebra-stripe crosswalks, advanced stop bars, high-visibility continental or diagonal crosswalk marking patterns should aim to increase pedestrian visibility and driver awareness. Typical intersection improvements are illustrated in **Figure 3-6**, **Typical Intersection Improvements**.



Sidewalks to accommodate pedestrians, dining, and bicycle parking



Landscaped medians



Figure 3-5, Pedestrian Network, Source: Urban Crossroads

Typical Intersection Improvements

Potential enhancements to intersections include the following elements to be further tailored based on individual intersection needs:

- Zebra-stripe crosswalks to all crossings
- · Pedestrian countdown signals to all crossing
- Audio signals to all crossings
- Advanced stop bars
- · Advanced yield markings
- Advanced yield signs
- Bulbouts



Figure 3-6, Typical Intersection Improvements , Source: Urban Crossroads

Pedestrian Network Policies

- 3.2.12 Construct missing sidewalk segments through the City's Capital Improvement Program and/or developer requirements.
- 3.2.13 Provide sidewalks with adequate widths (10-foot minimum on primary pedestrian corridors, 8-foot minimum on secondary pedestrian corridors and narrower sidewalks where it is required to be consistent with historic sidewalks) to allow for pedestrian travel, dining and auto transitional zones.
- 3.2.14 Upgrade crosswalks treatments with zebra-stripe crosswalks, advanced stop bars, high-visibility continental crosswalk marking patterns. Priority intersections include:
 - Pomona Transit Center
 - 2nd Street and Garey Avenue
 - 3rd Street and Garey Avenue
 - 4th Street and Garey Avenue
- 3.2.15 Provide pedestrian links and continuity between public gathering areas and other areas of interest, parking areas and attractions.

3.2.6 Transit Network

Public transportation in Downtown Pomona consists of both bus and rail service and can be characterized as the most transit-rich environment in the city. The Pomona Transit Center, centrally located within the Specific Plan area, provides a unique opportunity to promote transit-oriented development and land uses that benefit from access to regional transit service. The overall transit goal is to provide greater opportunity for residents and visitors to use public transit options by incorporating appropriate infrastructure in development plans to support transit as well as amenities to make these options appealing to users.

Regarding bus transit service, Foothill Transit, Omnitrans, and Los Angeles County MTA buses all operate within the City. Since the Pomona Transit Center is located centrally within Downtown Pomona, the Specific Plan area enjoys access to regional transit service from approximately ten routes according to schedules from Foothill Transit and Omnitrans. MTA operates two bus routes within Pomona connecting Downtown Pomona, Cal Poly Pomona, and the Lanterman Developmental Center with Downtown Los Angles, Union Station, Cal State Los Angeles, and Mount San Antonio College. Refer to Figure 3-7, Transit Network, illustrating existing transit routes and bus stops within the Specific Plan area.

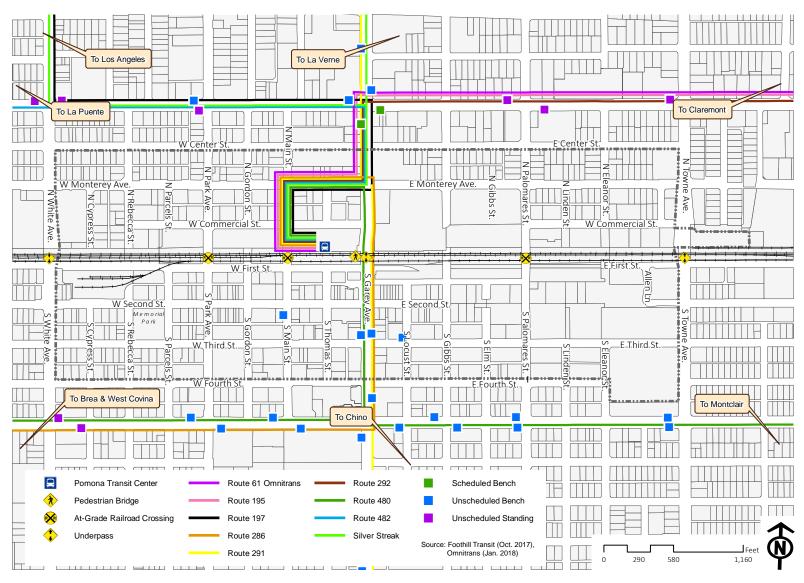


Figure 3-7, Transit Network, Source: Urban Crossroads

In addition to the transit services described above, the Pomona Valley Transportation Authority offers transportation programs to the area including the Pomona Valley Get About program for seniors and disabled and the Pomona Group Service which is open to the general public and available to groups of six or more rides. Further, Access Paratransit provides regional paratransit services in Los Angeles County for all locations within ¾ of a mile of an active bus line. This includes the entire City of Pomona.

In addition to the bus service described above, the Pomona Transit Center is served by rail service from Metrolink and Amtrak. The Metrolink Riverside line serves the Pomona Transit Center with destinations between Downtown Los Angeles and Riverside. Six trains stop in Pomona daily, with trains arriving every 30 minutes during the peak hour and peak direction, but there is virtually no "off-peak" service. No weekend trains currently operate on the Riverside line. Amtrak trains that stop at the Pomona Transit Center include the Sunset Limited and the Texas Eagle with service three times weekly in each direction. Providing rail service between Los Angeles and New Orleans, and between Los Angeles and Chicago, respectively.

Future transit infrastructure and service expansions are envisioned in the City's General Plan and by the transit agencies that serve the City. The MTA Long Range Transportation Plan envisions an expanded regional transit system that may help sustain the increasing mode share of transit in Pomona. By providing greater frequencies and better transit access to regional destinations, transit use may become a viable option for travel. Better coordination between bus and existing Metrolink rail service also offers the opportunity to improve the appeal of transit in Pomona.

Planning for future transit services for Downtown Pomona requires an understanding of existing services, those that are planned, and those that could be developed, and integrating those services with the anticipated land use patterns. The primary role of the Specific Plan is to establish the planned land use/development pattern in support of existing and future transit services. As transit service planning and implementation is ultimately the responsibility of transit agencies and other regional planning organizations, a detailed transit plan is beyond the scope of the Specific Plan. However, identifying goals and strategies for encouraging transit use and mandating the consideration or inclusion of significant elements is appropriate for the Specific Plan and can help regional service providers maximize service potential within Downtown Pomona.

The following section provides guidance in developing services that integrate existing and new transit services with redevelopment in the Specific Plan area. Transit recommendations for adjacent Holt Avenue (where future Bus Rapid Transit may be made available) are addressed in the Pomona Corridors Specific Plan. Flexibility in implementation is provided in order to adapt to changes in potential services and development scenarios.



Transit Network Policies

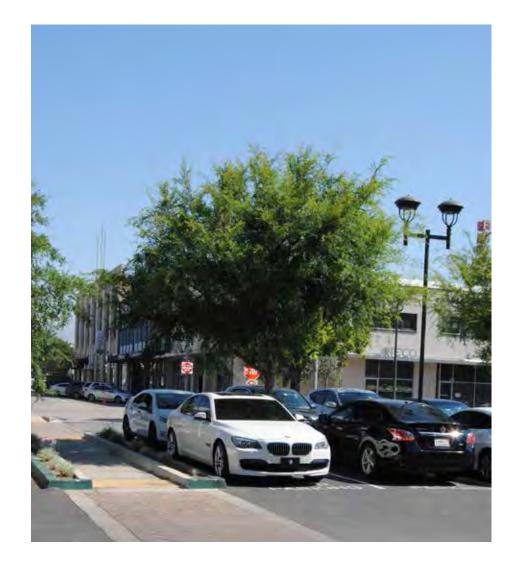
- 3.2.16 Incorporate transit stops and amenities into the public frontage areas along primary corridors (such as Garey Avenue) and within development along secondary arterials such as White Avenue and Towne Avenue. Certain transit services, such as bus rapid transit or express service may necessitate including a very limited number of stops within the mainline section of Holt or Garey Avenue to meet performance specifications for the individual service point at the Downtown Metrolink station and transit center.
- 3.2.17 Consider expansion of the Pomona Transit Center should increase service require it.
- 3.2.18 Continue to coordinate with local transit agencies to maintain a high level of bus (or equivalent) service running along Garey Avenue and connecting to the North Metrolink station and the Downtown Pomona Transit Center.

3.2.7 Parking Strategies

Parking Setting

Parking for the Downtown is either self-contained or located within parking facilities that serve groups of buildings. The majority of the Downtown area south of First Street, including Western University, has shared parking provided in public parking facilities with additional on-street parking accommodations. Chapter 2, Land Use and Urban form, establishes parking requirements for individual developments, while this section describes public parking strategies. The Specific Plan allows for the conversion of parking lots in this area into structured parking where appropriate.

Figure 3-8 depicts existing public parking lots in and near the Specific Plan area. Additional spaces can be provided by increasing the heights of existing parking structures or converting parking lots to structured parking as the parking demand grows over time.



Future Considerations

Changing economic and demographic conditions will continue to influence future parking needs. Self-driving cars, parking guidance, peer-to-peer vehicle sharing, telecommuting, transportation network companies and other forward-looking technologies will continue to shape future parking demands.

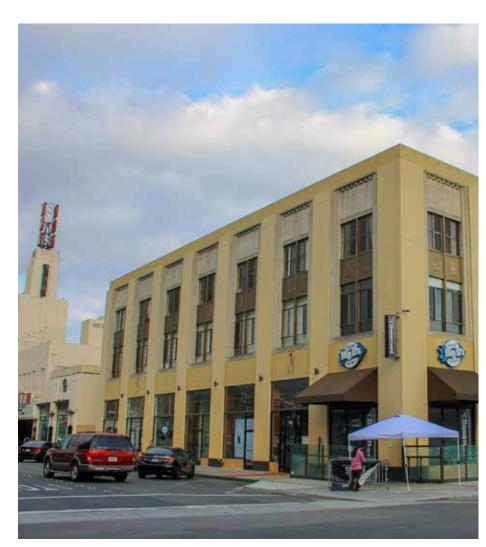
The recent rise in popularity of Transportation Network Companies (TNCs) such as Uber and Lyft, has the potential to significantly change the future parking demands in the Downtown. The TNC model, in which users can request rides through their smartphones, provides on-demand transportation in personal vehicles with direct point-to-point trips. The use rather than ownership model of TNCs may over time substantially reduce the need for dedicated parking. In addition, on-demand transportation increases the need to promote accessible drop-off and pick-up areas at key locations in the Downtown.

TNCs represent only one technology that is changing traditional parking demands. Other smart parking technologies will allow the City to better manage parking availability, occupancy and enforcement. Using smart vehicle detection sensors, smart parking management systems are available allowing the City to actively manage parking resources real-time. This smart parking management approach streamlines enforcement and reduces traffic congestion related to vehicles searching for available parking.

The City will prepare a parking management plan to create a long-term strategy that will address future parking needs within the Specific Plan area. The parking management plan should also prioritize TNC accessible drop-off and pick-up areas. To facilitate better utilization of existing parking facilities, parking strategies should include wayfinding improvements and application of smart parking technologies to inform users of available spaces and pricing.



Figure 3-8, Public Parking, Source: Urban Crossroads



Street parking

Parking Policies

- 3.2.19 Prepare parking management strategy for Downtown.
- 3.2.20 Incorporate improved wayfinding and technology to optimize utilizatino of existing parking supply.
- 3.2.21 Periodically evaluate funding sources and mechanisms for expanded/updated parking facilities or parking structures.
- 3.2.22 Evaluate parking lot design and parking lot management to ensure continued security for users.
- 3.2.23 Pursue the expansion of on-street parking throughout Downtown Pomona.

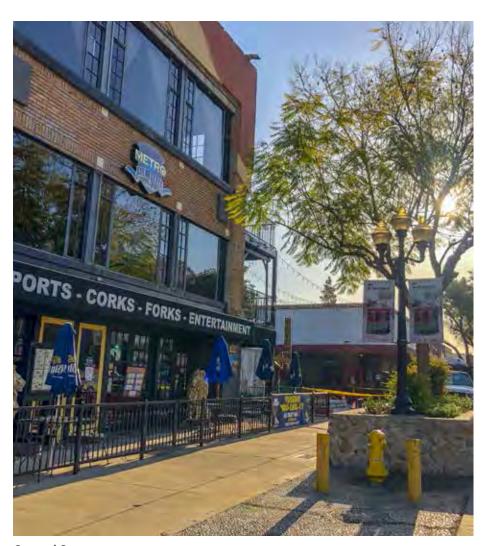
3.3 Placemaking

3.3.1 Vision

Downtown Pomona possesses a distinct identity within the larger landscape of the City of Pomona. It will serve as a diverse and dense hub of activity for businesses, families, and visitors that also embraces and encourages public art opportunities and preserves historic and cultural resources. Defining what makes Downtown Pomona unique is inherently a collective activity that involves the feedback and participation of many stakeholders. To that end, the process of placemaking, which establishes connections between people, community assets and public space, serves as a useful vehicle in establishing Downtown Pomona as a unique destination where people want to work, live, and play.

The vision of this section is twofold: first, to illustrate existing pathways and recommend new ones for stakeholders designing and pursuing placemaking strategies in Downtown Pomona; second, to provide guidelines and standards regarding historic preservation, temporary uses, privately owned public open space, and streetscapes in the Downtown Pomona Specific Plan.





The Downtown Pomona Specific Plan area is generally geographically divided into northern and southern sections by the Union Pacific Railroad right-of-way and into eastern and western sections by Garey Avenue. Historically, a relationship between bridging these portions of Downtown has not been established. In exploring placemaking strategies, the City of Pomona and stakeholders should consider improving key north-south connections, including at-grade crossings along Park Street, Main Street, and Palomares Street, underpass crossings at Garey, White, and Towne Avenues, and east-west connections running between Garey, White, and Towne Avenues, including Center, Monterey, First, Second, Third, and Fourth Streets. Activating these connections will allow for a more cohesive, connected Downtown Pomona.

Second Street

3.3.2 Downtown Placemaking Standards & Guidelines

Historic Landmarks

Downtown Pomona includes several designated historic landmarks and the Edison Historic District and in general is home to a wealth of historic and cultural features with distinct architecture and streetscapes. Future development of mixed-use, high density uses must remain sensitive to existing historic resources and should be designed to highlight those attributes when feasible. Parcels located within the Downtown Pomona Specific Plan will be subject to the standards and guidelines of Pomona Zoning Code Section .5809-13, Historic Preservation. Figure 3-9, Historic Areas and Landmark Structures illustrates historic resources in and near the Specific Plan.



Fox Theater



Renovated YMCA

Historic preservation within Downtown Pomona should consider the following guidelines:

- 1. Proposed demolition of pre-1945 structures that are identified as "contributing" in the City of Pomona historic resources surveys may require the preparation of a historic survey conducted by a certified architectural historian, at the discretion of the Development Services Director.
- 2. The City of Pomona should consult local historic groups and other interested stakeholders in establishing a list of historic resources that should be incorporated into a broader placemaking vision for Downtown Pomona.
- The design of new development located in Downtown Pomona neighborhoods with existing, historic housing stock should consider incorporating the architectural principles of these houses.
- 4. Whenever possible, existing historic landmarks within Downtown Pomona should be opened up to the general public through events and activities that increase community exposure to unique landmarks and history.
- 5. Placemaking strategies involving historic resources should be diverse and inclusive and represent the history and heritage of all people.



Figure 3-9, Historic Areas and Landmark Structures

Art in Public Places Program

The City of Pomona's Art in Public Places Program, adopted in 2011, encourages the development of public art citywide. As indicated in the Art in Public Places Manual, the mission of this program is to "increase citizens' appreciation of art, to improve the quality of life, and to enhance and identify Pomona as a unique community." The following principles should guide public art strategies in Downtown Pomona:

- 1. The vision of the Arts Colony which established Downtown Pomona as a place for the cultural arts extends to the entire boundary of the Downtown Pomona Specific Plan.
- 2. The Cultural Arts Commission and its Citizen's Advisory Committee, in conjunction with Development Services, should establish a strategic vision for implementing public art within Downtown Pomona, including identifying appropriate sites for the placement of murals and other public art.
- 3. The Art in Public Places program should increase understanding and enjoyment of art in Downtown Pomona and encourage interaction among residents, business owners, art patrons, and visitors across all socioeconomic and ethnic groups.
- 4. Downtown Pomona should serve as a destination that actively encourages an environment for artistic creativity and expression, which includes events, activities, and other initiatives.
- 5. All future development projects in Downtown Pomona should be encouraged to pursue public art on site that is accessible and visible. Development projects that also include a privately owned public space requirement should be designed in coordination with any Art in Public Places requirement.

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Figure 4-6, Examples of Public Art

Public Property and Right-of-Way

The City of Pomona currently provides two pathways for stakeholders to activate public spaces in the city. The first is a Special Event Permit, issued by the Community Services Department for any event or activity taking place on public property or in the public right-of-way. The second is an Outdoor Dining Permit, issued by the Public Works Department for any restaurant that wishes to provide outdoor dining in the public right-of-way.

In an effort to further activate the public realm in Downtown Pomona and provide stakeholders additional opportunities to make use of unutilized or neglected spaces, the following uses are encouraged across all public property and public right-of-way within the Downtown Pomona Specific Plan boundary:

- **1. Parklets.** A mini-park within the street right-of-way which extends the sidewalk, usually created by eliminating one or two parking spaces. May include a variety of uses including seating, tables, planting, and play elements.
- **2. Temporary Pedestrian Plazas.** Temporary public open space created by closing a portion of the street to vehicular traffic and introducing street furniture and programming.
- **3. Street Furniture.** Chairs, tables, and benches placed on sidewalks or other public right-of-way. Placement can be temporary or permanent, fixed or movable, depending on location and use. Businesses with active storefronts, such as restaurants or cafes, can sometimes place seats and tables in the public realm outside of their business as part of an outdoor dining permit.
- **4. Active Alleys.** An alley comfortable for pedestrians and people on bicycles, as well as for vehicles. A green alley is built with materials and features that capture stormwater, reduce heat, and provide a more comfortable and aesthetically pleasing environment.
- 5. **Pop Up Music.** Concerts and other musical activities to enliven spaces for transit riders and pedestrians.

The City should consider implementing a series of **Public Realm Activation Guides and Applications for the Downtown Pomona Specific Plan** to establish new pathways for stakeholders to activate public property and public right-of-way.









Figure 4-5, Primary Streetscape Furnishings

Privately Owned Public Open Space (POPOS)

In addition to the City's parks and recreations facilities, privately owned public open space (POPOS) may be provided through development projects pursuing a development incentive (as defined in Urban Form—Incentives, Section 2.2.5.4.3). The following guidelines shall apply to all cases in which a development includes POPOS:

- 1. POPOS refers to spaces dedicated to public use and enjoyment that are also owned and maintained by private property owners.
- 2. POPOS should be open and inviting and visible to the general public, with clear sightlines and paths connecting to the public right-of-way.
- 3. POPOS should be located at the same elevation as the sidewalk and enhance pedestrian circulation, or, if at a higher elevation, provide visible access from the ground floor.
- 4. POPOS should be safe and secure with easily accessible paths for ingress and egress and sufficient lighting.
- 5. POPOS should be comfortable and engaging through the provision of amenities such as well-designed, comfortable seating, greenery and trees, and open areas.

Streetscapes

Streetscapes refer to the natural and built fabric of the street with a focus on the design quality of the public realm and its visual effect. The following components should be considered in the design and implementation of streetscape improvements carried out by Public Works. If streetscape improvements are proposed by stakeholders, it shall be subject to review and approval of the Public Works Department in coordination with Development Services.

Lighting & Furnishings

Site furnishings create a more comfortable and inviting pedestrian experience, and lighting improves safety and pedestrian circulation. Proposed lighting and furnishings should incorporate the following principles:

- 1. Outdoor furniture and hardscape such as trellises, planters, benches, bicycle racks, trash receptacles, etc., shall be provided within the public realm, and shall be coordinated in terms of colors, materials and design to reinforce a cohesive project identity.
- 2. Outdoor furniture shall be provided in public gathering spaces to encourage pedestrian activity. Design of materials and colors of outdoor furniture and hardscape elements shall complement surrounding building architecture.

- 3. Illumination shall be provided for entrances of buildings, in storefronts, parking areas, and pedestrian walkways to promote convenient, safe and easy identification.
- 4. Pedestrian-scale decorative light fixtures are encouraged. Lighting should highlight architectural elements and details.
- 5. The use of a variety of different lighting types and intensities at entries, public gathering areas, parking lots and other areas where evening activity occurs will help to create an exciting nighttime environment.
- 6. Public street lights should follow the adopted antique street light policy.

Tree Palette

A major unifying element of the Downtown design theme is the pattern of street trees. As part of the development of a City of Pomona Street Tree Planting Plan, the Public Works Department, in conjunction with the Parks and Recreation Commission, should develop specific tree palettes for major and minor streets in Downtown Pomona.

Intersections & Crossings

Areas where multiple modes of travel mix, including pedestrians, cyclists, and automobiles. In designing or improving intersections and crossings in Downtown Pomona, the following examples should be considered, among other best practices:

- 1. Provide crosswalk markings that identify a safe path for pedestrians to cross while also signaling to motorists to prepare to yield or stop.
- 2. Provide decorative pavement materials using different colors, textures, and patterns to distinguish crosswalk markings in different environments, which also helps to identify unique places within Downtown.
- 3. Provided raised crosswalks where feasible to provide pedestrian walkways at the same level of sidewalks, and which allows for traffic calming.
- 4. Provide corner bulbouts as curb extensions at intersection corners which improves vehicular turning movement safety and shortens pedestrian crossings.
- 5. Provide bicycle pavement markings to alert bicyclists and motorists of each other's presence as they approach intersections.

Wayfinding

Public signage in the right-of-way that helps orient pedestrians, cyclists, and automobiles around Downtown Pomona. Wayfinding initiatives should incorporate the following principles:

- 1. Signage should "explain the environment" around an individual and provide clear and accurate information to navigate.
- 2. Signage should only contain information that is relevant to the context of the area and not overburden an individual with unnecessary information.
- 3. Signage should be provided for local resident and transit rider experiences by providing information on "first mile-last mile" solutions, such as proximity to Metrolink, a bus station, bicycle stations, or other modes of public transit, as well as historic and cultural landmarks.
- 4. Signage should also be provided for visitor and tourist experiences by providing information on businesses, parking areas, and notable landmarks.
- 5. Signage should relate in design to adjacent streetscape improvements such as site furnishings and lighting.

The City of Pomona should consider implementing a Complete Streets Manual for the Downtown Pomona Specific Plan which provides strategies for implementing best practices in streetscape design. Two examples of streetscapes that should be considered for improvements include First Street between White and Towne Avenues, which lacks critical pedestrian and bicycling networks and building frontages facing the railroad, and Second Street between South Main and South Gibbs, which reflects the original 1962 pedestrian mall and the historic origins of the Arts Colony.

3.4.1 Introduction

Infrastructure and public services are essential to the success of the Downtown Pomona Specific Plan. Topics include water, wastewater, storm drainage, and dry utilities as well as public services including police protection, fire protection, schools, parks and recreation, and solid waste. This chapter describes infrastructure and public services and relevant policies that support the Specific Plan area.

3.4.2 Water

Water service within the City is provided by the Pomona Public Works Department with supply sources including groundwater, treated surface water, imported water and water conservation. Groundwater is the primary source of water supply for the City, supplemented by imported water from the Metropolitan Water District of Southern California (Metropolitan) via the Three Valleys Municipal Water District (TVMWD), local surface water from the San Antonia and Evey Canyon watersheds, and a small percentage of non-potable, recycled water.

Regarding water conservation, the City is signatory to the Memorandum of Understanding (MOU) regarding Urban Water Conservation in California and is a member of the California Urban Water Conservation Council (CUWCC). As a signatory, the City has pledged to implement 14 conservation Best Management Practices (BMPs). These BMPs include: water survey programs, residential

plumbing retrofit, system water audits, landscape conservation programs, public and school education programs, conservation pricing, and a low-flush toilet replacement program. In addition to the BMPs that are part of the MOU, the City plans to implement additional conservation programs to further discourage water waste and overuse.

Water Policies

- 3.4.1 Reduce water consumption through a program of water conservation measures, such as use of recycled water, water-saving features, and drought-tolerant landscaping.
- 3.4.2 Require that water saving devices, as required by the California Plumbing Code, be installed in all residential, commercial, industrial and institutional facilities within the Specific Plan area. Such devices are capable of reducing the amount of water used indoors, resulting in substantial wastewater flow reductions.
- 3.4.3 Explore opportunities to provide recycled water to irrigate all parks, plazas, community facilities, linear parks, landscaped front yards and buffer zones. Where reasonable and feasible, commercial uses, schools and non-residential mixed-use developments should provide with dual plumbing to enable indoor recycled water use for non-potable uses to the extent feasible.

The City's 2010 Urban Water Management Plan indicates that, due to its existing water rights and groundwater storage agreements, an adequate supply of water should be available, with normal conservation efforts, for projected demands through 2035. As individual development projects are initiated within the Specific Plan area, localized improvements to the water system would be required as part of project design and review.



Recycled water used for irrigation

3.4.3 Sanitary Sewer

Wastewater service within Pomona is provided by the City's Public Works Department, and collected wastewater is treated by the Los Angeles County Sanitation Districts (LACSD). The LACSD is a conglomerate of 23 independent special districts responsible for collecting, treating and recycling, and disposing of wastewater and industrial wastes. The City's wastewater is treated and disposed of at the LACSD Pomona Water Reclamation Plant located at 295 Humane Way in Pomona.

LACSD has adopted ordinances over time establishing legal mechanisms to protect and finance operations and facilities, including the Industrial Wastewater Discharge Permit, Connection Fee, and Discharge Programs. The Connection Fee Program requires all new uses of the LACSD sewerage system, as well as existing uses that significantly increase the quantity of strength of their wastewater discharge, to pay their fair share of costs for providing additional conveyance, treatment, and disposal facilities.

Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR. Existing treatment plant capacity and infrastructure can accommodate planned future growth. As individual development projects are initiated within the Specific Plan area, localized improvements to the wastewater system would be required as part of the project design and review.

The Specific Plan area, as well as the majority of the City of Pomona, lies within the eastern boundary of the San Gabriel River Watershed. Because Pomona is largely built out, water quality is mainly affected by the City's storm drain system which is comprised of gutters and storm drains designed to prevent flooding by moving rain water away from city streets and directly into local rivers (which flow to the ocean). The southern portion of the City (which includes the Specific Plan area) discharges to San Antonio Creek/Chino Creek which is a tributary to the Santa Ana River. Pomona does not lie within a 100- or 500-year floodplain, as delineated by the Federal Emergency Management Agency (FEMA).

The primary regulatory control for the protection of water quality is the Federal National Pollution Discharge Elimination System (NPDES) permit administered by the State Water Resources Control Board (SWRCB). Further, Pomona is within the jurisdiction of the Los Angeles Regional Water Quality Control Board (RWQCB). The SWRCB and Los Angeles RWQCB establish requirements prescribing the quality of point sources of discharge and establish water quality objectives. In addition, the City of Pomona protects water resources by reducing the impact of pollutants from urban runoff through implementation of its Storm Water Pollution Prevention Program as required by the NPDES permit program. Further, the Pomona Municipal Code, Chapter 18 (Environment), Division 3 (Discharge Regulations and Requirements) sets forth regulations associated with stormwater.



Flow-through planters collect stormwater and filter sediment and pollutants

Development envisioned under the Specific Plan is primarily infill in nature. Therefore, only an incremental expansion in the quantity of net new impervious surfaces is expected as a part of future development. In compliance with federal, state, regional and local requirements, contractors constructing new development or redevelopment projects are required to implement best management practices (BMPs). As individual development projects are initiated within the Specific Plan area, localized improvements to addressing storm water and water quality would be required as part of the project design and review.

Storm Drainage Policies

- 3.4.4 Provide storm drain infrastructure to adequately serve new development and meet City standards.
- 3.4.5 Coordinate site design and building systems to remove pollutants from stormwater runoff prior to discharge to the storm drain system or receiving waters by creating a project that attempts to mimic a natural hydrologic regime through the use of site design and treatment strategies including, but not limited to:
 - Landscaped Setback Area/Open Space
 - Detention Basin/Pond
 - Retention Basin/Pond
 - Vegetated Swale/Strip
 - Rain Garden
 - Landscaped Tree Well
 - Grated Tree Well
 - Permeable/Pervious Paving
 - Waterscape
 - Rainwater Harvesting/Vault

3.4.5 Dry Utilities

Electricity Service

Southern California Edison provides electric service in the Specific Plan area. The current transmission and distribution system is anticipated to meet the demands of the proposed land uses. The City requires undergrounding of utilities pursuant to Chapter 62 – Utilities of the Municipal Code (Section 62-31. – Undergrounding of Lines).

Natural Gas

The Southern California Gas Company provides natural gas service for the Specific Plan area. SoCal Gas has facilities in the area and service would be provided in accordance with SoCal Gas policies and extension rules on file with the California Public Utilities Commission at the time contractual arrangements are made (project-by-project basis). In addition, energy conservation measures required by applicable energy conservation regulations (e.g. the California Green Building Code) will improve efficiency of long-term energy use for the Specific Plan area.

Telecommunications

Telephone service is provided by Verizon and internet/cable service is provided by Frontier Communications in the Specific Plan area. The existing system is anticipated to meet the demands of the proposed land uses.

3.4.6 Police Protection

The City of Pomona Police Department (PPD) provides local police services for Pomona. The PPD provides services in crime investigation, offender apprehension, community awareness programs, traffic control, and other services. Seven facilities provide police services in Pomona. Of these facilities, three provide first-response services and include the Main Station at 490 W. Mission Boulevard, the Traffic Bureau at 100 W. Commercial, and the Aero Bureau at 1905 McKinley. Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR where policies are provided to reduce crime and ensure law enforcement as the City experiences future growth.

Police Policies

3.4.6 Consider public safety in the design of new development and public spaces by promoting the integration of Crime Prevention Through Environmental Design (CPTED) principles for new development and public spaces. The three key principles of CPTED are: Natural Surveillance, Natural Access Control, and Territorial Reinforcement.

3.4.7 Fire Protection

Fire protection services are provided by the Los Angeles County Fire Department (LACFD) and include fire, rescue, and hazardous materials prevention and emergency services. The LACFD uses national guidelines for response time targets which consist of five minutes for the first arriving unit for fires and basic life support and eight minutes for advanced life support (paramedic) in urban areas. Pomona is part of the LACFD Division III and the nearest fire department facility is Station 181 – Division and Battalion Headquarters located at 590 S. Park Avenue near the southern boundary of the Specific Plan area.

Any new development that would occur with the Specific Plan area would be required to comply with all applicable federal, state, and local regulations governing the provision of fire protection services, including adequate fire access, fire flows, and number of hydrants. LACFD has adopted the 2016 California Fire Code with certain local amendments. These project-specific requirements include construction standards in new structures and remodels, road widths and configurations designed to accommodate the passage of fire trucks and engines, and requirements for minimum fire flow rates for water mains.

3.4.8 Schools

The Specific Plan area is located within the Pomona Unified School District (PUSD) boundaries. Existing facilities include elementary, middle and high schools as well as opportunities for higher-level education. In the Specific Plan area, additional educational institutions range from The School of Arts and Enterprise (a charter school located at 375 S. Main Street and 925 N. Garey Avenue) and Western University of Health Sciences located in the southeastern quadrant of the Specific Plan area. Implementation of the potential development allowed under the Specific Plan was addressed in the 2014 Pomona General Plan and EIR where policies are provided to address potential increases in student generation, including payment of the State Department of Education Development fee in conformance with AB 2926, which was enacted to provide for school facilities construction, improvements, and expansion.

3.4.9 Parks and Recreation

The City of Pomona owns and operates a number of developed parks within the City. Two parks are located in the Specific Plan area: Centennial Park and Memorial Park. Pomona Civic Center Park is located just south of the Specific Plan area and several planned additions to this facility are identified in the General Plan Open Space Network.

As indicated in the 2014 General Plan, the City is striving to achieve a target of three acres of parkland per 1,000 residents. Municipal Code Section 70.69 Parks and Recreation Improvement requires dedication of park lands or payment of in-lieu fees, or a combination thereof, for the construction of new dwelling units. Funds collected are solely for the construction of park improvements. In addition, the Parks and Recreation Division in the Community Services Department provides recreation programs and services to the citizens of Pomona. As the City is largely built out, creative approaches are needed to increase available park space and improve the existing parkland system.

Policies provided below will support expansion of parks, recreation and public spaces within the Specific Plan area. Please refer to Section 3.3, Placemaking for additional discussion and policies regarding park space in the Downtown area.



Thomas Plaza

Parks and Recreation Policies

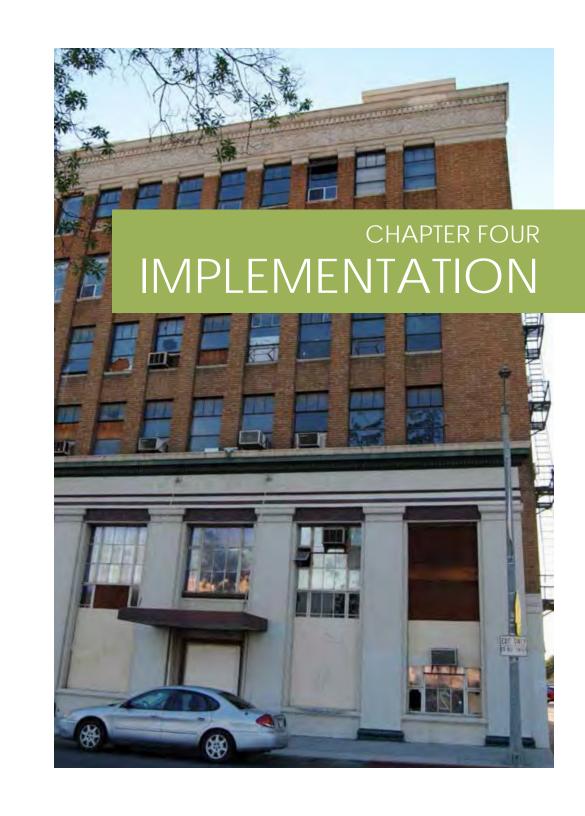
- 3.4.7 Support the development network of multi-purpose facilities to house visual and performing arts activities, such as exhibition, studio/class, performance, and theater/lecture space.
- 3.4.8 Consider allowing in-lieu fee payments based on the size of a project in higher intensity districts (such as Downtown) or for small developments.
- 3.4.9 Explore opportunities for adaptive reuse of historic or existing structures for performing and visual arts venues, as exemplified by the recently restored Fox Theater.

3.4.10 Solid Waste

The City of Pomona Public Works Department provides trash, recycling and special pickup services for the single-family residences, duplexes, triplexes and some fourplexes. Community cleanups coordinated by the City provide periodic neighborhood drop-off of bulky items for residents only. Franchise commercial waste haulers provide trash and recycling service for most fourplexes, all apartments with five or more units, as well as all commercial, governmental, and industrial facilities. Commercial haulers have non-exclusive, competitive franchise agreements and currently authorized franchise haulers include Athens Services, Burrtec Waste Industries, Valley Vista Services, and Waste Management.

Solid Waste Policies

3.4.10 All new development shall participate to the maximum extent practical in solid waste source reduction and diversion programs.



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4.1 Introduction

This chapter outlines the implementation program for the Specific Plan. The implementation program includes the following components:

- An overview of the ways the Downtown Specific Plan can facilitate economic investment in the plan area;
- A review of existing economic conditions influencing current and future development potentials in Downtown Pomona;
- A summary of the types of new development that are likely to be successful in the Downtown area;
- Recommended high-priority action items for Specific Plan implementation;
- · A review of potential funding sources/mechanisms for implementation of key Specific Plan initiatives;
- An overview of the State of California's Community Revitalization and Investment Authority legislation. This legislation provides local jurisdictions with many of the development tools previously associated with redevelopment agencies and may be a useful mechanism for implementation of the Specific Plan; and
- Identification of methods to leverage Opportunity Zone legislation and incentives.

Note that within this chapter there are numerous strategies and other entries that are universally applicable to many different communities. However, the ways in which they are ultimately adapted to Pomona will be somewhat unique and will depend on numerous specific conditions including available resources, the interrelationship of various programs, preferences of the applicable stakeholder groups, and other considerations. The City should consider implementing Architectural Design Incentives for the Downtown Pomona Specific Plan to promote and preserve the historical integrity of existing visible streetscape.



4.2 Attracting Private Investment and Providing Public Benefits

Effective implementation of a development plan typically involves both the public and private sectors. Whereas development of envisioned land uses is often "kick started" by various public sector initiatives, the ultimate goal of this type of planning effort is to attract desired private investment. Broadly speaking, there are two major ways that a municipality can facilitate private development:

- By creating a "conducive development environment" that is consistent with prevailing market demand for various land uses. This may include the following types of actions or policies:
 - Zoning, design guidelines, etc. that are responsive to market needs at the individual-establishment level, while maintaining the overall character of the area that preserves and enhances its general marketability;
 - b. Information about the concepts, intent, etc. of the plan area to prospective investors/tenants;
 - Streamlined permitting and entitlement processes (i.e., minimizing the need for discretionary approval processes);
 - d. Area-wide "amenity" investments, including landscape and streetscape improvements;

- e. Focusing of available resources to address critical issues (e.g., homelessness) that would otherwise serve as impediments to attracting businesses and consumers to the plan area;
- f. Marketing programs to enhance the area's identity and recognition among consumers;
- g. Marketing efforts to attract targeted development and business types;
- h. Coordinating or supporting placemaking initiatives at the neighborhood level, as part of an overall branding effort; and
- i. Clearinghouse roles (e.g., coordination of funding resources and dissemination of information related to investment in the plan area).
- 2. By providing focused development support to area businesses, property owners, and key development projects. This can involve the following types of initiatives:
 - a. Facilitating financing of area-wide and/or project specific infrastructure requirements;
 - b. Facilitating the use of supplemental financing mechanisms to support needed services (e.g., security, landscape maintenance, etc.) in the plan area;
 - c. Assembling land for key "opportunity sites"; and
 - d. Providing loans and/or grants for various business and property improvement purposes such as building façade renovations.

4.3 Economic Conditions Influencing Development Potentials in Downtown Pomona

Downtown Pomona has made impressive progress since the original DPSP was adopted in 1994. Through growth of the Arts Colony – which was a prominent theme in the 1994 Plan – the area has developed a notable market niche for "nightlife" activities (attracting over one million visitors per year) and has experienced visible reinvestment in historic buildings. These accomplishments notwithstanding, the area has a number of remaining challenges to address. The area's key strengths and challenges from a market perspective are identified below.

4.3.1 Strengths

- Excellent "institutional" anchors, including:
 - Western University of Health Sciences a graduate school for the health professions offering degrees in osteopathic medicine, dental medicine, optometry, podiatric medicine, nursing, physician assistant studies, physical therapy, pharmacy, biomedical sciences and veterinary medicine;
 - ° The School of Arts and Enterprise a public, charter high school focused on visual and performing arts;
 - ° The Pomona Transit Center located in the heart of downtown (100 W. First Street), serving as a multi-modal Metrolink, Amtrak, Foothill Transit, and Omnitrans station;
 - Fox Theater an architectural landmark that was completely renovated in 2009 (after being mostly closed since 1977) to provide a state-of-the-art facility for concerts, cinema, performances, and parties.
- An eclectic mix of arts/entertainment oriented businesses, which have helped to position the downtown as a regional destination for nightlife and special events.
- An organized business community, spearheaded by the Downtown Pomona Owners Association (DPOA), which effectively coordinates marketing, special events, enhanced security, and public realm beautification for a portion of the downtown area.
- An emerging "urban residential" market (most notably the highly successful Monterey Station apartment community located at the southeast corner of Holt Avenue and Garey Avenue).
- Additional planned private investment (residential, movie theater, retail), which will potentially serve as an effective catalyst for accelerated development in the plan area.

4.3.2 Challenges

- The area's "daytime economy" is limited, making it difficult to attract and retain quality retail businesses.
- The historic Antique Row, which also featured prominently in the 1994 DPSP, has long struggled to remain viable and is currently in a state of decline.
- Local business stakeholders recognize a longstanding need to diversify the retail tenant mix (to complement the evening-oriented arts/ entertainment businesses).
- The limited (but gradually growing) housing stock in the immediate vicinity of the downtown limits the demographic potential for supporting additional neighborhood-oriented retail uses.
- Despite the progress made in terms of revitalization of key buildings, overall beautification (including almost-instant graffiti removal), and improved public safety, much remains to be done on the cleanup of vacant and underutilized properties and addressing larger social issues such as homelessness.

This section of the Implementation chapter describes several potential funding sources and financing mechanisms that may be important tools for implementation of the Specific Plan.

4.3.3 Reauthorization and Expansion of Property and Business Improvement District (PBID)

In California, there are two separate laws that authorize the formation of a Business Improvement District (BID): The Parking and Business Improvement Area Law of 1989 and the Property and Business Improvement District Law of 1994. Both laws enable a city, county, or joint powers authority (made up of cities and/ or counties only) to establish a BID and levy annual assessments on businesses within its boundaries. Improvements which may be financed include parking facilities, parks, fountains, benches, trash receptacles, street lighting, and decorations. Services that may be financed include promotion of public events, furnishing music in public places and promotion of tourism. In addition to the above, the 1994 Act also allows financing of streets, rehabilitation or removal of existing structures, and security facilities and equipment.

The 1989 Act allows financing of marketing and economic development, and various supplemental municipal services such as security and sanitation. Neither law allows bonds to be issued by the BIDs.

Downtown Pomona has existing Property and Business Improvement District (PBID). The Downtown Pomona Owners Association (DPOA) and its associated funding mechanism – the PBID – have been instrumental in the significant progress the Downtown has made since the adoption of the original DPSP in 1994. Critical activities spearheaded by DPOA (and funded by the PBID) include:

- Marketing and special events;
- Maintenance of public areas (pocket parks, streetscape, etc.);
- · A highly-effective graffiti removal program; and
- Private security patrols (to supplement City police services).

The existing PBID was established in 2004 and will need to be reauthorized by 2019. The existing PBID covers only a portion of the Specific Plan area. The DPOA is currently investigating options for expanding the area covered by the PBID as part of the reauthorization process. Alternatively, a new (separate) PBID could be established to cover the Downtown areas not included in the existing PBID. In either case, expanded PBID coverage will be a crucial element of implementing the updated DPSP.

4.3.4 Enhanced Infrastructure Financing Districts

Senate Bill No. 628, creating enhanced infrastructure financing districts (EIFDs) took effect on January 1, 2015. EIFDs are designed to fund infrastructure development and community revitalization, through issuing bonds, establishing a public financing authority, and adopting an infrastructure financing plan. EIFDs include a provision for using tax increment financing. A broad range of community development and revitalization projects can be funded through this mechanism. Given the limitations on PBIDs related to funding major infrastructure (since BIDs cannot issue bond), an EIFD could provide an important means of funding Downtown infrastructure projects that are beyond the means of the City's General Fund and normal Capital Improvement Program.

Along with the EIFD, the bill allows a city/county to adopt an infrastructure financing plan and issue bonds upon approval of 55% of the voters. As excerpted from the Bill's text, the bond funds would be used to "finance public facilities or other specified projects of communitywide significance, including, but not limited to, brownfield restoration and other environmental mitigation; the development of projects on a former military base; the repayment of the transfer of funds to a military base reuse authority; the acquisition, construction, or rehabilitation of housing for persons of low and moderate income for rent or purchase; the acquisition, construction, or repair of industrial structures for private use; transit priority projects; and projects to implement a sustainable communities strategy." Prior to the adoption of an EIFD and infrastructure financing plan, the bill requires the legislative body to establish a public financing authority, which would be comprised of members of the legislative body of the participating entities and of the public. In addition, the bill would require the adoption of a "resolution of intention" that, among others would include the following:

- District boundaries
- · Description of the proposed public facilities/development that would be financed or assisted by the EIFD
- Need for the EIFD and goals to achieve

The infrastructure financing plan, along with agreement from affected taxing agencies, would provide the mechanism to fund infrastructure projects through tax increment financing. Specifically, the bill authorizes the creation of an EIFD for up to 45 years from the date on which the issuance of bonds is approved. In addition, the bill would authorize a city, county, or special district that contains territory within an EIFD to loan moneys for projects/activities that are listed in the infrastructure financing plan. Finally, the city/county, through its infrastructure financing plan, could choose to allocate any portion of its net available revenue to the EIFD.

4.3.5 What is allowed

An EIFD can finance only public capital facilities or other specified projects of communitywide significance that provide significant benefits to the district or the surrounding community, including, but not limited to, all of the following:

- 1. Highways, interchanges, ramps and bridges, arterial streets, parking facilities, and transit facilities.
- 2. Sewage treatment and water reclamation plants and interceptor pipes.
- 3. Facilities for the collection and treatment of water for urban uses.
- 4. Flood control levees and dams, retention basins, and drainage channels.
- 5. Child care facilities.
- 6. Libraries.
- 7. Parks, recreational facilities, and open space.
- 8. Facilities for the transfer and disposal of solid waste, including transfer stations and vehicles.
- 9. Brownfield restoration and other environmental mitigation.
- 10. The development of projects on a former military base.

- 11. The repayment of the transfer of funds to a military base reuse authority pursuant to Section 67851 that occurred on or after the creation of the district.
- 12. The acquisition, construction, or rehabilitation of housing for persons of low and moderate income, as defined in Section 50093 of the Health and Safety Code, for rent or purchase.
- 13. Acquisition, construction, or repair of industrial structures for private use
- 14. Transit priority projects, as defined in Section 21155 of the Public Resources Code, that are located within a transit priority project area.
- 15. Projects that implement a sustainable communities strategy, when the State Air Resources Board, pursuant to Chapter 2.5 (commencing with Section 65080) of Division 2 of Title 7, has accepted a metropolitan planning organization's determination that the sustainable communities strategy or the alternative planning strategy would, if implemented, achieve the greenhouse gas emission reduction targets.

4.3.6 What is not allowed

Restrictions on EIFDs include the following:

- An EIFD can only be created after the City/County after the specified conditions related to the wind down of the former redevelopment agency (if one was created by the city/ county) have been satisfied.
- 2. Cannot divert property tax revenue from schools or from any non-consenting tax entity any taxing entity contributing tax increment must consent and opt into the EIFD.
- 3. A district may not finance routine maintenance, repair work, or the costs of an ongoing operation or providing services of any kind.

Category	EIFD	IFD
Created	Adopt infrastructure financing plan - city/county legislative body	2/3 vote of the electorate
Issue of bonds based on tax increment financing	55% vote of the electorate	2/3 vote of the electorate
Eligible projects	Broader range – e.g., transit, lmi housing, sustainable communities strategies, environmental remediation, etc., in addition traditional infrastructure projects	Public capital facilities (more limited)
Financing authority	Cad dedicate more revenue sources to the funding of infrastructure – e.g., can devote portions of their periodic distributions from the Redevelopment Property Tax Trust Fund, funds received from the Mello-Roos Community Facilities Act of 1982 and funds from the Benefit Assessment Act of 1982, among others	More limited
Longevity	45 years from date bonds are issued or loans are approved	30 years from initial formation
Eminent Domain	Can exercise eminent domain powers under the Polanco Redevelopment Act associated with the cleanup of environmentally impacted properties	Not available

Source: Holland & Knight. http://www.hklaw.com/Publications/Enhanced-Infrastructure-Financing-Districts-SB-628-Beall-11-12-2014/ (accessed on 5/20/15)

4.3.7 Landscaping and Light Act

The Landscaping and Lighting Act of 1972 created a flexible tool used by local government agencies to pay for landscaping, lighting and other improvements and services in public areas. As a form of benefit assessment, it is based on the concept of assessing only those properties that benefit from improvements financed, either directly, or indirectly through increased property values. Because it is considered a benefit assessment, a 1972 Act assessment is not subject to Proposition 13 limitations. The City of Pomona has several existing Landscaping and Lighting Districts (including, in the vicinity of the Specific Plan area, the South Garey Maintenance District).

4.3.8 Local Funding Opportunities Administered by the Los Angeles County Metropolitan Transit Authority.

The Specific Plan's focus on transit and transportation infrastructure provides an important nexus to potential funding opportunities administered by the Los Angeles County Metropolitan Transit Authority. Key funding programs are summarized in Table 6-2, Los Angeles County Metropolitan Transit Authority Key Funding Programs.

Table 4-2, Los Angeles County Metropolitan Transit Authority Key Funding Programs						
Funding Source	Eligible Uses Potentially Relevant to DPSP					
Proposition A – voter enacted (1980) ½-cent sales tax in Los Angeles County.	Exclusively to benefit public transit (including fare subsidy programs).					
DPSP-relevant component:						
 Local return program (25%) funds are distributed to cities on a per capita basis for public transit uses. 						
Proposition C – voter enacted (1990) ½-cent sales tax in Los Angeles County.	Capital costs of transit centers including facilities, access improvements, landscaping, bike lockers, rehabilitation, and other amenities.					
DPSP-relevant components:	Commuter bikeways and bike lanes; fare subsidy programs.					
 Commuter Rail/Transit Centers/Park-n-Ride (10% allocated on a call for projects basis) 	eominater sinemays and sine lanes, rare sassia, programs.					
 Local return program (20%) funds are distributed to cities on a per capita basis 						
Measure R – voter enacted (2008) ½-cent sales tax in Los Angeles County.	Major street resurfacing, rehabilitation and reconstruction; pothole					
DPSP-relevant component:	repair; left turn signals; bikeways; pedestrian improvements; streetscapes; signal synchronization; and transit.					
Local return program (15%) funds are distributed to cities on a per capita basis for public transit uses.						
Measure R – voter enacted (2016) ½-cent sales tax in Los Angeles County (increasing to one cent in 2039, at the expiration of Measure R).	Major street resurfacing, rehabilitation and reconstruction; pothole repair; left turn signals; bikeways; pedestrian improvements;					
DPSP-relevant component:	streetscapes; signal synchronization; and transit.					
Local return program (16%) funds are distributed to cities on a per capita basis for public transit uses.						
Transportation Development Act (TDA) – a Local Transportation Fund (LTF) for each county derived from ¼ cent of the 7/25 statewide retail sales tax.	Bicycle and pedestrian facilities.					
DPSP-relevant component: TDA Article 3 (2% allocation for bicycle and pedestrian facilities), distributed to cities based on a population formula.						

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Measure A was passed by Los Angeles County voters in November 2016 and will replace funding under Proposition A passed in the 1990's. The measure adds a parcel tax of one-and-a-half cent per square foot of developed property. The measure is projected to raise \$94 million annually. The funds will go directly to cities and local communities to protect, enhance and maintain neighborhood parks, open space, trails, beaches, natural habitat and rivers, creeks and streams.

4.3.9 State of California, Active Transportation Program (ATP)

- Increase the proportion of trips accomplished by biking and walking;
- · Increase safety and mobility for non-motorized users;
- Advance the active transportation efforts of regional agencies to achieve greenhouse gas (GHG) reduction goals;
- Enhance public health;
- Ensure that disadvantaged communities fully share in the benefits of the program; and
- Provide a broad spectrum of projects to benefit many types of active transportation users.

4.3.10 Community Development Block Grant (CDBG) Funds

Federal Community Development Block Grant (CDBG) funds are allocated to local public jurisdictions through HUD to provide opportunities for low-to-moderate income persons and areas. CDBG funds support: 1) development of viable urban communities, 2) preservation of existing housing stock, 3) the elimination of slums and blight, and 4) expanding economic opportunities for eligible persons. The CDBG program requires that funds be targeted at households that earn less than 80% of the area median income. Eligible activities include infrastructure improvements, rehabilitation, economic development programs, code enforcement, administration, fair housing and public services.

4.4 Overview of Community Revitalization and Investment Authorities

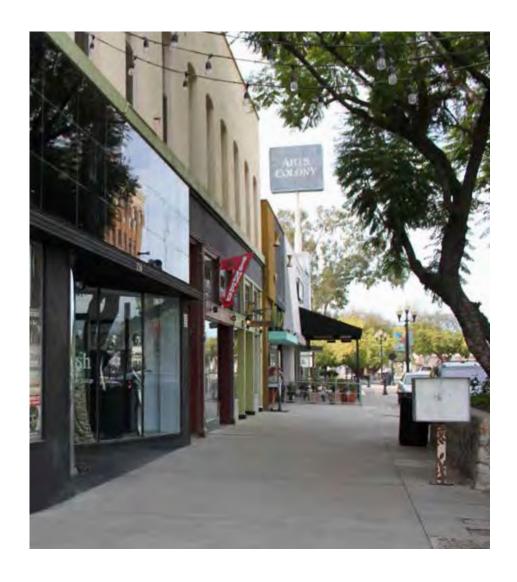
State legislation adopted in 2015 (amended in 2016) authorizes the formation of Community Revitalization and Investment Authorities (CRIAs). The CRIA legislation is intended to allow local government agencies to address the effects of blight, in effect addressing the role that was previously fulfilled by redevelopment agencies in California.

- 1. Acquire and transfer real property. The authority shall retain controls and establish restrictions or covenants running with the land sold or leased for private use for such periods of time and under such conditions as are provided in the plan. The establishment of such controls is a public purpose under the provisions of this part.
- 2. Issue bonds.
- 3. Borrow money, receive grants, or accept financial or other assistance or investment from the state or the federal government or any other public agency or private lending institution for any project or within its area of operation, and may comply with any conditions of the loan or grant.
- 4. Adopt a community revitalization and investment plan.
- 5. Make loans or grants for owners or tenants to improve, rehabilitate, or retrofit buildings or structures within the plan area.
- 6. Construct foundations, platforms, and other like structural forms necessary for the provision or utilization of air rights sites for buildings to be used for residential, commercial industrial, or other uses contemplated by the revitalization plan.
- 7. Provide direct assistance to businesses within the plan area in connection with new or existing facilities for industrial or manufacturing uses, except as specified in this division.

4.4.1 Criteria

One of the key general provisions is the requirement to implement a community revitalization plan within a community revitalization and investment area. As required by the Bill, the area is required to meet the following conditions:

- 1. An annual median household income that is less than 80 percent of the statewide annual median income.
- 2. Three of the following four conditions:
 - A. Nonseasonal unemployment that is at least 3 percent higher than statewide median unemployment, as defined by the report on labor market information published by the Employment Development Department in January of the year in which the community revitalization plan is prepared.
 - B. Crime rates that are 5 percent higher than the statewide median crime rate, as defined by the most recent annual report of the Criminal Justice Statistics Center within the Department of Justice, when data is available on the California Attorney General's Internet Web site.
 - C. Deteriorated or inadequate infrastructure such as streets, sidewalks, water supply, sewer treatment or processing, and parks.
 - D. Deteriorated commercial or residential structures.



4.4.2 Overview

As excerpted from the Bill's summary, the CRIA legislation authorizes a local agency to form a CRIA "within a community revitalization and investment area, as defined, to carry out provisions of the Community Redevelopment Law in that area for purposes related to, among other things, infrastructure, affordable housing, and economic revitalization. The bill would provide for the financing of these activities by, among other things, the issuance of bonds serviced by tax increment revenues, and would require the authority to adopt a community revitalization and investment plan for the community revitalization and investment area that includes elements describing and governing revitalization activities. The bill would also provide for periodic audits by the Controller. The bill would also require the Department of Housing and Community Development, advised by an advisory committee appointed by the Director of Housing and Community Development, to periodically review the calculation of surplus housing under these provisions. The bill would require certain funds allocated to the authority to be deposited into a separate Low and Moderate Income Housing Fund and used by the authority for the purposes of increasing, improving, and preserving the community's supply, as specified. The bill would, if an authority failed to expend or encumber surplus funds in the Low and Moderate Income Housing Fund, require those funds to be disbursed towards housing needs. The bill would require an authority to make relocation provisions for persons displaced by a plan and replace certain dwelling units that are destroyed or removed as part of a plan. The bill would authorize an authority to acquire interests in real property and exercise the power of eminent domain, as specified."

4.4.3 Authorized Actions

As indicated in Bill, an authorized Authority can do the following:

- 1. Provide funding to rehabilitate, repair, upgrade, or construct infrastructure.
- 2. Provide for low- and moderate-income housing.
- 3. Remedy or remove a release of hazardous substances pursuant to the Polanco Redevelopment Act.
- 4. Provide for seismic retrofits of existing buildings in accordance with all applicable laws and regulations.

4.4.4 Key Differences from Community Redevelopment Agencies

Although Authorities created under this law will function in a similar role compared to redevelopment agencies, which were previously authorized under Community Redevelopment Law, some of the key differences include the following:

- Public Members. Along with the three members of a legislative body of the city, county, or joint city-county that created the authority, the authority is required to include two public members who live or work in the plan area. Supporters contend that this requirement improves transparency.
- Low- and moderate-income housing. A minimum of 25% of the funds in a plan area are required to be set aside for affordable housing, compared to 20% under the previous redevelopment system.
- Tax increment. Previous redevelopment law "shifted" property tax increment from special districts (such as schools, fire districts, etc.) in effected plan areas. The existing Bill requires cities, counties, and special districts to agree to contribute a share of property tax increment to fund proposed improvements.
- Dissolution Mechanism. At 10-year intervals the public could stop the Authority's operations by filing a petition and putting the plan to a public vote.

4.5 Leveraging the City of Pomona's Opportunity Zones

The Opportunity Zone (OZ) program – a new federal tax incentive for real estate and business investment in economically-distressed areas – represents a significant and time-sensitive opportunity to catalyze implementation of the Downtown Specific Plan. The program has the potential to be a "game changer" in terms of enhancing the investment attractiveness of designated areas; virtually all development/business types are eligible to utilize the benefits (only "sin" businesses are excluded).

4.5.1 What are Opportunity Zones?

The Tax Cuts and Jobs Act of 2017 established Opportunity Zones as a mechanism to provide substantial tax incentives for investment in designated census tracts. Investments made by individuals through special funds in these zones are allowed to defer or eliminate federal taxes on capital gains.

An Opportunity Zone is an economically-distressed community where new investments, under certain conditions, may be eligible for preferential tax treatment. Localities qualify as Opportunity Zones if they have been nominated for that designation by the State and that nomination has been certified by the Secretary of the U.S. Treasury and the Internal Revenue Service.

An Opportunity Zone provides a new tool for investors, fund managers and communities by utilizing privately sourced funds into eligible economic development and community reinvestment projects.

4.5.2 Opportunity Zones in the City of Pomona and the Downtown Specific Plan Area

A total of 879 of the designated Opportunity Zones (census tracts) are in California. Of the 879 zones statewide, four census tracks (covering approximately 1,608 acres) are in the City of Pomona. One of Pomona's four census tracts overlaps into the Downtown Specific Plan Area.

City's Potential Roles in Maximizing Benefits of Opportunity Zones In defining potential City actions to maximize the benefits of the OZ program to Pomona, it is important to recognize the following aspects of the program:

- Local governments are not directly Involved in OZ administration. The actual OZ program, being part of the federal tax code, is administered by the U.S. Department of the Treasury. Participation in the program (by developers and investors) does not require interface with local governments. In some communities, investments in Opportunity Zones are already occurring absent any local effort to promote them. However, to fully maximize the advantages associated with having Opportunity Zones in a community, local jurisdictions can play a vital role in raising awareness of the program and, especially, in aggressively promoting investment-ready development and business opportunities in their designated zones.
- The clock is running on the availability of OZ tax benefits. The designated OZ census tracts were certified by the U.S. Treasury on April 9, 2018. Their certification is valid for ten years (i.e., through 2028), regardless of when qualifying investments are made. Investments initiated in 2019 will enjoy the full 10 years of tax benefits from the program. Investments made in subsequent years will be able to utilize the tax benefits for a reduced number of years. From the community's perspective, it will be critical to act fast in order to fully leverage the program benefits (in terms of attracting development and investment), Appropriately, the City of Pomona has already begun promoting its Opportunity Zones. The time-sensitivity of the program applies not only to marketing of opportunities but also to the need for the City to facilitate expedited development approvals in order for projects to be developed in time to take maximum advantage of tax benefits.
- "Opportunity Funds" are efficient vehicles for attracting investors. Private "Opportunity Funds" have been established throughout the U.S. in anticipation of the implementation of the OZ program. These funds provide a mechanism for qualified investors to pursue roll-overs of investment gains (e.g., from stocks or real estate) into Opportunity Zone investments, thereby taking advantage of the OZ program in terms of reductions in capital gains taxes. Opportunity Funds are not only efficient from the investor's standpoint, but also serve as a convenient marketing target for communities attempting to attracting investment to their zones.

4.5.3 OZ Action Plan for Pomona Downtown Specific Plan

The following specific steps are recommended for near-term action as part of the City's implementation of the Downtown Specific Plan:

- Showcase market-feasible, investment-ready development opportunities. Within the OZ census tract that overlaps with the Downtown Specific Plan area, identify specific development and investment opportunities than can be immediately marketed to prospective developers, investors and Opportunity Funds. The Downtown opportunities should be included in a larger list of OZ opportunities in the City (i.e., the list should also include development opportunities for the three census tracts not in the Downtown area).
- Fast-track approval processes. For the listed opportunity sites/projects, determine current entitlement status and identify any additional measures the City could take to expedite/ streamline development projects on these sites.
- 3. Prioritize infrastructure investments. Within the Downtown Specific Plan area, prioritize infrastructure and other public investments that would most directly enhance developability of prospective OZ projects/sites.

- 4. Prepare marketing materials. Market-based opportunities and supporting City policies/ programs should be succinctly summarized in print and online media. As part of the marketing materials, the City should consider developing an "OZ Prospectus" which makes a specific case (via a simple pro forma financial analysis) for investing in Pomona's OZ-designated census tracts.
- 5. Network through OppSites.com. This evolving platform provides a forum for communities with Opportunity Zones to connect and interact with prospective developers and investors. The City of Pomona should consider listing its development opportunities on this website.

4.6 Implementation Action Plan

The vision and goals presented in the Specific Plan are supported by actions outlined in Table 6.3, Implementation Action Plan Matrix. The action plan provides a summary of the key Specific Plan recommendations and is presented in a table format. The table is organized by topic, listing regulatory actions in the first section and improvements and programs in the second section. For each action item, the lead and support department at the City of Pomona is listed under the responsibility heading followed by an indication of priority of the action and funding sources that would be most closely associated with the activity. Outside agencies and organizations are identified as well where appropriate. It is intended that this list of public actions be incorporated in the City's annual budget setting process and included in the Capital Improvement Plan for execution.

Table 4-3, Implementation Action Plan Matrix					
Action Itom		Responsibility		Potential Funding	
Action Item	Timing	Lead	Support/Coordination	Sources	

Timing:

- 1 = Early Implementation (1-3 years)
- 2 = Near Term Implementation (4-10 years)
- 3 = Long Term Implementation (10-20 years)

Funding Source Acronyms:

PBID - Property and Business Improvement District (existing and/or expanded).

Metro – various local funding opportunities administered by the Los Angeles County Metropolitan Transit Authority, including Proposition A, Proposition C, Measure R, Measure M, and the Transportation Development Act (TDA).

EIFD - potential Enhanced Infrastructure Financing District.

LLD - Landscape and Lighting District (new district or existing South Garey Maintenance District).

ATP – local funding opportunities through the State's Active Transportation Program.

VPD - Vehicle Parking District (existing district focused on parking maintenance and capital improvements in the Downtown business area.

CRIA – potential Community Revitalization and Investment Authority.

Measure A – Los Angeles County funding source (successful November 2016 ballot initiative) for local parks and open space.

CDBG - Community Development Block Grant funds.

PUC - Public Utilities Commission.

Regulatory Actions				
Downtown Pomona Specific Plan: Adopt Specific Plan Update		Development Services	Public Works, Economic Development	Metro Grant
Pomona Corridors Specific Plan: Amend Specific Plan map for identified parcels.	1	Development Services		General Fund

Table 4-3, Implementation Action Plan Matrix					
Action Item	Timing	Re	Potential Funding		
Action Item		Lead	Support/Coordination	Sources	
Development Code: Amend Zoning Map for identified parcels.	1	Development Services		General Fund	
Expand Property and Business Improvement District: Expand geographic scope of the existing Property Business Improvement District (PBID) administered by the Downtown Pomona Owners Association (DPOA). As an option to expanding the existing PBID, consider establishing a separate PBID that covers the portions of Downtown not covered by the existing PBID.	1	Economic Development	Development Services, Downtown Business Improvement District, Chamber of Commerce	General Fund, PBID	
Community Revitalization and Investment Authority: Investigate options for establishing a Community Revitalization and Investment Authority (CRIA) as a potential mechanism for economic development initiatives in the Downtown area.	1	Economic Development	PBID	General Fund	
Enhanced Infrastructure Financing District: Investigate options for establishing an Enhanced Infrastructure Financing District (EIFD) as a potential funding mechanism for capital improvements in the Downtown area.	1	Economic Development	PBID	General Fund	
Opportunity Zones: Develop marketing materials and related incentives/support policies to maximize developer/investor interest in the City's OZ-designated census tracts.	1	Economic Development	Development Services	General Fund	
Improvement Projects/Programs					
Public Realm Activation Guides and Applications for the Downtown Pomona Specific Plan: Series of guides to establish new pathways for stakeholders to activate public property and public right-of-way.	3	Development Services	Public Works	General Fund, PBID	

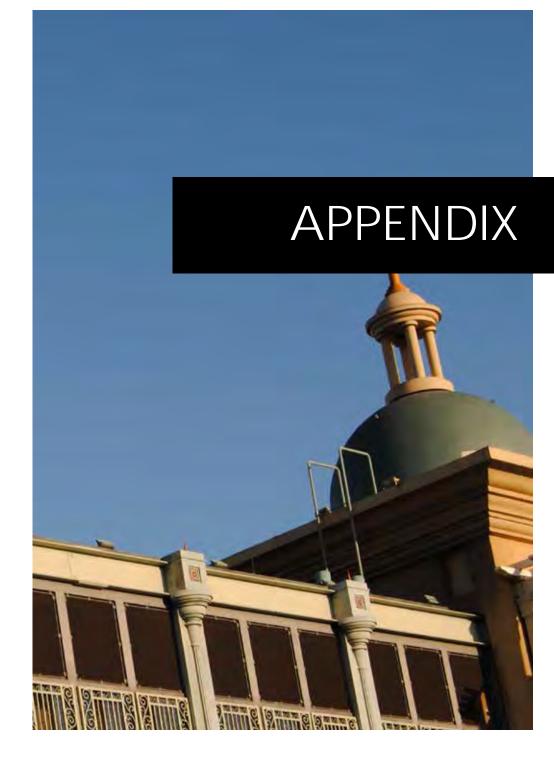
Table 4-3, Implementation Action Plan Matrix					
Action Item		Responsibility		Potential Funding	
Action Item	Timing	Lead	Support/Coordination	Sources	
Landscaped Medians: Install landscape medians on major arterials to reduce scale, including Towne Avenue and White Avenue.	1,2	Public Works	Development Services	Metro, EIFD, LLD	
Ridesharing Service: Develop program for coordination and location of ridesharing services (such as Uber, Lyft, Bird).	1	Public Works	Development Services	PBID	
Transit/Bus Stops: Continue to coordinate with transit providers to ensure adequate number and location of transit stops.	Ongoing	Public Works, Development Services	Omnitrans	Omnitrans, Metro	
Pomona Transit Center Expansion: Evaluate expansion of the Pomona Transit Center to adjacent properties if warranted by service increases.	Ongoing	Public Works	Metrolink, Omnitrans	Metro, Omnitrans, EIFD	
Bicycle Facilities: Implement bicycle facilities along Second Street and Palomares Street.	1, 2	Public Works	Development Services	ATP, Metro	
Bicycle Parking: Install bicycle parking facilities as recommended in the Active Transportation Plan for the following locations: Pomona Transit Center, Pomona Post Office, The Glass House/Fox Theater, Western University, Pomona Public Library/Superior Courthouse.	1, 2	Public Works	Development Services	ATP, Metro, Private Development	
Intersections: Install pedestrian improvements the following intersections pursuant to Active Transportation Plan recommendations: Pomona Transit Center, 2nd Street/Garey Avenue, 3rd Street/Garey Avenue, 4th Street/Garey Avenue.	1, 2	Public Works		ATP, Metro, Private Development, EIFD	

Table 4-3, Implementation Action Plan Matrix					
Action Item		Re	Potential Funding		
Action Item	Timing	Lead	Support/Coordination	Sources	
Sidewalks: Install missing sidewalks segments along N. Parcels Street, N. Gibbs Street, N. Linden Street, N. Eleanor Street, W. Commercial Street, E. Commercial Street, S. Elm Street.	1, 2	Public Works	Development Services	EIFD, Private Development	
Parking Management Strategy: Prepare parking management Strategy for Downtown including incorporation of updated wayfinding and newer technologies to optimize utilization of parking supply.	1	Public Works	Development Services	VPD, PBID	
Metrolink Parking/Amenities: Coordinate with Metrolink to ensure adequate parking supply and supporting amenities such as information kiosks and directories.	Ongoing	Public Works, Economic Development	Metrolink	Metro	
Transit Technology: Implement transit operator and transit user ITS and technology advancements to improve transit/rideshare operations and user convenience	2	Omnitrans, Metrolink	Public Works	Omnitrans, Metro	
Parking Lot Design/Management: Evaluate parking lot design and parking lot management to ensure continued security for users.	Ongoing	Public Works		VPD, PBID	
Parking Funding: Periodically evaluate funding mechanisms for expanded/updated parking facilities and parking structures.	Ongoing	Public Works		VPD, PBID	
Branding Package: Develop a branding package including message, themes, and graphics	1	Economic Development	Chamber of Commerce	General Fund, PBID, CDBG, CRIA	
Wayfinding and Gateway Program: Prepare a comprehensive wayfinding and gateway program serving motorists, bicyclists and pedestrians	1	Economic Development, Public Works	Development Services	PBID, PVD	

Table 4-3, Implementation Action Plan Matrix					
Action Item		Responsibility		Potential Funding	
Action item	Timing	Lead	Support/Coordination	Sources	
Primary Gateway – Garey Avenue Entry Corridor: Design and install gateway treatment.	1	Economic Development, Public Works	Development Services	EIFD, PBID	
Secondary Gateways: Design and install gateway treatment at Garey Avenue/Monterey Avenue, Towne Avenue/Third Street, Second Street/White Avenue.	2	Economic Development, Public Works	Development Services	EIFD, PBID	
Pedestrian Street - Second Street: Design and install refurbishment improvements between Park Avenue and Gibbs Street.	1	Public Works	Development Services	EIFD, PBID	
Pedestrian Street - Locust Street: Between Mission Boulevard and Second Street design and install pedestrian-oriented improvements with limited vehicular access.	3	Public Works	Development Services	EIFD, PBID	
Memorial Park: Design and install enhancements to existing park.	2	Community Services/ Parks and Recreation	Development Services	Measure A, EIFD	
New Park: Design and install new park west of Eleanor Street between Third Street and Second Street.	3	Community Services/ Parks and Recreation	Development Services	Measure A, EIFD	
Streetscape Amenities: In areas adjacent to sidewalks provide sidewalks, bicycle racks, benches and other street furniture.	1, 2	Public Works	Development Services	Private Development, Metro	

Table 4-3, Implementation Action Plan Matrix						
Action Item	Timing	Re	Potential Funding			
Action Item		Lead	Support/Coordination	Sources		
Specific Street Tree Palette : Develop specific tree palettes for major and minor streets in Downtown Pomona as part of the development of a City of Pomona Street Tree Planting Plan.	1	Public Works	Community Services/ Parks and Recreation	General Fund, Grants, CIP, CRIA, PBID		
Pedestrian Lighting: Install pedestrian-scale lighting.	2	Public Works	Development Services	EIFD, Private Development, LLD		
Business Incentives: Develop a menu of business incentives such as streamlined application review or waive/defer fees	2	Economic Development, Development Services	Business Improvement District, Chamber of Commerce	General Fund, CDBG, CRIA		
Underground Utilities: Replace existing overhead utilities with underground utilities where practical.	3	Public Works	Utility Providers	PUC (Rule 20), Private Development		
Public Realm Activation Guides : Develop a series of public realm activation guides and applications to establish new pathways for stakeholders to activate public property and public right-of-way.	1, 2	Development Services	Public Works	General Fund, CRIA, PBID, Private Development		
Complete Streets Manual: Prepare a manual with strategies for implementing best practices in streetscape design.	2, 3	Development Services	Public Works	General Fund, CRIA, PBID, Private Development		

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A.1 Definitions

Land Uses

Residential Uses

Single-Unit Dwelling, Detached. A dwelling unit that is designed for occupancy by one household with private yards on all sides. This classification includes individual manufactured housing units.

Single-Unit Dwelling, Attached. A dwelling unit that is designed for occupancy by one household located on a separate lot from any other unit (except a second dwelling unit, where permitted), and is attached through common walls to one or more dwellings on abutting lots. An attached single-unit dwelling is sometimes called a "townhouse" or a "condominium".

Two-Unit Dwelling. A residential building containing two dwelling units, both of which are located on a single parcel (also referred to as a "duplex" or "two-flat".) The dwelling units are attached and may be located on separate floors or side-by-side.

Multi-Unit Development. Three or more attached or detached dwelling units on a single lot. Types of multi-unit residential include townhouses, multiple detached residential units, and apartment buildings.

Work/Live Units. An area comprised of one or more rooms or floors in a new building or in one originally designed for industrial or commercial occupancy which has been or will be remodeled or altered, which includes cooking space and sanitary facilities, sleeping space, and working space reserved for persons residing therein.

Family Day Care. A day care facility licensed by the State of California, located in a residential unit where resident of the dwelling provides care and supervision for children under the age of 18 for periods of less than 24 hours a day.

Small. A facility that provides care for eight or fewer children, including children who reside at the home and are under the age of 10.

Large. A facility that provides care for seven to 14 children, including children who reside at the home and are under the age of 10.

Residential Care Facilities. A facility licensed by the state of California to provide living accommodations, 24-hour care for persons requiring personal services, supervision, protection, or assistance with daily tasks. Amenities may include shared living quarters, with or without a private bathroom or kitchen facilities. This classification includes those both for and not-for-profit institutions, but excludes Supportive Housing and Transitional Housing.

Small. A facility that is licensed by the state of California to provide care for six or fewer persons.

Large. A facility that is licensed by the state of California to provide care for more than six persons.

Residential Facility, Assisted Living. A facility that provides a combination of housing and supportive services for the elderly or functionally impaired, including personalized assistance, congregate dining, recreational, and social activities. These facilities may include medical services. Examples include assisted living facilities, retirement homes, and retirement communities. These facilities typically consist of individual units or apartments, with or without kitchen facility, and common areas and facilities. The residents in these facilities require varying levels of assistance.

Supportive Housing. Dwelling units with no limit on length of stay, that are occupied by the target population as defined in subdivision (d) of Section 53260 of the California Health and Safety Code, and that are linked to onsite or offsite services that assist the supportive housing resident in retaining the housing, improving his or her health status, and maximizing his or her ability to live and, where possible, work in the community.

Transitional Housing. Transitional housing is housing that has a predetermined end point in time, and operated under a program that requires the termination of assistance, in order to provide another eligible program recipient to the service. The program length is usually no less than six months.

Public/Semi-Public Uses

Colleges and Trade Schools. Institutions of higher education providing curricula of a general, religious, or professional nature, granting degrees and including junior colleges, business and computer schools, management training, technical and trade schools, however excluding personal instructional services such as music lessons.

Convention Center. A facility accommodating large gatherings of people and used for any combinations of the following: conventions, conferences, seminars, product displays, recreational activities, entertainment functions, and accessory uses such as food preparation and service for on-site consumption.

Cultural Institutions. An institution and/or associated facility engaged in activities to promote aesthetic and educational interest among the community that are open to the public on a regular basis. This classification includes performing arts centers for performances and events; spaces for display or preservation of objects of interest in the arts or sciences; libraries; museums; historical sites; aquariums; art galleries; and zoos and botanical gardens, all of which are public or private. This does not include schools or institutions of higher education providing curricula of a general nature.

Day Care Centers. Establishments providing non-medical care for persons on a less than 24-hour basis other than Family Day Care. This classification includes nursery schools, preschools, and day care facilities for children or adults, and any other day care facility licensed by the State of California.

Government Offices. Administrative, clerical, or public contact offices of a government agency, including postal facilities and courts, along with the storage and maintenance of vehicles. This classification excludes corporation yards, equipment service centers, and similar facilities that require maintenance and repair services and storage facilities for related vehicles and equipment (see Public Utilities).

Hospital and Clinics. State-licensed facilities providing medical, surgical, psychiatric, or emergency medical services to sick or injured persons. This classification includes facilities for inpatient or outpatient treatment, including substance-abuse programs, as well as training, research, and administrative services for patients and employees. This classification excludes veterinaries and animal hospitals (see Animal Care, Sales, and Services).

Hospitals. A facility providing medical, psychiatric, or surgical services for sick or injured persons, primarily on an inpatient basis, and including supplementary facilities for outpatient and emergency treatment, diagnostic services, training, research, administration, and services to patients, employees, or visitors. The institutions are to be licensed by the state of California to provide surgical and medical services.

Clinic. A facility providing medical, psychiatric, or surgical service for sick or injured persons exclusively on an outpatient basis including emergency treatment, diagnostic services, administration, and related services to patients who are not lodged overnight. Services may be available without a prior appointment. This classification includes licensed facilities offering substance abuse treatment, blood banks and plasma centers, and emergency medical services offered exclusively on an out-patient basis. This classification does not include private medical and dental offices that typically require appointments and are usually smaller scale.

Instructional Services. Establishments that offer specialized programs in personal growth and development such as music, martial arts, vocal, fitness and dancing instruction.

Park and Recreation Facilities. Parks, playgrounds, recreation facilities, trails, wildlife preserves, and related open spaces, all of which are noncommercial. This classification includes playing fields, courts, gymnasiums, swimming pools, picnic facilities, tennis courts, golf courses, and botanical gardens, as well as related food concessions or community centers within the facilities.

Parking Lots and Structures. Surface lots and structures offering parking for a fee when such use is not incidental to another on-site activity.

Performing Arts, Fine Arts, Dinner Theater. Facilities for pubic assembly and group entertainment that provides on-stage theatrical and musical performances.

Public Assembly. A facility for public or private meetings or assembly, including community centers, banquet halls, union halls, meeting halls, religious facilities, and membership organizations. Included in this classification is the use of functionally related facilities for the use of members and attendees such as kitchens, offices, multi-purpose rooms, classrooms and storage.

Public Safety Facilities. Facilities providing public-safety and emergency services, including police and fire protection and emergency medical services, with incidental storage, training and maintenance facilities.

Schools. Facilities for primary or secondary education, including public schools, charter schools, and private and parochial schools having curricula comparable to that required in the public schools of the State of California.

Social Service Facilities. Facilities providing a variety of supportive services for disabled and homeless individuals and other targeted groups on a less than 24-hour basis. Examples of services provided are counseling, meal programs, personal storage lockers, showers, instructional programs, television rooms, and meeting spaces. This classification is distinguished from licensed day care centers (see Day Care Facility), clinics, and emergency shelters providing 24-hour care (see Emergency Shelter).

Tutoring Facilities. Facilities offering academic instruction to individuals or to groups in a classroom setting where an adult accompanies a minor. Facilities where minors are not accompanied by adults are classified as Day Care Centers.

Commercial Uses

Animal Care, Sales, and Services. Retail sales and services related to the boarding, grooming, and care of household pets including:

Animal Sales and Grooming. Retail sales of animals and/or services, including grooming, for animals on a commercial basis. Typical uses include dog bathing and clipping salons, pet grooming shops, and pet stores and shops. This classification excludes dog walking and similar pet care services not carried out at a fixed location, and excludes pet supply stores that do not sell animals or provide on-site animal services.

Veterinary Services. Veterinary services for small animals. This classification allows 24-hour accommodation of animals receiving medical services but does not include kennels.

Art Sales and Services.

Art Gallery. Retail sales and display of pieces of art such as paintings, sculptures, ceramics, crafts and other works of visual art. Art pieces may be made on site.

Artist Studio. Space primarily used as a work room for at least one artist that may be open to the public for demonstrations, classes, and retail sales and may involve the use of hand tools or small mechanical equipment. Examples include acting, dance, design, furniture restoration, film and animation, jewelry, screen printing, painting, photography, recording, sculpture. This does not include artist-related activities that involve heavy mechanical equipment, which are defined under "Artisanal Manufacturing".

Artisanal Manufacturing. Any establishment primarily engaged in on-site production of goods which involves the use of heavy mechanical equipment and the incidental direct sale to consumers of only those goods produced on site. Includes large format ceramic studios, carpentry, decorative ironworks, glassworks, printing, and woodworking.

Artist Cooperative (Co-Op). An organization or association jointly owned or controlled by its membership that provides professional facilities and services for artists, including studios, workshops, equipment, exhibition galleries, and educational resources.

Banks and Financial Institutions. Financial institutions providing retail banking services. This classification includes only those institutions serving walk-in customers or clients, including banks, savings and loan institutions, check-cashing services, and credit unions.

Cinema/Theaters. Any facility for the indoor display of films and motion pictures on single or multiple screens. This classification may include incidental food and beverage service to patrons as well as auditoriums within buildings.

Commercial Entertainment and Recreation. Provision of participant or spectator entertainment to the general public. These classifications may include restaurants, snack bars, and other incidental food and beverage services to patrons.

Indoor Sports and Recreation. Establishments providing predominantly participant sports, indoor amusement and entertainment services conducted within an enclosed building, including coin-operated electronic amusement centers. Typical uses include bowling alleys, billiard parlors, card rooms, health clubs, ice and roller skating rinks, indoor racquetball courts, athletic clubs, and physical fitness centers.

Eating and Drinking Establishments. Businesses primarily engaged in serving prepared food and/or beverages for consumption on or *off the premises.*

Bars/Night Clubs/Lounges. Businesses serving beverages for consumption on the premises as a primary use and including on-sale service of alcohol including beer, wine, and mixed drinks. This use includes micro-breweries where alcoholic beverages are sold and consumed on site and any food service is subordinate to the sale of alcoholic beverages.

Restaurant, Full Service. Restaurants providing food and beverage services to patrons who order and are served while seated and pay after eating. Take-out service may also be provided.

Restaurant, Limited Service. Establishments where food and beverages are consumed on the premises, taken out, or delivered, but where limited table service is provided. This classification includes cafes, cafeterias, coffee shops, delicatessens, fast-food restaurants, sandwich shops, limited-service pizza parlors, self-service restaurants, and snack bars with indoor or outdoor seating for customers. This classification includes bakeries that have tables for on-site consumption of products as well as bakery product plants. It excludes catering services that do not sell food or beverages for on-site consumption.

Restaurant, Take-Out Only. Restaurants where food and beverages are prepared on a customer-demand basis and can be taken out or delivered, but are not consumed on the premises. No seating or other facilities for on-premises dining are provided.

Drive-Through Facility. A motor vehicle drive-through facility which is a commercial building or structure or portion thereof which is designed or used to provide goods or services to the occupants of motor vehicles. It includes, but is not limited to, banks and other financial institutions, fast food establishments, and film deposit/pick-up establishments, but shall not include drive-in movies, service stations, or car-wash operations.

Outdoor Dining & Eating. The use of a public sidewalk and public right-of-way for the placement of tables, chairs, benches, planters, umbrellas, and related items for the purpose of serving food and/or beverages in conjunction with and adjacent to an eating establishment or food court.

Food and Beverage Manufacturing (Small Scale). A small scale food and beverage products manufacturing that must include a retail sales component. It is characterized by local or regional products, specialty or artisanal foods. Examples include small coffee roasters, micro-breweries, micro-distilleries, wine manufacturing, meat or fish processing, and wholesale bakeries.

Hotels. An establishment providing overnight lodging to transient patrons. These establishments may provide additional services, such as conference and meeting rooms, restaurants, bars, or recreation facilities available to guests or to the general public.

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Maintenance and Repair Services. Establishments engaged in the maintenance or repair of office machines, household appliances, furniture, and similar items. This classification excludes maintenance and repair of motor vehicles (see Automotive/Vehicle Sales and Services) and personal apparel (see General Personal Services).

Nurseries and Garden Center. Any establishment(s) primarily engaged in retailing nursery and garden products, such as trees, shrubs, plants, seeds, bulbs, and sod that are predominantly grown elsewhere. These establishments may sell a limited amount of a product they grow themselves. This classification includes commercial and wholesale greenhouses and nurseries offering plants for sale.

Offices. Offices of firms, organizations, or public agencies providing professional, executive, management, administrative or design services, such as accounting, architectural, computer software design, engineering, graphic design, interior design, investment, insurance, and legal offices, excluding banks and savings and loan associations with retail banking services (see Banks and Financial Institutions). This classification also includes offices where medical and dental services are provided by physicians, dentists, chiropractors, acupuncturists, optometrists, and similar medical professionals, including medical/dental laboratories within medical office buildings, but excludes clinics or independent research laboratory facilities (see Research and Development) and hospitals.

Business, Professional, and Technology. Offices of firms, organizations, or agencies providing professional, executive, management, administrative, financial, accounting, or legal services, but excluding those that primarily provide direct services to patrons that visit the office (see Offices, Walk-In Clientele).

Medical and Dental. Offices providing consultation, diagnosis, therapeutic, preventive, or corrective personal-treatment services by doctors and dentists; medical and dental laboratories that see patients; and similar practitioners of medical and healing arts for humans licensed for such practice by the State of California. Incidental medical and/ or dental research within the office is considered part of the office use if it supports the on-site patient services.

Walk-In Clientele. Offices providing direct services to patrons or clients without prior appointments. This use classification includes employment agencies, insurance agent offices, real estate offices, travel agencies, utility company offices, and offices for elected officials. It does not include banks or check-cashing facilities, which are separately classified and regulated (see Banks and Financial Institutions).

General Personal Services. An establishment providing non-medical services to individuals as a primary use, of personal convenience, as opposed to products that are sold to individual consumers, or from/by companies. Personal services include barber and beauty shops, massage establishments, shoe and luggage repair, photographers, laundry and cleaning services and pick-up stations, copying, repair and fitting of clothes, and similar services.

Retail Sales.

Convenience Markets. Establishments primarily engaged in the provision of frequently or recurrently needed small personal items or services for residents within a reasonable walking distance. These include various general retail sales and personal services of an appropriate size and scale to meet the above criteria. Typical uses include neighborhood grocery stores, convenience markets, and drugstores.

Food and Beverage Sales. Retail sales of food and beverages for off-site preparation and consumption. Typical uses include food markets, groceries, liquor stores, and retail bakeries.

General Retail. The retail sale or rental of merchandise. not specifically listed under another use classification. This classification includes retail establishments with 25,000 square feet or less of sales area; including department stores, clothing stores, furniture stores, pet supply stores, small hardware stores (with 10,000 square feet or less of floor area), and businesses retailing the following goods: toys, hobby materials, handcrafted items, jewelry, cameras, photographic supplies and services (including portraiture and retail photo processing), medical supplies and equipment, pharmacies, electronic equipment, sporting goods, kitchen utensils, hardware, appliances, antiques, art galleries, art supplies and services, paint and wallpaper, carpeting and floor covering, office supplies, bicycles, video rental, and new automotive parts and accessories (excluding vehicle service and installation). Retail sales may be combined with other services such as office machine, computer, electronics, and similar small-item repairs.

Private Garden. A private food-producing garden that is accessory to the primary use of the site.

Recharging Station. A public or private parking space that is served by battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive means) to a battery or other energy storage device in an electric vehicle.

Solar Energy System. Roof- or building-mounted solar energy systems attached to the top of a building or structure, generally secured using racking systems that minimize impacts and is mounted either level with the roof or tilted toward the sun. Does not include ground-mounted or freestanding solar energy systems.

Signs

A-Frame (sandwich board) sign: A temporary sign that is moveable and not secured or attached to the ground and that is constructed so as to form an "A" or tent-like shape, hinged or not hinged at the top with each face held at an appropriate distance by a supporting member.

Architectural ledge sign: A sign with individual letters, numbers, symbols, etc. which stand on top of a horizontal projection from a building wall that forms a narrow shelf or architectural projection.

Animated Sign: A sign that uses movement or change of lighting to depict action or create a special effect or scene.

Awning sign: A sign that is printed on or attached to an awning valence.

Building edge sign: Sign placed on the corner of a building that is located at the intersection of two public streets (excluding alleys).

Building frontage: The building elevation that faces a street or major parking area, and that is roughly parallel to it.

Cabinet sign: Sign that contains all the text and/ or logo symbols within a single enclosed cabinet and may be illuminated.

Canopy: A protective roof-like covering, often of canvas, mounted on a frame over a walkway or projecting from a wall over a door or window.

Canopy sign: A sign that is printed on or attached to a canopy typically located above a building entrance or store window.

Captive balloon sign: Any object inflated with hot air or lighter-than-air gas that is tethered to the ground or a structure.

Channel letters: Individually cut three-dimensional letters, numbers or graphics that can be either illuminated or non-illuminated or solid backlit (reverse channel letters).

Commercial signage: Any non-residential signage.

Commercial use: A land use or other activity involving the sale of goods or services.

Directional sign: A freestanding on-site sign designed to direct or guide pedestrians or vehicular traffic that is non-advertising in nature, except for logo.

Electronic message center: A sign with a changing display/ message display: A sign capable of displaying words, symbols, figures or images that can be electronically or mechanically changed by remote or automatic means.

Freestanding banner sign: A sign that is supported by a base that rests on the ground and is not supported by or attached to a building.

Halo or halo-type sign. A sign where the light source is concealed behind an opaque face and the rays of illumination are projected outwards toward the edge of the sign forming a "halo" effect around the exterior of the sign. Light does not pass through the face of a halo sign.

Hanging sign (also under-canopy sign): Hanging signs are similar to projecting signs except that they are suspended below a marquee or under a canopy. Hanging signs are generally smaller than projecting signs due to their lower mounting height.

Historical sign: Sign listed on the DPSP register of historical signs.

Historical structure: Any building or structure located in the DPSP that is designated as a historic landmark, a contributor to a designated historic district, or identified in the DPSP as a potential historic landmark.

Inflatable sign: A sign that is a cold air inflated object, which may be of various shapes, made of flexible fabric, resting on the ground or structure and equipped with a portable blower motor that provides a constant flow of air into the device. Inflatable devices are restrained, attached, or held in place by a cord, rope, cable or similar method. The term inflatable device shall not include any object that contains helium, hot air or a lighter-than-air substance.

LED (*light emitting diode*) *sign*: A sign that uses LEDs to display an image or sign copy.

Marquee sign: A sign that is attached to or made part of any permanent roof-like structure projecting beyond a building or extending along a wall of a building.

Monument sign: A low-profile freestanding sign constructed on or supported solely by a pedestal base, or similar ground structure, and that has a horizontal dimension equal to or greater than its vertical dimension.

Nonconforming sign: A sign lawfully constructed and/or installed before the adoption of this Sign Guide that does not conform to the requirements of the Sign Guide.

Off-site signs: A sign advertising uses, facilities, products, or services, not located or sold on the premise on which the sign is located.

Over-Sized Projecting Sign: Vertically oriented projecting sign with curvilinear silhouettes faced with painted graphics and visible neon. These signs are double-sided and project from a building wall a portion of which may extend above the building roof. Sign copy is stacked vertically rather than horizontally. These types of signs were historically located on buildings built from the 1920s through the 1940s.

Painted wall sign: A sign that is painted or silk screened onto a building wall or freestanding structure.

Pedestrian sign: A sign near street or sidewalk level that is oriented and scaled to the pedestrian rather than the motorist. Projecting and under canopy signs are examples of pedestrian signs.

Permanent banner sign: Vertically oriented sign that hangs from projecting metal brackets.

Pole sign: A freestanding sign that is built or attached to one or more poles or posts that does not meet the requirements of a monument sign.

Principal building façade: The front or main part of a building facing a street or public space, especially one with enhanced architectural treatment.

Projecting sign: A sign attached to a building and extending from the wall at right angles to the wall.

Restaurant menu sign: A freestanding or wall sign that incorporates a menu for the restaurant on the same site.

Sans serif: Term used to describe typeface fonts without serifs. Serifs are small horizontal lines extending from the tops and bottoms of a typeface letter.

Sign: Any device (including, but not limited to, letters, words, numerals, figures, emblems, pictures or any part of combination) that is designed, intended, used or located so as to be attract the attention of the public.

Sign area: Area of the sign included within the outer dimensions of a sign face display area including all portions not part of the necessary supporting structure and excluding frame and supports. For wall signs that do not have a frame or separate background or discernable boundary, the area shall be calculated by measuring the area enclosed by straight lines drawn to enclose the extremities of the letters, numbers, symbols, graphics, symbols, background material, designs, and pictures that area part of the advertisement including those areas between letters, words intended to be read together and any device intended to draw attention to the sign message included in the computation of the sign area.

Temporary Sign. A category of signs intended to be displayed for a limited time.

Tenant directory Sign: The tenant directory sign is used to identify multi-tenant buildings and businesses that may not have direct frontage on a public street.

Under-Canopy Sign. See definition for hanging sign.

Wall Sign: A sign attached to or painted on a wall of a building or structure such that the wall is the supporting structure for or forms the background surface of the sign and which does not project more than 12 inches from the building or structure.

Window Signs: Interior signs that are painted, posted, displayed or etched on an interior transparent surface, including windows or doors, and that are located within two feet of a window.

Window Graphic Signs: Interior signs painted on vinyl or similar material that are attached to the back of a storefront window and that cover no more than 20% of the window.



Downtown Pomona Specific Plan

Finding of Consistency

prepared for

City of Pomona

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prepared with the assistance of

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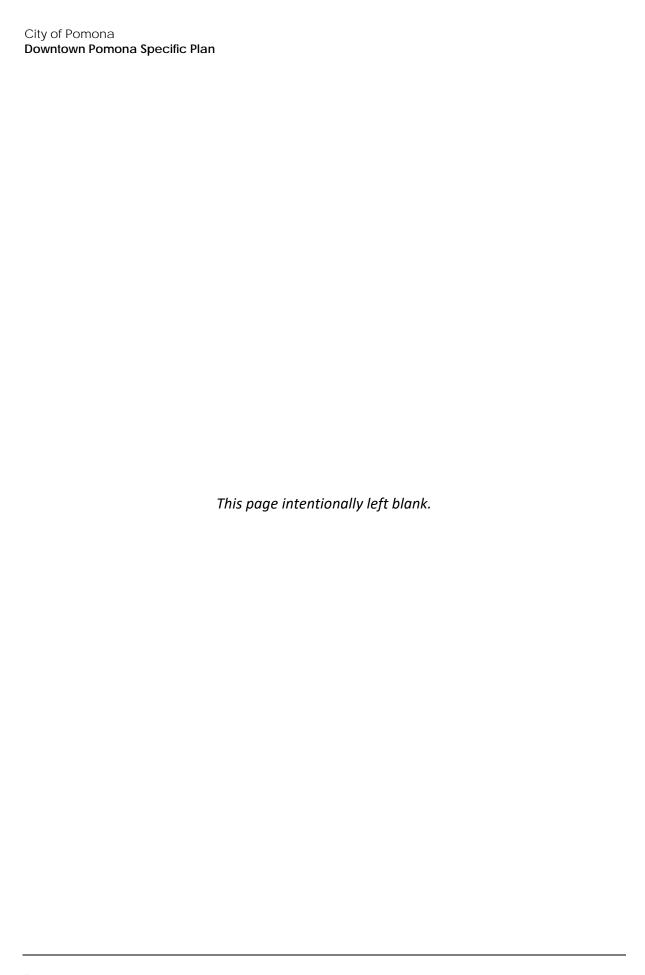


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1 Introduction

This Finding of Consistency evaluates the updated Downtown Pomona Specific Plan's consistency with the Final Environmental Impact Report (hereafter referred to as the 2014 Final EIR) for the City of Pomona General Plan Update. The 2014 Final EIR, which was certified in March 2014, establishes the City's vision for development of the City through the year 2035. This Finding of Consistency has been prepared pursuant to the applicable provisions of §15183 of the California Environmental Quality Act (CEQA) Guidelines.

While the City's General Plan establishes the framework for future development and provides comprehensive policies for the City, the Downtown Pomona Specific Plan (hereafter referred to as "Specific Plan") is a comprehensive document that implements the vision for the Specific Plan area as established by the General Plan. The Specific Plan was originally adopted in 1994 and has had several targeted updates. In 2017, the City of Pomona began a comprehensive update to the Specific Plan, funded by a grant from the Los Angeles County Metropolitan Transportation Authority (Metro) Round 4 Transit Oriented Development (TOD) Planning Grant Funds. The Specific Plan provides for more precise implementation of the goals, objectives, and policies outlined in the General Plan. As described in the updated Specific Plan, Downtown Pomona is centrally located near several freeways and major universities and the Specific Plan area encompasses approximately 232 acres with boundaries at Center Street to the north, Towne Avenue to the east, Fourth Street to the south, and White Avenue to the west (RRM Design Group 2019). Figures 1 and 2 show the regional context and boundaries of the Specific Plan area, respectively.

General Plan Vision

As envisioned in the General Plan, Downtown Pomona is the community's center of business, culture, and living. The Specific Plan area contains destinations such as the Pomona Transit Center and pedestrian bridge, the Arts Colony, Thomas Street, the Fox Theater, and Western University of Health Sciences. The General Plan reinforces Downtown as the mixed-use "heart of the City," and emphasizes available activities and transit service for locals and visitors. The greatest building heights and development intensities will be focused Downtown, which, when paired with the Pomona Transit Center, would encourage an environment where people can live and work without relying on automobiles. Streetscape and infrastructure improvements will enhance the City's walkability and the visual character while new parks and outdoor gathering spaces will make the district more community-oriented. Surrounding the Downtown core, the Downtown neighborhood will consist of a range of contemporary housing types and compatible uses, all concentrated within walking distance of the core's theater, shops, restaurants, cafes, nightlife, and amenities. As infill development proceeds and the region continues to invest in its transit infrastructure, the neighborhood will benefit from an increasing level of service and transit ridership generated by the Pomona Transit Center (RRM Design Group 2019).

Downtown Pomona Specific Plan

The updated Specific Plan would facilitate development in Downtown Pomona consistent with the vision for the area under the General Plan. As shown in Figure 1, the Plan area is in the City's center, where future mixed-use, streetscape, and infrastructure improvements would promote the Plan area as the "heart of the City". The Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height, setbacks) that reflect the area's visual character as envisioned by the City's General Plan. Figure 3 shows the individual district boundaries of the Plan area's four districts: Mixed Use – Central Business District (MU-CBD), Mixed Use – Institutional (MU-I), Mixed Use – High Density Residential (MU-HDR), and Residential – Multifamily (R-MF).

As discussed in the Specific Plan, the Mixed Use – Central Business District would allow for mixeduse development to support a pedestrian-friendly urban environment and provide for a variety of employment, entertainment, cultural, and shopping opportunities for City residents and visitors. The Mixed Use – Institutional District would serve as the major governmental, educational and office center for Downtown Pomona, but would still allow for mixed-use development with residential and retail uses. Institutional uses envisioned for this district would include support uses for the adjacent civic center (e.g., notaries, copy centers, legal counsel) and higher education development, particularly a campus expansion for Western University. With the location of the Pomona Transit Center within the Mixed Use - High Density Residential District, this district is envisioned as a pedestrian-friendly, high-density commuter residential neighborhood with ancillary retail, service, and office uses. The Residential – Multifamily District is intended to provide areas for a wide variety of medium to high-density residential development (e.g., townhouses, condominiums, apartment buildings) and schools, daycare centers, parks, and community facilities. These districts are consistent with the General Plan's land use designations for the Plan area, which consist of Activity Center, Transit-Oriented District: Core, Transit-Oriented District: Neighborhood, and Urban Neighborhood. Overall, consistent with the vision for the Plan area under the General Plan, the Specific Plan includes goals, policies, and regulations to facilitate an increase in mixeduses, pedestrian-friendly streets, and transit mobility that would encourage residential and commercial activity in Downtown Pomona.

Plan Area Refinements

As part of the Downtown Pomona Specific Plan update effort, City staff identified related refinements to the Plan area's boundaries and designations. Prior to the update effort, many parcels were included in both the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan. Associated refinements delineate just one governing specific plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. These refinements would increase the Plan area to approximately 380 acres with boundaries at Holt Avenue to the north, Towne Avenue to the east, Mission Boulevard to the south, and White Avenue to the west. Figure 4 and Table 1 provide an exhibit and matrix describing boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Nonetheless, these refinements would also facilitate an increase in land uses that would encourage residential, commercial, and open space activity in Downtown Pomona.

CEQA Guidelines Applicability

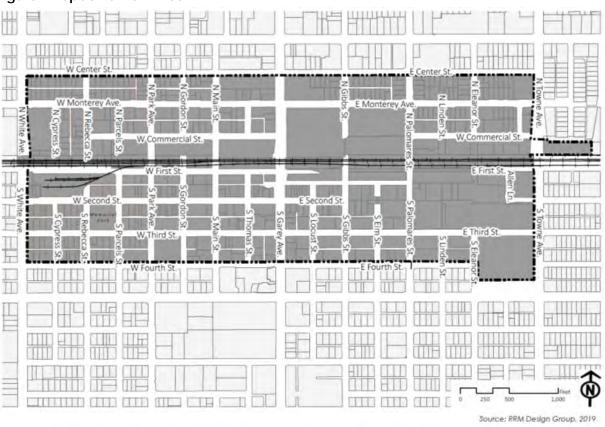
According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified", except as might be necessary to determine whether there are project-specific significant effects. Therefore, the following discussion evaluates the updated Specific Plan's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether the Specific Plan would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR.

Figure 1 Regional Context



Source: RRM Design Group, 2019

Figure 2 Specific Plan Area



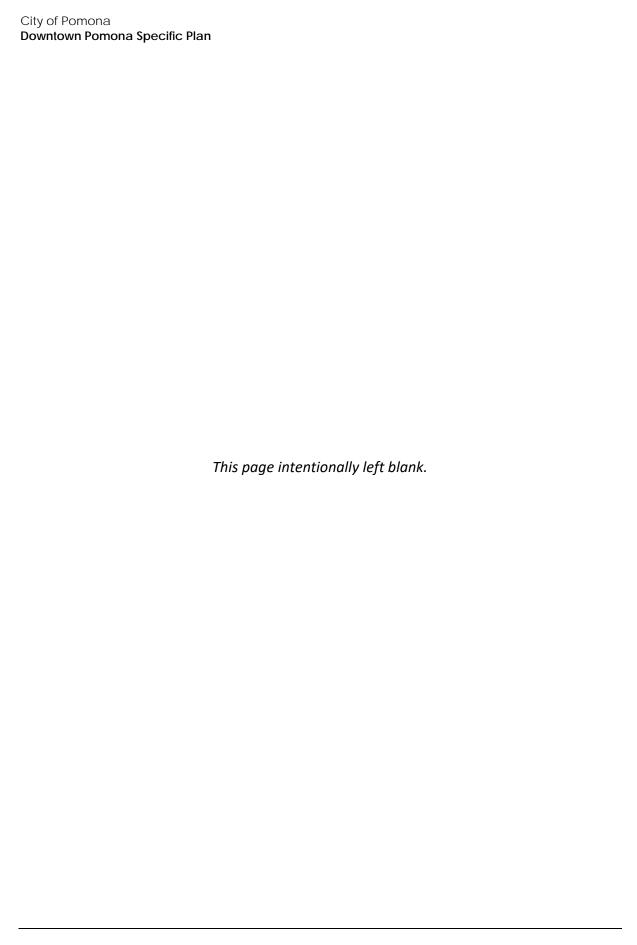


Figure 3 Specific Plan Districts



Source: RRM Design Group, 2019

Finding of Consistency 9

____Feet 1,000

Source: RRM Design Group, 2019

Figure 4 Specific Plan Area Refinements W Holt Ave. C Holt Ave. М anor St. E Center St. E Monterey Ave. E Third St. BB KK E Mission Blv DD W Mission Blvd. НН Adopted Specific Plan Boundary (1994) Proposed Specific Plan Boundary (2019) === Railroad Parcels currently in Downtown Pomona Specific Plan (DPSP); recommended for Pomona Corridors Specific Plan (PCSP)

Parcels currently in PCSP and DPSP; recommended for PCSP

Open Space, Pomona Zoning Ordinance

10

As discussed under *Plan Area Refinements*, Table 1 shows a comparison between the existing and proposed land uses associated with the boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Associated refinements delineate just one governing specific plan between the 1994 Downtown Pomona Specific Plan and the 2014 Pomona Corridors Specific Plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. Nonetheless, these refinements would also facilitate an increase in land uses that would encourage residential, commercial, and open space activity in Downtown Pomona.

Table 1 Refinements Matrix

Area	Existing Land Use(s)	Proposed Land Use
A	DPSP Mixed Use-Institutional (MU-I)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
В	DPSP Mixed Use-Institutional (MU-I) PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
С	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
D	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
E	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
F	DPSP Mixed Use-Institutional (MU-I)	
G	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
Н	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
I	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	PCSP City Gateway Segment + Height Overlay (CG + HT)
J	DPSP Mixed Use-Arterial Retail (MU-AR)	_
K	DPSP Mixed Use-Arterial Retail (MU-AR)	_
L	DPSP Residential-Multi-Family (R-MF); PCSP City Gateway Segment (CG)	PCSP City Gateway Segment (CG)
M	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP City Gateway Segment (CG)	_
N	DPSP Mixed Use-Arterial Retail (MU-AR)	_
0	DPSP Residential-Single-Family (R-SF) PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
Р	DPSP Residential-Single-Family (R-SF)	
Q	DPSP Mixed Use-Arterial Retail (MU-AR)	

Area	Existing Land Use(s)	Proposed Land Use
R	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	
S	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Neighborhood Center + Height Overlay (NC + HT)	PCSP Neighborhood Center + Height Overlay (NC + HT)
Т	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Downtown Gateway Segment + Height Overlay (DG + HT)
U	DPSP Mixed Use-High Density Residential (MU-HDR); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	DPSP Mixed Use-High Density Residential (MU-HDR)
V	DPSP Mixed Use-Institutional (MU-I); PCSP Downtown Gateway Segment + Height Overlay (DG + HT)	_
W	DPSP Mixed Use-Institutional (MU-I); DPSP Residential-Single-Family (R-SF)	Pomona Zoning Ordinance Open Space (PZC OS)
Х	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Downtown Core (DTC)	DPSP Mixed Use-Central Business District (MU-CBD)
Υ	DPSP Mixed Use-Institutional (MU-I)	Pomona Zoning Ordinance Open Space (PZC OS)
Z	DPSP Residential-Single-Family (R-SF)	PCSP Midtown Segment (MT)
AA	DPSP Mixed Use-Central Business District (MU-CBD)	-
ВВ	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Downtown Core (DTC)	PCSP Downtown Core (DTC)
СС	DPSP Mixed Use-Institutional (MU-I)	PCSP Midtown Segment (MT)
DD	DPSP Mixed Use-Arterial Retail (MU-AR); PCSP Midtown Segment (MT)	_
EE	DPSP Mixed Use-Central Business District (MU-CBD); PCSP Midtown Segment (MT)	_
FF	DPSP Mixed Use-Arterial Retail (MU-AR)	_
GG	DPSP Mixed Use-Arterial Retail (MU-AR)	_
НН	DPSP Mixed Use-Institutional (MU-I)	Pomona Zoning Ordinance Open Space (PZC OS)
II	DPSP Mixed Use-Institutional (MU-I)	PCSP Midtown Segment (MT)
11	DPSP Mixed Use-Institutional (MU-I)	_
KK	DPSP Mixed Use-Institutional (MU-I); PCSP Midtown Segment + Height Overlay (MT + HT)	PCSP Midtown Segment + Height Overlay (MT + HT)
LL	DPSP Mixed Use-Institutional (MU-I) PCSP Midtown Segment (MT)	PCSP Midtown Segment (MT)
MM	DPSP Residential-Multi-Family (R-MF); PCSP Midtown Segment (MT)	_

Area	Existing Land Use(s)	Proposed Land Use
NN	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Midtown Segment + Height Overlay (MT + HT)
00	DPSP Mixed Use-Arterial Retail (MU-AR)	
PP	DPSP Mixed Use-Arterial Retail (MU-AR)	PCSP Midtown Segment (MT)
QQ	DPSP Mixed Use-Arterial Retail (MU-AR)	
RR	DPSP Mixed Use-Arterial Retail (MU-AR);	PCSP Midtown Segment + Height Overlay
	PCSP Midtown Segment + Height Overlay (MT + HT)	(MT + HT)
SS	DPSP Mixed Use-Arterial Retail (MU-AR);	PCSP Midtown Segment (MT)
	PCSP Midtown Segment + Height Overlay (MT + HT)	

As shown in Table 1, the 2014 Pomona Corridors Specific Plan would govern most of the proposed land uses, which are defined as follows (Pomona 2014):

- Downtown Core (DTC) Promote a vibrant mixed-use urban shopping district. Facilitate the augmentation of the retail core with additional daytime and nighttime activity anchors, shopping, eating, and entertainment venues, residences, offices, and hotel rooms, as well as a widening variety of pedestrian amenities, connected streets, and public spaces.
- Downtown Gateway Segments (DG) Provide for a mixture of townhomes, smaller scale multi-family buildings, and single and multiple tenant retail shops, services, offices, or hotels all oriented towards the street. Encourage streetscape improvements including park-like, treelined landscaped medians to help break up wide corridors, continuous sidewalk-side street tree canopies and planter strips to create a comfortable "buffer zone" for pedestrians, and broad sidewalk to promote walkability.
- City Gateway Segments (CG) Provide for new multifamily housing that would mix compatibly with commercial, office, and lodging neighbors. Encourage streetscape improvements that reinforce these segments as important, high volume, and high visibility concentrations of commercial sales and services, as well as potential locations for higher density housing.
- Midtown Segments (MT) Promote housing and housing compatible development that fronts onto the street and would create a neighborhood edge along streets lined with shade trees and decorative neighborhood-scale streetlights. In areas characterized by large scale blocks, new development would break up "superblocks" with new internally connected streets for buildings to face and for access to parking facilities. The resulting smaller blocks would increase neighborhood walkability.

2 Environmental Impacts

The following discussion addresses each of the environmental issues studied in the 2014 Final EIR for the City's General Plan Update, comparing the effects of the updated Specific Plan, including the boundary and designated refinements, to the effects of the adopted General Plan and overall consistency between the two documents. These environmental issues include aesthetics, air quality, biological resources, cultural resources, geology and soils, greenhouse gases, hazards and hazardous materials, hydrology and water quality, land use and planning, noise, population and housing, public services, recreation, transportation and traffic, and utilities and service systems. However, in December 2018, Appendix G (CEQA Checklist) of the CEQA Guidelines was amended and two new environmental issues were added: energy and wildfire. These topics are discussed under *Greenhouse Gases* and *Hazards and Hazardous Materials* in the following discussion.

Aesthetics

Scenic Vistas

As stated in the 2014 Final EIR, scenic views in the City include background mountain views and depend upon the viewer's vantage point and orientation. The 2014 Final EIR concluded that development of new urban uses would be regulated by the General Plan's policies and result in a less than significant impact to scenic vistas. Given the City's surrounding hillside and mountain views, new development facilitated under the Specific Plan would potentially affect scenic vistas from various Downtown locations. However, as envisioned in the General Plan Update, Downtown Pomona is currently slated for the greatest building heights and development intensities in the City, which would be further regulated by the Specific Plan. Nonetheless, the Plan area is flat and urbanized. Development facilitated by the Specific Plan would be consistent to what was considered in the 2014 Final EIR and would not significantly affect any identified scenic vistas. Therefore, the Specific Plan would not result in an impact to scenic vistas beyond that identified in the 2014 Final EIR.

Scenic Resources

As stated in the 2014 Final EIR, no officially designated State Scenic Highways traverse the City and development under the General Plan would not impact scenic resources. Although State Route 57 between State Route 90 and State Route 60 is considered eligible by the California Department of Transportation for this designation, the eligible highway segment is located approximately 2.5 miles southwest from City limits and existing development interrupts views from the from the highway of the City and potential future development that would occur under the Specific Plan. Therefore, similar to what was determined in the 2014 Final EIR, the Specific Plan would not impact scenic resources within a State-designated scenic highway.

Visual Character

As concluded in the 2014 Final EIR, the General Plan contains goals and policies designed to enhance the City's visual character through complementary development and result in a less than significant on the visual character of the City. The Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height) that reflect the area's visual character as envisioned by the City's General Plan. As

stated in the 2014 Final EIR, the General Plan emphasizes building form and character in districts and neighborhoods to allow a mix of land uses, a renewed Downtown Pomona, and redefined corridors. The Specific Plan would implement goals, policies, and regulations to facilitate an increase in mixed-uses, pedestrian-friendly streets, and transit mobility that would encourage residential and commercial activity in and around the Plan area. New development under the Specific Plan would change the visual character of Downtown; however, such development would be consistent with the General Plan's vision for this area and subject to existing building and development standards in the City's Municipal Code. The Specific Plan would not result in an impact to visual character beyond that identified in the Final EIR.

Light and Glare

As stated in the 2014 Final EIR, development under the General Plan would primarily result in infill of vacant lands, as well as intensification and the reuse of existing sites, and most new development would not be located adjacent to large pieces of undeveloped lands where added light and glare would result in an impact. Similarly, the Specific Plan area is primarily built-out, and a substantial amount of ambient light from urban uses (e.g., buildings, parking lots or structures, vehicle surfaces or headlights) already exists. Therefore, new development under the Specific Plan would not create a significant change compared to existing lighting conditions. Furthermore, the Specific Plan includes standards for sign illumination permitted in the Plan area, including the requirement for light sources to be shielded and directed away from adjacent residential uses. Consistent with the finding in the 2014 Final EIR for the General Plan Update, the Specific Plan would also not result in an impact from increased light and glare.

Air Quality

Air Quality Management Plan

As stated in the 2014 Final EIR, projects that are consistent with the growth projections in the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS) prepared by the Southern California Association of Governments (SCAG) are considered consistent with the South Coast Air Quality Management District (SCAQMD) Air Quality Management Plan (AQMP) growth projections. The 2014 Final EIR concluded that implementation of the General Plan would facilitate population growth consistent with SCAG forecasts. Overall, the Specific Plan would encourage higher density, mixed-use, walkable development around activity centers and transit stations in the Plan area as envisioned by the General Plan and would not generate growth beyond levels anticipated by the General Plan. By facilitating increased density, mixed-uses, and transit mobility, implementation of the Specific Plan would also reduce per capita air pollutant emissions associated with vehicle use, which is also consistent with the AQMP. Therefore, as with the General Plan, the Specific Plan would be consistent with the AQMP growth projections and air quality control measures.

Air Quality Emissions

Individual development projects facilitated by the Specific Plan would generate temporary construction and long-term operational air pollutant emissions (e.g., vehicle trips and stationary sources), including potential increases in carbon monoxide (CO) concentrations and odors. Nonetheless, development facilitated by the Specific Plan would be consistent to what was considered in the 2014 Final EIR and would not generate a new impact related to air quality when

compared to buildout under the General Plan. Furthermore, the South Coast Air Basin has been in attainment of state and federal CO standards for several years. As determined in the 2014 Final EIR, future development associated with the General Plan would not result in traffic congestion at intersections that would cause an exceedance of CO standards. The Specific Plan would encourage development around activity centers and transit stations in the Plan area as envisioned by the General Plan. Therefore, because development intensity and associated trip generation would be consistent with the General Plan, the Specific Plan would not generate CO concentrations exceeding levels anticipated by the General Plan. Furthermore, odors from new residential, commercial, and open space uses would be consistent with existing odors throughout the Plan area. Overall, the Specific Plan would not result in air quality impacts from construction or operation emissions beyond those identified in the 2014 Final EIR.

Biological Resources

As discussed in the 2014 Final EIR, new or intensified development in Pomona could lead to the direct or indirect degradation of special-status species, sensitive habitats, nesting avian species, natural and man-made wetlands, that would be subject to potential impacts from construction (e.g., demolition, fugitive dust) and operation (e.g., noise, lighting) of urban uses in the Plan area. The Specific Plan would facilitate new and intensified development in Downtown Pomona, which does not support a wide diversity of biological resources, does not contain wildlife movement corridors, and does not pertain to a Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. Nevertheless, as with the General Plan, development under the Specific Plan would be subject to federal and state natural resources regulations and Mitigation Measure BIO-2 from the Final EIR, which would reduce impacts to nesting avian species. Because the Plan area is urbanized, impacts to special-status species, sensitive habitats, and wetlands would not occur under the Specific Plan. Furthermore, development under the Specific Plan would be subject to the City's Oak Tree Preservation Ordinance, which prohibits trimming, pruning, cutting, relocation or removal of any oak tree subject to the Ordinance unless a valid permit has been issued by the City. Based on these facts, the Specific Plan would not result in an impact to biological resources beyond that identified in the 2014 Final EIR.

Cultural Resources

Historic Resources

As discussed in the 2014 Final EIR, several designated historic resources are in or in proximity to Downtown Pomona and implementation of the General Plan would result in a significant, but mitigable, impact to historic resources. Similarly, any future development project facilitated by the Specific Plan that is located near an identified historical resource in Downtown would have the potential to result in a substantial adverse change to a historical resource. As with the General Plan, individual development projects would be subject to Mitigation Measure CUL-1 from the Final EIR, which would require historical evaluation of any affected property and implementation of methods aimed at reducing impacts to historic resources. The Specific Plan would not result in an impact to historic resources beyond that identified in the Final EIR.

Cultural Resources

As concluded in the 2014 Final EIR, ground-disturbing activities associated with development under the Specific Plan could result in damage or destruction to archaeological resources, paleontological resources, and human burials; however, compliance with existing federal, state, and local policies

would reduce impacts to a less than significant level. Although the Plan area is built-out, ground-disturbing activities that could occur under the Specific Plan would also have the potential to damage or destroy historic or prehistoric archaeological resources, paleontological resources, and human burials outside of formal cemeteries that may be present on or below the ground surface. However, as stated in the 2014 Final EIR, various goals and policies in the General Plan would require a qualified on-site archaeological and/or paleontological monitor whenever there is a potential to affect cultural resources during ground-disturbing as well as the scientific recovery and evaluation of any unearthed cultural resource. Furthermore, Section 5097 of the California Public Resources Code and Sections 7050.5, 7051, and 7054 of the California Health and Safety Code have specific provisions for the protection and treatment of human burials. Therefore, as with the General Plan, the Specific Plan would comply with existing federal, state, and local policies and result in less than significant impacts to cultural resources. Overall, the Specific Plan would not result in an impact to cultural resources beyond that identified in the 2014 Final EIR.

Geology and Soils

As concluded in the 2014 Final EIR, implementation of the General Plan would result in additional structures in the City with exposure to geologic impacts; however, compliance with applicable building standards would reduce impacts to less than significant levels. The Specific Plan would facilitate development in Downtown Pomona as envisioned by the City's General Plan, resulting in an increase in residential and mixed-use buildings in the City consistent to what was anticipated for the Plan area and considered in the 2014 Final EIR. Additional residents and employees would be potentially exposed to the effects of strong seismic ground shaking, seismic-related ground failure, liquefaction or landslides, erosion or loss of topsoil, expansive soils, subsidence, or collapse. Individual Downtown area developments would be required to comply with the California Building Code (CBC), which provides standards for excavation, grading, and earthwork construction; fills and embankments; expansive soils; foundation investigations; and liquefaction potential and soils strength loss. In addition, as with all new development under the General Plan, new development under the Specific Plan greater than one acre in size would be required to comply with the provisions of the General Construction Activity Stormwater Permit adopted by the State Water Resources Control Board (SWRCB), which would require the employment of Best Management Practices (BMPs) to limit the extent of eroded materials from a construction site. All development on sites of between one and five acres would be required to comply with the provisions of the National Pollution Discharge Elimination System (NPDES) Phase II regulations concerning the discharge of eroded materials and pollutants from construction sites. Furthermore, all developers would be required to obtain a grading permit prior to grading activities. In turn, as described in the 2014 Final EIR, all work requiring a grading permit would be required to have an approved Erosion Control Plan. Overall, the Specific Plan would not result in a geologic impact beyond that identified in the 2014 Final EIR and impacts would be less than significant.

Greenhouse Gases

As stated in the 2014 Final EIR, the General Plan would encourage compact development; promote the establishment and practice of alternative transit, such as walking and biking, as a mode of transportation; increase use of renewable energy resources; and reduce per capita energy consumption, which would be consistent with the goals of SB 375 and would contribute to long-term reductions in per capita greenhouse gas (GHG) emissions. The 2014 Final EIR concluded that the increase in per capita GHG emissions under the General Plan would be less than significant.

Downtown Pomona Specific Plan

Development facilitated by the Specific Plan would generate new GHG emissions through vehicle use and energy consumption. However, as discussed under *Air Quality*, the Specific Plan would encourage higher intensity, mixed-use, walkable development around activity centers and transit stations in the Plan area as envisioned by the General Plan. By increasing development intensity and encouraging mixed-uses and transit mobility, the Specific Plan would also reduce vehicle trips consistent with the General Plan. Furthermore, as stated the Specific Plan, energy conservation measures required by applicable energy conservation regulations, such as those in the California Green Building Code, would minimize long-term energy use. Overall, development facilitated by the Specific Plan would reduce per capita GHG and other air pollutant emissions, consistent with the goals of the SCAG RTP/SCS and applicable GHG reduction targets. The Specific Plan would not increase GHG emissions beyond what is anticipated in the 2014 Final EIR and all Downtown area development would be subject to regulations in place at the time the development is proposed that are aimed at achieving statewide GHG reduction targets, including the 2030 target of a 40 percent reduction from 1990 emission levels outlined in Senate Bill (SB) 32. The Specific Plan would not result in an impact from GHG emissions beyond that identified in the 2014 Final EIR.

Hazards and Hazardous Materials

Hazardous Waste and Materials

As concluded in the 2014 Final EIR, implementation of the General Plan would result in the transport, use, storage, and disposal of hazardous materials within the City; however, compliance with applicable federal, state, and local standards would reduce associated impacts to less than significant levels. Implementation of the Specific Plan would facilitate development in Downtown Pomona. As with development under the General Plan, construction and demolition activities that could result in the release of lead and/or asbestos would be conducted according to California Occupational Safety and Health Administrations (Cal/OSHA) standards. In addition, developers would comply with all applicable federal, state, and local laws and regulations pertaining to the transport, use, disposal, handling, and storage of hazardous waste, including but not limited to, Title 49 of the Code of Federal Regulations. Although unlikely with residential and mixed-uses, any development that handles or uses hazardous materials would be required to comply with the regulations, standards, and guidelines established by the Environmental Protection Agency (EPA), State, Los Angeles County, and City of Pomona related to storage, use, and disposal of hazardous materials. With compliance with existing applicable federal, state, and local regulations, the Specific Plan would not result in impacts associated with hazardous materials beyond those identified in the 2014 Final EIR.

Site Contamination

As discussed in the 2014 Final EIR, the City contains over 190 sites that have been identified as being contaminated from the release of hazardous substances in the soil, including landfills, sites containing leaking underground storage tanks, and large and small-quantity generators of hazardous waste. The 2014 Final EIR concluded that implementation of state and local regulations as well as General Plan policies would reduce impacts from known contaminated sites in the City to less than significant levels. As shown in Figure 4.6-1 of the 2014 Final EIR, various contaminated sites are were identified in the Specific Plan area. While implementation of the Specific Plan could lead to development of these sites, such development would be required to undergo remediation and cleanup under the Department of Toxic Substances Control (DTSC) and the Regional Water

Quality Control Board (RWQCB) prior to development. The Specific Plan would not result in impacts from contaminated sites beyond those identified in the 2014 Final EIR.

Airport Safety

While parts of the City are in the current airport land use plan of Brackett Field, a public use airport in the City of La Verne operated by Los Angeles County, the refined Downtown Specific Plan area is located approximately two miles south of the airport. Therefore, implementation of the Specific Plan would not result in the construction of residential and mixed-uses within two miles of an airport facility and would not result in a safety impact for people residing or working in the Plan area beyond that identified in the Final EIR.

Emergency Response and Evacuation Plans

The General Plan contains policies to provide for the proper implementation of adopted emergency response and evacuation plans. The 2014 Final EIR concluded that, with implementation of these policies, development under the General Plan would result in less than significant impacts to emergency response and evacuation plans. Because development envisioned for Downtown Pomona under the Specific Plan is consistent with the General Plan, the Specific Plan would not result in an impact to adopted emergency response and evacuation plans beyond that identified in the 2014 Final EIR.

Wildfire Hazards

Parts of Pomona are susceptible to wildland fires due to its hilly terrain, dry weather conditions and the nature of its plant cover. As concluded in the 2014 Final EIR, policies included in the General Plan would reduce the exposure of people or structures to significant risk of loss, injury, or death involving wildland fires to less than significant levels. However, the Downtown Specific Plan area is primarily built-out and located in the City's center (see Figure 1), which is fully urbanized and surrounded by other urban development. Therefore, development facilitated by the Specific Plan would not be directly susceptible to wildfires. The Specific Plan would not result in an impact involving wildland fires beyond that identified in the 2014 Final EIR.

Hydrology and Water Quality

Water Quality and Wastewater

As concluded in the 2014 Final EIR, compliance with the NPDES permit requirements, the City's Municipal Code, and General Plan policies would reduce water pollutants from the operation of new development to the maximum extent practicable, resulting in less than significant impacts to water quality and waste discharge requirements. Development under the Specific Plan could result in an increase in pollutants in stormwater and wastewater, although water quality standards and waste discharge requirements would not be violated. Water quality degradation from construction would be specific to each individual project, and thus would depend largely on the areas affected and the length of time soils are subject to erosion, and construction activities on-site. However, as stated in the Specific Plan, the primary regulatory control for the protection of water quality NPDES permit administered SWRCB. The SWRCB and Los Angeles RWQCB establish requirements prescribing the quality of point sources of discharge and establish water quality objectives. In addition, the City of Pomona protects water resources through implementation of its Storm Water Pollution Prevention Program as required by the NPDES permit program. Further, Chapter 18 (Environment) Division 3

(Discharge Regulations and Requirements) of the City's Municipal Code sets forth regulations associated with stormwater. All these requirements would apply to development throughout the City, including the Downtown Specific Plan area. Therefore, the Specific Plan would not result in an impact regarding pollutants in stormwater and wastewater beyond that identified in the Final EIR.

Groundwater Supplies, Drainage, Flooding, and Runoff

The 2014 Final EIR concluded that compliance with the NPDES permit requirements, the City's Municipal Code, and General Plan policies, would reduce impacts to groundwater supplies, drainage patterns, downstream floods, and urban runoff to less than significant levels. As with the General Plan, new development facilitated by the Specific Plan would be primarily consists of infill in already urbanized areas, where increases in impervious surfaces would be minimal. As stated in the Specific Plan, only an incremental expansion in the quantity of net new impervious surfaces is expected as a part of future development. In compliance with federal, state, regional and local requirements, contractors constructing new development or redevelopment projects are required to implement BMPs. Furthermore, the Specific Plan includes storm drainage policies, which would provide for storm drain infrastructure and systems (e.g., detention basins, vegetated swales, rain gardens, permeable paving, rainwater harvesting) to remove pollutants from stormwater runoff prior to discharge. Therefore, the Specific Plan would not result in impacts related to groundwater supplies and quality, existing drainage patterns, downstream flooding, and polluted runoff beyond those identified in the 2014 Final EIR.

Hydrological Risks

As stated in the 2014 Final EIR, development under the General Plan would not place people or structures within a flood zone. While the City is within the San Antonio Dam inundation zone, compliance with the City's Municipal Code and General Plan policies would reduce flooding impacts to less than significant levels. The 2014 Final EIR also concluded that, due to the City's geographic location, people or structures in the City would not be impacted by a seiche, tsunami, or mudflow. As stated in the 2014 Final EIR, the potential for a flood event exists within Pomona in the form of dam inundation from the San Antonio Dam and Live Oak Reservoir. While new development under the Specific Plan could increase the quantity of residential uses with risk of inundation, Section 18-640 of the Municipal Code requires all new construction and substantial improvements within a flood-prone area to be designed and adequately anchored to prevent flotation, collapse, or lateral movement. The section also requires the use of flood-resistant building materials and construction methods. Sections 18-641 and 18-642 of the City's Municipal Code establish requirements for the construction of utilities in flood-prone areas. Overall, the Specific Plan would not increase risk of inundation or increase exposure of people, employees, or structures to a flood risk beyond that identified in the 2014 Final EIR.

Land Use and Planning

As stated in the 2014 Final EIR, the General Plan emphasizes building form and character in districts and neighborhoods to allow a mix of land uses, a renewed Downtown Pomona, and redefined corridors. The 2014 Final EIR concluded that no substantial land use or circulation changes that would physically divide an established community would occur under the General Plan. As discussed under *Aesthetics*, the Specific Plan establishes a district-based zoning approach in which each of the Plan's districts would be regulated by form standards (e.g., density, number of stories, height) that reflect the area's visual character as envisioned by the City's General Plan. Figure 3 shows the boundaries of the Plan area's four districts: Mixed Use – Central Business District (MU-CBD), Mixed

Use – Institutional (MU-I), Mixed Use – High Density Residential (MU-HDR), and Residential – Multifamily R-MF). Furthermore, as part of the Downtown Pomona Specific Plan update effort, City staff identified related refinements to the Plan area's boundaries and designations. Prior to the update effort, many parcels were included in both the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan. Figure 4 and Table 1 provide an exhibit and matrix describing boundary and designation refinements accompanying the updated Downtown Pomona Specific Plan. Nonetheless, the Specific Plan includes goals, policies, and regulations to facilitate an increase in mixed-uses, pedestrian-friendly streets, and transit mobility that, in conjunction with the associated refinements, would encourage residential and commercial activity in and around the Plan area and improve overall connectivity. Therefore, as with the General Plan, implementation of the Specific Plan would not physically divide an established community.

Consistency with Applicable Plans

As concluded in the 2014 Final EIR, implementation of the General Plan would be generally consistent with applicable adopted plans, regulations, or policies. Several regionally and locally adopted land use plans, policies, and regulations are applicable to development under the Specific Plan, which include the SCAQMD AQMP, SCAG RTP/SCS, the City's Municipal Code, and the City's General Plan. As discussed under Air Quality, the Specific Plan would be consistent with development envisioned in the General Plan and within SCAG growth forecasts. By increasing the density, mixed-uses, and transit mobility, implementation of the Specific Plan would also be expected to generally reduce per capita air pollutant emissions associated with vehicle use, which is also consistent with the AQMP. The Specific Plan refines the uses set forth in the General Plan and provides further guidance for development in the Plan area but would not change the future land use pattern for the area relative to the General Plan. Furthermore, associated refinements delineate just one governing specific plan between the 1994 Downtown Pomona Specific Plan and 2014 Pomona Corridors Specific Plan to avoid conflicting regulations as well other minor changes in line with the updated General Plan. Therefore, the refined Specific Plan would not generate substantial conflicts with the City's Zoning Map or General Plan. The Specific Plan would not involve consistency conflicts with land use plans, policies, or regulations not identified in the 2014 Final EIR.

As discussed in the 2014 Final EIR, the City does not currently have a habitat conservation or natural community conservation plan. The Downtown Specific Plan area also is not subject to a habitat conservation or natural community conservation plan. Furthermore, development under the Specific Plan would be subject to the City's Oak Tree Preservation Ordinance. Therefore, the Specific Plan would not conflict with any habitat conservation plan or natural community conservation plan.

Farmland Conversion

As concluded in the 2014 Final EIR, the City does not contain land designated as Prime Farmland, Unique Farmland or Farmland of Statewide Important by the State's Farmland and Mapping Program. The Downtown Specific Plan area is built-out with urban uses. Therefore, as with the General Plan, the Specific Plan would not convert potential agricultural lands to urban uses or conflict with a Williamson Act contract.

Noise

Temporary and Permanent Noise

As concluded in the 2014 Final EIR, implementation of the City's existing noise regulations and standards, as well as goals and policies of the General Plan, would reduce potential noise impacts related to existing and future land uses to less than significant levels. Development facilitated by the Specific Plan would occur within Downtown Pomona and along redefined corridors and would generate temporary and permanent sources of noise. Under the Specific Plan, the primary source of temporary or periodic noise would be construction activity and maintenance work. However, Section 18-305 of the City's Municipal Code allows noise sources associated with construction, repair, remodeling or grading of any real property or during authorized seismic surveys, provided such activities do not take place between the hours of 8 PM and 7 AM on weekdays, including Saturday, or at any time on Sunday or a federal holiday, and provided the noise level created by such activities does not exceed the noise standard of 65 dB(A) plus the limits specified in section 18-311(b) of the Municipal Code as measured on residential property. With compliance with these restrictions, construction noise impacts associated with buildout of the Specific Plan would not create a significant adverse effect on sensitive receptors. Because development facilitated by the Specific Plan would be similar to that envisioned in the General Plan, operational noise associated with development under the Specific Plan would remain the same as what was considered in the Final EIR. Implementation of the Specific Plan would not result in temporary or operational noise impacts beyond those identified in the Final EIR.

Groundborne Vibration

Groundborne vibration in Pomona is generated primarily by two sources: temporary construction activities and permanent traffic on roadways and railways. As concluded in the 2014 Final EIR, construction vibration would result in less than significant impacts; however, vibration impacts associated with active rail lines would result in significant, but mitigable, impacts. Construction activities under the Specific Plan would also generate groundborne vibration. However, as with individual developments under the General plan, the City would review the potential for such impacts before it issues building permits and would require measures such that physical damage to neighboring buildings would not occur before issuing a building permit. Automotive traffic on roadways and train traffic on railways under the Specific Plan would also produce groundborne vibration. The Specific Plan would facilitate new residential uses in transit-oriented districts subject to potential vibration-related impacts from rail lines along First Street. However, as with the General Plan, the Specific Plan would comply with Mitigation Measure N-1 of the 2014 Final EIR, which would reduce the impact of groundborne vibration on new residential uses located near Pomona's active transit rail line along East 1st Street. Therefore, implementation of the Specific Plan would not result in impacts from groundborne vibration beyond those identified in the 2014 Final EIR.

Airport Noise

The 2014 Final EIR concluded that future development at the Fairplex could include residential uses that could be exposed to airport noise levels in excess of existing thresholds, which would result in a significant, but mitigable, impact. However, as discussed under *Hazards and Hazardous Materials*, the refined Downtown Specific Plan area is located approximately two miles south of Brackett Field. As determined in the 2014 Final EIR, most of the City is located outside of the airport's noise contours, including the Downtown Specific Plan area. While development facilitated by the Specific

Plan would be exposed to temporary and intermittent overflight noise, the Specific Plan would not result in exposure to airport noise.

Population and Housing

Growth

As concluded in the 2014 Final EIR, implementation of the General Plan would not generate growth in population, jobs, and housing in excess of SCAG's RTP/SCS forecasts. As discussed under *Air Quality*, the Downtown Specific Plan would encourage higher density, mixed-use, walkable development around activity centers and transit stations in Downtown Pomona as envisioned by the General Plan but would not generate growth beyond levels anticipated in the General Plan or the 2014 Final EIR. Furthermore, as envisioned by the General Plan, the Specific Plan would facilitate the development of a mix of residential and commercial uses in proximity to one another, which would encourage a balance of jobs and housing. Therefore, the Specific Plan would not result in an exceedance in population and housing projections or increase population growth beyond what is forecast in the 2014 Final EIR.

Displacement

As concluded in the 2014 Final EIR, implementation of the General Plan would not result in the displacement of substantial numbers of people or housing given that the General Plan would facilitate the development of new housing. As with the General Plan, new and intensified development under the Specific Plan would likely displace existing residences. However, consistent with the General Plan, future buildout under the Specific Plan would increase residential density in Downtown Pomona and offset any displaced residences. Therefore, the Specific Plan would not result in the displacement of substantial numbers of people or housing and would not increase impacts related to displacement beyond those identified in the 2014 Final EIR.

Public Services

Fire Protection

As concluded in the 2014 Final EIR, development facilitated by the General Plan increase demand for fire protection services and potentially create the need for new fire protection facilities; however, compliance with applicable codes and regulations and compliance with General Plan Update policies would reduce impacts to a less than significant level. Fire protection services are provided by the Los Angeles County Fire Department (LACoFD) and include fire, rescue, hazardous materials prevention, and emergency services. As stated in the Specific Plan, the LACoFD uses national guidelines for response time targets which consist of five minutes for the first arriving unit for fires and basic life support and eight minutes for advanced life support (paramedic) in urban areas. Pomona is part of the LACFD Division III and the nearest fire department facility is Station 181 - Division and Battalion Headquarters located at 590 South Park Avenue near the southern boundary of the Plan area. Any new development that would occur under the Specific Plan would be in the existing LACoFD service area and would be required to comply with all applicable federal, state, and local regulations governing the provision of fire protection services, including adequate fire access, fire flows, and number of hydrants. These project-specific requirements include construction standards in new structures and remodels, road widths and configurations designed to accommodate the passage of fire trucks and engines, and requirements for minimum fire flow rates

Downtown Pomona Specific Plan

for water mains. Based on these facts, the Downtown Specific Plan would not result in an impact to existing fire protection facilities nor result in the need for additional fire protection facilities and no impacts beyond those identified in the 2014 Final EIR would occur.

Police Protection

The 2014 Final EIR concluded that development facilitated by the General Plan increase demand for police protection services and potentially create the need for new police protection facilities; however, compliance with applicable codes and regulations and compliance with General Plan Update policies would reduce impacts to a less than significant level. The Pomona Police Department (PPD) provides police protection services in Pomona. As stated in the Specific Plan, the PPD provides services in crime investigation, offender apprehension, community awareness programs, traffic control, and other services. Seven facilities provide police services in Pomona. Of these facilities, three provide first-response services and include the Main Station at 490 W. Mission Boulevard, the Traffic Bureau at 100 West Commercial, and the Aero Bureau at 1905 McKinley. Implementation of the potential development allowed under the Specific Plan was addressed in the Final EIR, which includes policies to reduce crime and provide for law enforcement as the City experiences future growth. Nonetheless, police policies included in the Specific Plan would promote the integration of Crime Prevention Through Environmental Design principles (i.e., Natural Surveillance, Natural Access Control, and Territorial Reinforcement) for new development and public spaces. Overall, the Specific Plan would not result in an impact to police protection facilities or result in the need for additional police protection facilities. No impacts beyond those identified in the Final EIR would occur.

Schools

As concluded in the 2014 Final EIR, implementation of the General Plan would result in an increase in student enrollment; however, schools in the City have adequate capacity to serve the additional students and impacts associated with an increase in students were determined to be less than significant. The Specific Plan area is located in the Pomona Unified School District (PUSD). Existing facilities include elementary, middle and high schools as well as opportunities for higher level education. The increase in residential development under the Specific Plan would increase enrollment in local schools in the PUSD. Nonetheless, as with development under the General Plan, the payment of statutory fees pursuant to Section 65995 (3) (h) of the California Government Code (Senate Bill 50, chaptered August 27, 1998), "...is deemed to be full and complete mitigation of the impacts of any legislative or adjudicative act, or both, involving, but not limited to, the planning, use, or development of real property, or any change in governmental organization or reorganization." Therefore, the Specific Plan would not result in a significant impact related to schools and would have no impact related to schools beyond that identified in the Final EIR.

Recreation

As stated in the Final 2014 EIR, the City's target parkland to population ratio is 3 acres per 1,000 residents. Based on this target, there is an existing deficit in parkland and recreational facilities in the City. The 2014 Final EIR concluded that development of new parkland, bicycle facilities, and pedestrian facilities under the General Plan, as well as implementation of goals and policies associated with the General Plan, would reduce impacts associated with the demand for recreational facilities to less than significant levels. As with development under the General Plan, an increase in population resulting from implementation of the Specific Plan could increase demand on existing parkland and recreational facilities such that deterioration of these facilities would be

accelerated. However, Section 70.69 of the City's Municipal Code requires dedication of park lands or payment of in-lieu fees, or a combination thereof, for the construction of new dwelling units. In addition, improvement and expansion of the existing bicycle and pedestrian network envisioned under the Specific Plan would provide additional recreational facilities for use by City residents in addition to other planned parkland and recreation improvements in the General Plan. Furthermore, park and recreation policies included in the Specific Plan support the expansion of parks, recreation, and public spaces in Downtown Pomona. Based on these facts, the Specific Plan would not result in a significant impact related to recreation and would have no impact to parkland and recreational facilities beyond that identified in the 2014 Final EIR.

Transportation and Traffic

Transportation Facilities

As determined in the 2014 Final EIR, traffic associated with development facilitated by the General Plan would degrade operations at several intersections and three Congestion Management Program (CMP) facilities (i.e., two freeway segments and one intersection), to below identified City standards. While none of these intersections and CMPP facilities are in the Specific Plan area, development facilitated by the Specific Plan would contribute additional vehicle trips throughout the City, including intersections and CMP facilities that are expected to operate unacceptably under buildout of the General Plan. Nonetheless, while the Specific Plan would incrementally contribute to the significant and unavoidable impacts to these intersections and CMP facilities associated with the General Plan, the Specific Plan would not increase traffic impacts to the roadway network beyond those identified in the 2014 Final EIR since it would not increase development intensity or associated vehicle trips compared to the General Plan.

Air Traffic Patters

As discussed in the 2014 Final EIR, there are no airports in the City and implementation of the General Plan would not impact air traffic pattern. As discussed under *Hazards and Hazardous Materials* and *Noise*, the Downtown Specific Plan area is located approximately two miles south of Brackett Field. Therefore, the Specific Plan would not affect air traffic patterns and would have no impact beyond that identified in the 2014 Final EIR.

Traffic Hazards and Site Accessibility

As concluded in the 2014 Final EIR, development under the General Plan would be required to meet City design standards and comply with General Plan policies aimed at improving traffic safety and accessibility, which would reduce impacts associated with traffic hazards to less than significant levels. As with development associated with the General Plan, individual projects under the Specific Plan would be required to meet all applicable local and State regulatory standards for site design and emergency access, including those in the CBC, Municipal Code, and Fire Code. Furthermore, projects under the Specific Plan would be required to undergo independent project-level design review by the City. Potential traffic hazards or inadequate emergency access from a design feature or incompatible use associated with individual projects would be reviewed during this process. Therefore, the Specific Plan would not generate traffic hazards or site accessibility issues and would have no impact beyond that identified in the 2014 Final EIR.

Alternative Transportation

As concluded in the 2014 Final EIR, implementation of the General Plan would enhance the use of alternative transportation modes in the City and would result in less than significant impacts. Implementation of the Specific Plan would enhance the use of alternative transportation modes, including transit, bicycling, and walking consistent with the General Plan. One of the central directives of the Specific Plan is to create pedestrian-friendly streets that include integrated transportation systems, provide pedestrian and bicycle links, and improve overall vehicular circulation. The General Plan includes a range of policies aimed at enhancement of alternative transportation mode opportunities throughout the City. These and Specific Plan policies aimed at improving transit mobility would reduce impacts related to alternative transportation to a less than significant level such that the Specific Plan would not result in impacts beyond those identified in the Final EIR.

Utilities and Service Systems

Water Supply

As stated in the 2014 Final EIR, implementation of the General Plan would increase demand for water supply; however, the City's 2010 Urban Water Management Plan (UWMP) projects that adequate water supply would be available to serve population growth in the City through 2035. The 2014 Final EIR concluded that development under the General Plan would have less than significant impacts to water supply. As with the General Plan, development facilitated by the Specific Plan would increase demand for water supply. However, water demand projections in the City's Urban Water Management Plan (UWMP) are based on SCAG population growth forecasts for the City. As stated in the 2014 Final EIR, the UWMP projects that the City will have adequate water supply, with normal conservation efforts, to meet demand through 2035 in average year. As discussed under Air Quality, development facilitated by the Specific Plan would be consistent with that envisioned by the General Plan and within SCAG growth forecasts. Therefore, water supplies would be adequate to serve population growth facilitated by the Specific Plan. Furthermore, as discussed in the Specific Plan, the City is signatory to the Memorandum of Understanding regarding Urban Water Conservation in California and is a member of the California Urban Water Conservation Council. As a signatory, the City has pledged to implement 14 conservation BMPs, which include water survey programs, residential plumbing retrofit, system water audits, landscape conservation programs, public and school education programs, conservation pricing, and a low-flush toilet replacement program. In addition to these BMPS, the Specific Plan includes water polices to reduce water consumption, require water saving devices for all development in the Plan area, and explore recycled water opportunities for irrigation. The Specific plan would not result in significant impacts to water supply or involve any impact beyond that identified in the 2014 Final EIR.

Wastewater Generation

The 2014 Final EIR concluded that implementation of the General Plan would not violate wastewater treatment requirements of the RWQCB and impacts associated with wastewater would be less than significant. Implementation of the Specific Plan would result in an increase in wastewater generation, and wastewater generated by new residential growth would require treatment. Wastewater service in Pomona is provided by the City's Public Works Department, and collected wastewater is treated by the Los Angeles County Sanitation Districts (LACSD). As discussed in the Specific Plan, LACSD has adopted ordinances over time establishing legal mechanisms to protect and finance operations and facilities, including the Industrial Wastewater Discharge Permit,

Connection Fee, and Discharge Programs. The Connection Fee Program requires all new uses of the LACSD sewerage system to pay their share of costs for providing additional conveyance, treatment, and disposal facilities. Therefore, as stated in the 2014 Final EIR, existing treatment plant capacity and infrastructure would accommodate planned future growth. The Specific Plan would not result in a significant impact to wastewater treatment facilities and would have no impact beyond that identified in the 2014 Final EIR.

Solid Waste Generation

As stated in the 2014 Final EIR, implementation of the General Plan would increase solid waste generation; however, landfills serving the City have adequate capacity to accept the additional waste and future development would comply with General Plan policies aimed at increasing recycling in the City. The 2014 Final EIR concluded that impacts from solid waste generation under the General Plan would be less than significant. Development facilitated by the Specific Plan would also increase the amount of solid waste sent to area landfills. However, as stated in the 2014 Final EIR, landfills serving the City have adequate capacity to accept the additional waste. Furthermore, as with development under the General Plan, individual developments facilitated by the Specific Plan would also be reviewed on a project-by-project basis and solid waste impacts would be evaluated based on existing and planned disposal facilities and capacities available. The Specific Plan also includes solid waste policies requiring all new development to participate in solid waste source reduction and diversion programs to the maximum extent practical. The Specific Plan would not result in significant impacts related to solid waste generation and would have no impact beyond that identified in the 2014 Final EIR.

Conclusion

As discussed in *Environmental Impacts*, development facilitated by the updated Downtown Specific Plan would be consistent with the development intensity established by the City's General Plan. Environmental impacts associated with the Specific Plan would be similar to those anticipated for the Downtown Plan area in the 2014 General Plan Final EIR and the Specific Plan would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required for the Specific Plan under CEQA.

3 References

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RRM Design Group. 2019. Downtown Pomona Specific Plan. Document.

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> POMONA CITY OF LEGAL ATTN: CITY CLERK PO BOX 660 POMONA, CA 91769

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Project Title: Comprehensive Update to the Downtown Pomona Specific Plan

Project Applicant: City of Pomona

Project Location: Area generally bounded by Holt Avenue to the North, Towne Avenue to the East, Mission Boulevard to the South, and White Avenue to the West.

Project Description: A comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019).

Lead Agency: City of Pomona, Development Services Department, Planning Division

Public Hearing Date The public hearing is scheduled for <u>Wednesday, June 26, 2019 at 7:00 p.m.</u>
& Location/Time: in the City of Pomona City Council Chambers located at 505 S. Garey Avenue in the City of Pomona.
<u>Environmental Review</u>

According to §15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified," except as might be necessary to determine whether there are project-specific significant effects. Staff has evaluated the proposed project 's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether it would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Environmental impacts associated with the proposed project would be similar to those anticipated for the project area in the 2014 General Plan Final EIR and would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required under CEQA. This environmental review will be considered by the Planning Commission at the public hearing for this project scheduled for June 26, 2019. Public Hearing Notice

Any interested individual may appear in person or by agent at the Planning Commission hearing and be heard on any matter relevant to such proceedings. The staff report on this matter will be available in the Planning Division on or about June 20, 2019. For questions, please call Ata Khan, Senior Planner, Planning Division at (909) 620-7365.

PLEASE NOTE: If you challenge this matter in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City of Pomona, Planning Division at, or prior to, the public hearing.

Para Información en Español, llame (909) 620-2191.

Date: June 11, 2019

Anita D. Gutierrez, AICP Development Services Director

Publication Date: June 14, 2019 Rosalia Butler City Clerk, City of Pomona

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POMONA CITY OF LEGAL ATTN: CITY CLERK **PO BOX 660** POMONA, CA 91769

> PROOF OF PUBLICATION (2015.5 C.C.P.)

STATE OF CALIFORNIA County of Los Angeles

I am a citizen of the United States, I am over the age of eighteen years, and not a party to or interested in the above-entitled matter. I am the principal clerk of the printer of INLAND VALLEY DAILY BULLETIN, a newspaper of general circulation printed and published daily for the City of Pomona, County of Los Angeles, and which newspaper has been adjudged a newspaper of general circulation by the Superior Court of the County of Los Angeles, State of California, on the date of June 15, 1945, Decree No. Pomo C-606. The notice, of which the annexed is a true printed copy, has been published in each regular and entire issue of said newspaper and not in any supplement thereof on the following dates, to wit:

06/14/2019

I declare under the penalty of perjury that the foregoing is true and correct.

Executed at Rancho Cucamonga, San Bernardino Co., California, on this 17th day of June, 2019.

John almeide

Signature

(Space below for use of County Clerk Only)

Legal No.

0011281538

NOTICE OF PUBLIC HEARING

Project Title:

Comprehensive Update to the Downtown

Pomona Specific Plan

Project Applicant:

City of Pomona

Project Location:

Area generally bounded by Holt Avenue to the North, Towne Avenue to the East, Mission Bouleyard to the South, and White

Project Description:

Mission Boulevara to the South, and white Avenue to the West.
A comprehensive update to the Downtown Pomona Specific Plan (SPA 6409-2016) and related updates to the Pomona Corridors Specific Plan (SPA 12106-2019) and Pomona Zoning Ordinance (ZONE 12104-2019).

Lead Agency:

City of Pomona, Development Services

Department, Planning Division

Public Hearing Date

The public hearing is scheduled for Wednesday, June 26, 2019 at 7:00 p.m.
& Location/Time: in the City of Pomona City Council Chambers located at 505 S. Garey Avenue in the City of Pomona.

Environmental Review

According to \$15183(a) (Projects Consistent with a Community Plan or Zoning) of the CEQA Guidelines, additional environmental review is not required for projects "which are consistent with the development density required for projects "which are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR was certified," except as might be necessary to determine whether there are project-specific significant effects. Staff has evaluated the proposed project's consistency with the certified 2014 Final EIR prepared for the City's General Plan Update to determine whether it would have new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Environmental impacts associated with the proposed project would be similar to those anticipated for the project area in the 2014 General Plan Final EIR and would not result in any new or increased severity significant environmental effects beyond those identified in the 2014 Final EIR. Mitigation beyond that identified in the 2014 Final EIR is not required. As such, pursuant to §15183 of the CEQA Guidelines, no additional environmental review or documentation is required under CEQA. This environmental review will be considered by the Planning Commission at the public hearing for this project scheduled for June 26, 2019.

Public Hearing Notice

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Rosalia Butler

City Clerk, City of Pomona

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